

# **Downtown Development Plan and US 730 Streetscape Plan**

**Irrigon, OR**

**DECEMBER 2009**

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*photos of Irrigon and the planning process*



## Introduction

The City of Irrigon and its citizens are excited to continue the visioning and planning work that will help define and establish their downtown. To date, the City has worked with various public agencies and design professionals to create a Downtown Development Framework Plan (2006), design and construct a new City Hall (2007), adopt a model zoning and development code for the downtown (2007), and secure Federal economic stimulus funds to construct sidewalks along First Street to improve pedestrian safety and connectivity in the community. The City has capitalized on the energy and momentum of these projects by creating this *Downtown Development Plan and US 730 Streetscape Plan* to further refine the concepts and ideas that have been expressed over the past three years. The resulting work is this document in two parts: *Part 1: Downtown Development Plan* and *Part 2: US 730 Streetscape Plan*.

The purpose of this document is to articulate a clear vision for the future location and development of a downtown for Irrigon and provide a level of detail for the concepts that will enable the City to pursue grants and other funding sources for infrastructure and development projects in the new Downtown and along US 730. The City hopes to attract and sustain new businesses, residents, and visitors through the comprehensive development of a diverse and vibrant Downtown while also serving the needs and desires of the City's current residents.

The proposed concepts in this Plan were developed assuming a 20-year planning horizon, which means that most of the illustrations and recommendations show how Irrigon could potentially be in the future - not how it is today. This document should serve as a strategic guide for growth so that both public and private resources are not used inefficiently. As such, it should be revisited and updated periodically to ensure that the Plan adequately reflects current economic, social, and community goals.

## Planning Process

The planning process began in late-November 2008 with a project kick-off meeting and an extensive review of background materials. Over a two-day period in early January 2009, the project team visited Irrigon to tour and photograph the study area, meet with individual community stakeholders, and host a public open house to gather additional information about the community and the residents' aspirations and desires for the future of the city.

After the site visit and public open house, the project team prepared an existing conditions report that included draft goals and objectives, a summary of previous planning efforts, a local market assessment, a transportation and traffic analysis, and an opportunities and constraints analysis.

In March 2009, the project team returned to Irrigon with three different downtown development and US 730 streetscape concepts and presented the

### **A VISION FOR DOWNTOWN IRRIGON**

*The City of Irrigon strives to develop a sustainable, diverse and vibrant downtown that enriches the experience of local residents and enables them to meet most, if not all, of their daily needs in town. A unique sense of place, reflecting a rich historic and agricultural heritage, will foster pride and establish downtown as the heart of the community: a place where visitors and locals will be more inclined to shop, relax, socialize, and gather. As part of this vision, Columbia Avenue is redefined as Irrigon's "Main Street" and becomes an integral part of the downtown business and civic core.*

benefits, challenges, and various trade-offs between them to the project's Project Management Team (PMT) and Technical Advisory Committee (TAC) for review and comment. Later that month, the project team returned with refined products to lead a public design workshop during which community members were asked to review the concepts, indicate the preferred elements from each, and provide input. The project team used this feedback to prepare a preferred concept for both the Downtown Development Plan and US 730 Streetscape Plan that reflected the community's desires.

Following the public design workshop in late March, the project team further refined the preferred concepts for the Downtown Development Plan and US 730 Streetscape Plan. These refined concepts and recommendations were presented to the Planning Commission, City Council, and general public on June 2, 2009 in a combined work session. Between June 2009 and December 2009, the US 730 Streetscape portion of the plan was refined to address ORS 366.215 and the need to maintain acceptable highway travel widths for the Oregon Freight Commission. The comments from that session were incorporated in this final draft of the Downtown Development Plan and US 730 Streetscape Plan report. All of the previous work products can be found in the Appendix to this document.

### **Goals and Objectives**

In addition to the vision established by this planning process, several goals were created to help direct public policy decision-making and measure success over time. The full set of goals and objectives can be found in the Appendix to this document.

#### ***GOAL 1: COMMUNITY DESIGN***

Make Downtown Irrigon a vital, lively, pedestrian-friendly focal point of the community.

#### ***GOAL 2: CONNECTIONS***

Create a Downtown transportation system that promotes safe mobility for all modes of travel to and within Downtown and along and across US 730.

#### ***GOAL 3: REVITALIZATION***

Identify and implement economic development strategies to revitalize Downtown Irrigon.

#### ***GOAL 4: SUSTAINABILITY***

Develop Downtown Irrigon and US 730 in a sustainable way to conserve resources and infrastructure investment.

These goals and objectives helped guide and assess the concepts that were developed during the project's planning and design process.

## **Part 1: Downtown Development Plan**



## Downtown Development Master Plan

### Downtown Concept

The new Downtown Irrigon builds on the existing fabric of what is perceived to be “downtown” today north of US 730 between Division Street and the existing Stoke’s Landing Senior Center. Today it is home to the post office, health clinic, assistance center, senior center, Moose Lodge, City Hall, and Morrow County annex. In the future, Downtown is envisioned to develop stores, offices, public spaces, and community services in this location so that Downtown Irrigon becomes a place to shop, visit, relax, and provide a place for the seeds of civic engagement and community involvement to take root.

The new Downtown is easy to see and access from US 730, yet removed enough to encourage viable downtown development with significantly less auto and truck traffic, making the area safer and more pleasant for pedestrians. The new commercial / retail core is oriented to Columbia Avenue between a new north-south street and Third Street; the 100 percent corner is the intersection of Columbia Avenue and First Street. Columbia Avenue would be considered, for all intents and purposes, the new “Main Street” where adjacent pedestrian-oriented development will be built to the edge of sidewalks that are wide enough to support street trees, stormwater swales, street furniture, and outdoor seating. Parking will be provided on-street or in surface lots situated behind or to the side of development.

New development in the Downtown will likely be one-story high and modestly situated along Columbia Avenue and Main Street to create a tightly-knit, pedestrian-scale, and walkable downtown core. The City already has a zoning and development code in place to support and encourage this type of development.

A central feature of the new Downtown vision includes a publicly-owned Irrigon outdoor market and public plaza. The legacy of the highway fruit stands were consistently mentioned by community stakeholders and residents during the master planning process. Irrigon used to be home to a number of roadside stands that sold a variety of fruits and vegetables - particularly watermelons and onions - during the summer months. Only one roadside fruit stand remains today, but the community feels that there is latent demand for a place where people could bring their home or farm-grown produce and/or goods to sell during the summer. Similar to a covered farmer’s market, the concept would include a city-constructed pavilion-style structure that is serviced with electricity and water, and lease out spaces to community members who have fruit and vegetables to sell. The outdoor market could have multiple functions and provide a forum for other complementary events in the off-season.



*Example photographs of envisioned public investments for Downtown Irrigon*

The proposed plaza and outdoor market would bookend the downtown district with the planned Memorial Plaza in front of City Hall. The plazas should have distinct identities that are realized through their different program elements. Where the Memorial Plaza may be more appropriate for honoring military personnel and introspective reflection, the plaza in front of the outdoor market may be more active and programmed with community events on summer weekends and evenings. The plaza could also include an interactive water feature that provides a space for children and their parents to cool off during the summer, as well as educates visitors about the Columbia River and provides directions to the river and its recreation trails.

Figure 1 illustrates the Downtown Development Plan as a conceptual plan view artistic rendering, showing where and how both new public and private development could potentially develop over time. The approximate building area and their respective parking spaces are displayed on the left side of the graphic for comparison purposes. The graphic plan also illustrates sidewalks, parking areas, street trees and landscaping, trails, and roadway crossing treatments.

It is important to note that the development program for the Downtown Development Plan is based on existing land availability and redevelopment suitability, not current economic growth forecasts. Preliminary market analyses completed in late 2008 indicated that Irrigon could support up to 18,000 additional square feet of commercial development in the next ten years. The Downtown Development Plan graphic shows significantly more development than this for several reasons: (1) this plan is intended to provide a framework for growth to demonstrate where and how development could occur when local economic conditions can support more growth; (2) the planning horizon for this plan is 20 years, during which a number of things could affect the community, including its ability to absorb more growth; and (3) the development shown in the Downtown Development Plan is intended to illustrate both the benefits and costs of growth.

One potential limitation to the Downtown Development Plan is its inability to accommodate *prodigious* growth. The central core is able to absorb more development than what is shown in the graphic plan if existing uses are redeveloped and the district expands slightly to the east and west. There is also substantial opportunity to redevelop existing buildings and sites throughout the new downtown and along US 730 to increase developable area, if necessary.



**CONCEPTUAL DEVELOPMENT FORM**

- ① commercial development (12,800 sf - 48 parking spaces)
- ② commercial development (2,400 sf - 8 parking spaces)
- ③ hotel / motel (25 or 50 rooms - 73 parking spaces)
- ④ commercial development (2,000 sf - 34 parking spaces)
- ⑤ commercial development (16,000 sf - shared parking)
- ⑥ commercial development (1,750 sf - shared parking)
- ⑦ downtown residential (14 units @ 1,500 sf each)
- ⑧ commercial development (12,000 sf - 30 parking spaces)
- ⑨ commercial development (10,000 sf - 16 parking spaces)
- ⑩ new commercial development (5,000 sf - 19 parking spaces shared with Moose Club)
- ⑪ new commercial development (6,000 sf - 24 parking spaces)
- ⑫ redeveloped commercial building (12,000 sf - 80 parking spaces)

**POTENTIAL PUBLIC IMPROVEMENTS**

- A Irrigon outdoor market (5,000 sf)
- B public parking lot (65 parking spaces)
- C columbia plaza (5,600 sf)
- D public library (6,600 sf - parking shared in public lot)
- E public parking lot (39 parking spaces)
- F public building (4,500 sf - parking shared in public lot)
- G memorial plaza (8,800 sf)
- H streetscape improvements



**DEVELOPMENT PROGRAM SUMMARY**

LAND USE	TOTAL SQUARE FOOTAGE
commercial	77,000 sf
residential	21,000 sf
public buildings	16,100 sf
plaza / open space	14,400 sf
<b>total</b>	<b>128,500 sf</b>

This illustrated plan is a conceptual rendering. Proposed street alignments and development footprints are for planning purposes and do not represent final locations or actual development proposals.



**Figure 1. DOWNTOWN DEVELOPMENT PLAN**  
 S E R A Irrigon Downtown Development Plan and US 730 Streetscape Plan  
 JUNE 2009 / updated DECEMBER 2009







## Public Right-of-Way Improvements

The transportation system in Downtown Irrigon will change substantially over the next 20 years to improve connectivity and safety for pedestrians, bicyclists, and motor vehicles. As the area develops over time, Downtown public rights-of-way will be formalized with curbs and sidewalks, paved parking lanes and travel ways, and localized stormwater systems. Figure 2 illustrates the local transportation network in Downtown Irrigon. US 730 is discussed separately in Part 2 of this document.

As shown on the map, there are a number of identified changes to vehicle circulation in Downtown:

- Closing Columbia Avenue to vehicle travel at the Third Street intersection;
- A new public street connection between Columbia Avenue and US 730;
- Local street widening;
- Realignment/extension of Idaho Street between First Street and Seventh Street;
- The First Street / US 730 intersection will require mitigation in the future, either a signal or a roundabout.

### Columbia Avenue Closure at Third Street

The existing Columbia Avenue/Third Street intersection is located too close to the US 730/Third Street intersection. This intersection spacing deficiency is a safety concern as it allows intersection turning movements and vehicle queues to occur within a close proximity of one another. While relatively minimal today, future development along Columbia Avenue can significantly increase traffic demand that will necessitate improvements to the spacing deficiency. To address this deficiency, the Downtown Development Plan calls for Columbia Avenue to be disconnected from Third Street. This would involve developing a vehicular turnaround on Columbia Avenue located just west of Third Street. While vehicular movements between Columbia Avenue and Third would be removed, bicycle and pedestrian movements would still be encouraged through wide sidewalks and other pedestrian scale treatments.

### New Public Street Connection

A new public local street connection has been identified as part of the downtown plan to improve circulation between US 730 and Columbia Avenue west of First Street. This new local street alignment is shown in Figure 2 and would be located approximately 600 feet west of First Street.

### **Local Street Widening**

In order to improve mobility for all modes of transportation within the downtown core, a number of local street widening needs have been identified. These needs are shown in Figure 2 and include the following:

- Widening the Third Street southbound approach at US 730 to provide for a southbound left and right-turn lane.
- Widening First Street from US 730 to Columbia Avenue in order to obtain a 60-foot right-of-way. At US 730, the southbound and northbound approaches will need a separate left and shared through/right-turn lane.
- Widening Columbia Avenue from First Street to Third Street to obtain a 60-foot right-of-way.
- Widening Fourth Street from US 730 to Idaho Avenue to obtain a 60-foot right-of-way.

### **Idaho Street Extension**

Although the focus of the Downtown Development Plan is located north of US 730, it is recognized that there is still an important and viable commercial and residential component to the City located south of US 730. Unlike the north side of the highway, a large portion of the City located south of US 730 lacks a network of east-west local streets that support and provide access to the commercially zoned land along the highway. As such, access is typically provided via individual driveways to US 730. To improve east-west circulation on the south side of the highway and reduce reliance upon US 730 for access and local street travel, the Downtown Development Plan calls for a new Idaho Avenue alignment between First Street and Division Street as well as an extension of Idaho Avenue from Division Street to Seventh Street.

Between First and Fourth Street, a segment of the Idaho Avenue corridor was vacated as part of a historical land use action. As such, this missing segment creates a gap in the Idaho Avenue corridor that limits its use as parallel local street to US 730. In order to reestablish the Idaho Avenue corridor, new right-of-way will need to be acquired in order to fill in this gap. As shown in Figure 2, the Downtown Development Plan has identified a new, more southerly alignment for the missing segment of this new corridor. This realigned segment will create a new intersection with First Street that will ensure sufficient long-term separation from the existing US 730/First Street intersection.

Between Division Street and Seventh Street, the Downtown Development Plan calls for the extension of Idaho Avenue. This extension will require new right-of-way, but when complete, there will be a continuous Idaho Avenue connection between First Street and Fourteenth Street along the south side of US 730.

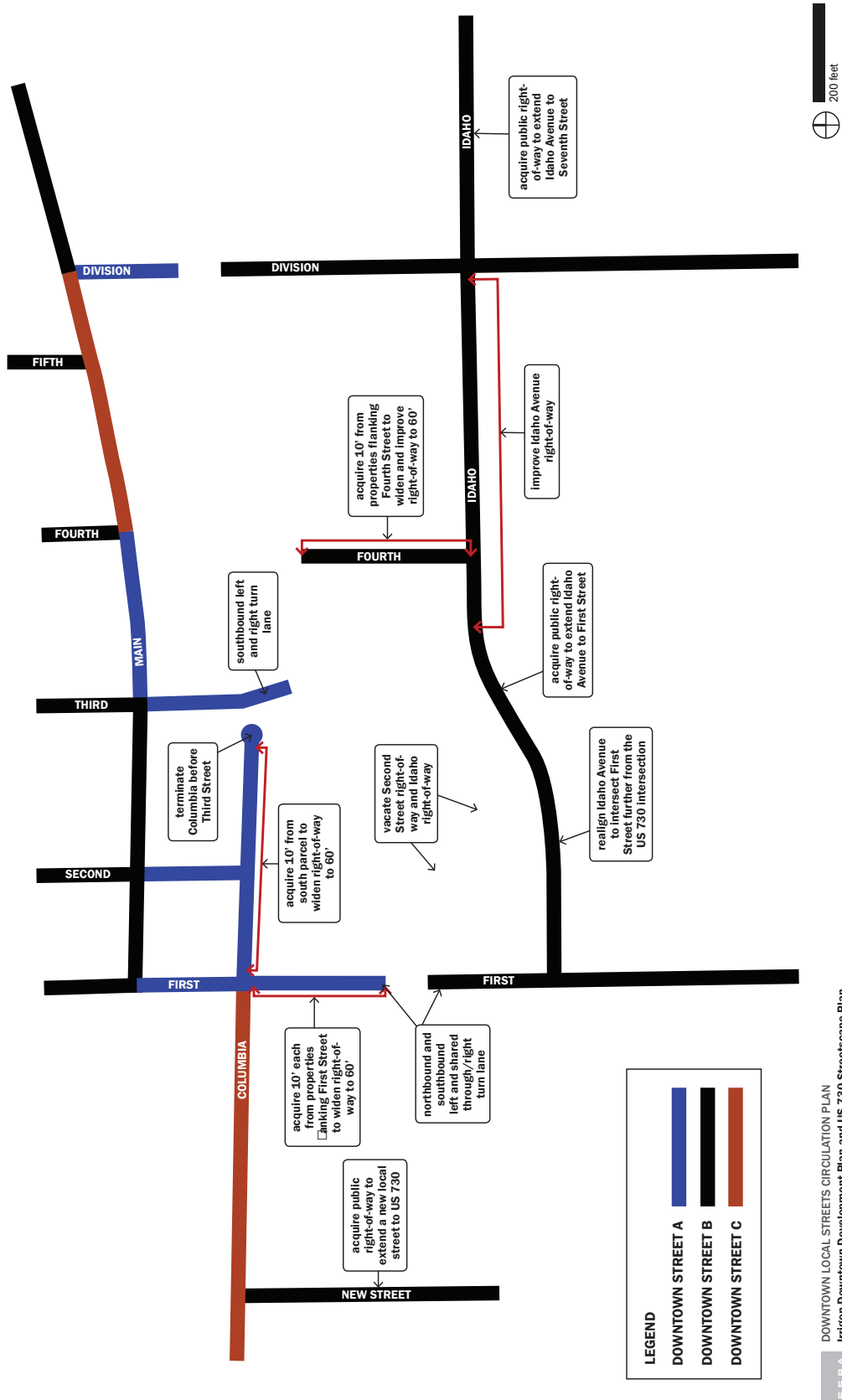


Figure 2. Downtown Local Streets Circulation Plan

## Downtown Streets

All of the existing and proposed public streets in Downtown will be improved to have 5 to 10-foot sidewalks, stormwater swales to capture stormwater runoff, on-street parking on both sides of the street, and a travel lane in each direction. Curb extensions at the intersections will improve pedestrian crossing safety, improve intersection sight distance, and provide ancillary stormwater treatment areas. The bicycle facility on Downtown streets will be a shared bikeway, where bicyclists and vehicles share the travel lane. The speeds and traffic volumes on these streets are typically low enough that separate bicycle facilities are not warranted. The streets included in the Downtown Development Plan are identified in Figure 2.

## Street Cross-Sections

The local streets in Downtown will have a 60-foot or greater public right-of-way. This right-of-way dimension provides adequate space for a comfortable pedestrian realm, on-street parking, and travel lanes. The Downtown street dimensions in this Plan vary slightly from the adopted standards in the City's Transportation System Plan (TSP), which will need to be amended to reflect the changes. Currently, the Local Street standard in the TSP is not appropriate for a downtown setting. As such, four new Downtown street standards for the Downtown core are proposed as part of this Plan.

### Downtown Street A

Downtown Street A (Figure 5, following page) will have a 60-foot public right-of-way and include two 12-foot travel lanes, an 8-foot parallel parking lane with a mid-block stormwater swale on either side of the street, and two 10-foot sidewalks. The sidewalk zones on Downtown Street A streets will have a 6-inch curb, 2-foot paver band for buried utilities with intermittent 4-foot by

*Sand set pavers are proposed for a 24-inch utility strip from the back of curb and in the tree wells. Pavers, while initially more expensive (primarily due to labor costs), are attractive, long-lasting, easy to replace and repair, and come in a variety of shapes, sizes, and colors for unlimited design opportunities.*

*They help preserve the streetscape investment by providing easy access to utilities and by allowing trees to grow without cracking or heaving the concrete sidewalk. For adjacent property owners (who will be responsible for long-term maintenance), they are relatively easy and inexpensive to repair.*

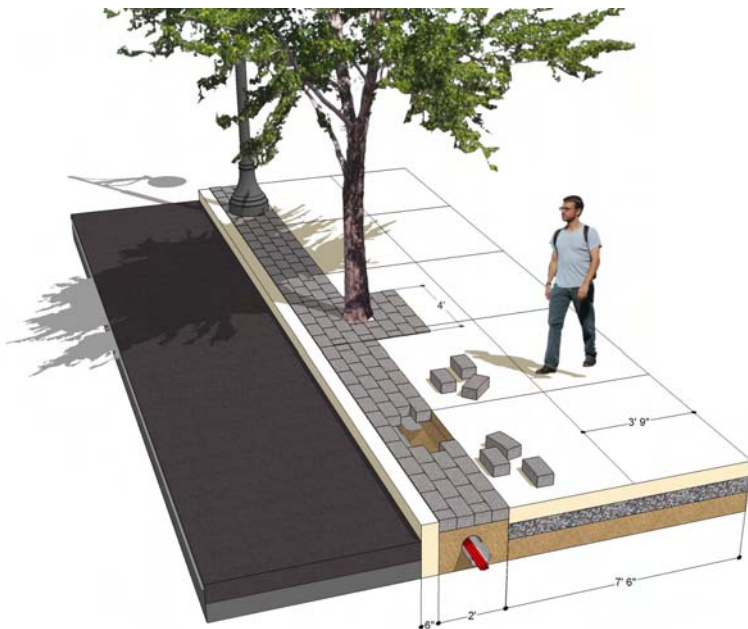


Figure 3. Downtown Street A sidewalk dimensions

4-foot tree wells, and a roughly seven foot pedestrian zone for walking (Figure 3). Street trees should be placed between 25 feet and 30 feet on center, unless the street tree conflicts with an existing tree, retaining wall, utility or similar physical barrier.

### ***Downtown Street B***

Downtown Street B (Figure 6) is similar to the existing standard in the City's TSP. It will have a 60-foot public right-of-way and include two 12-foot travel lanes, an 8-foot parallel parking lane on either side of the street, and an 11-foot pedestrian zone that includes a five foot stormwater swale and a five foot sidewalk (Figure 4). A one-foot curb provides a narrow disembarking area for vehicle passengers. As a general guideline, 18-inch cuts in the curb should occur approximately every 16 to 18 feet to allow stormwater to flow into the swale during storm events. This recommendation should be confirmed or adjusted by a registered civil engineer, landscape architect, or stormwater specialist during future design phases.

### ***Downtown Street C***

Columbia Avenue is the only Downtown street with a 70-foot public right-of-way. This condition allows angle parking to be established on one side of the street; parallel parking will be on the other side of the street (Figure 7). Mid-block stormwater swales will capture and treat stormwater runoff.



*Figure 4. Downtown Street B sidewalk dimensions*



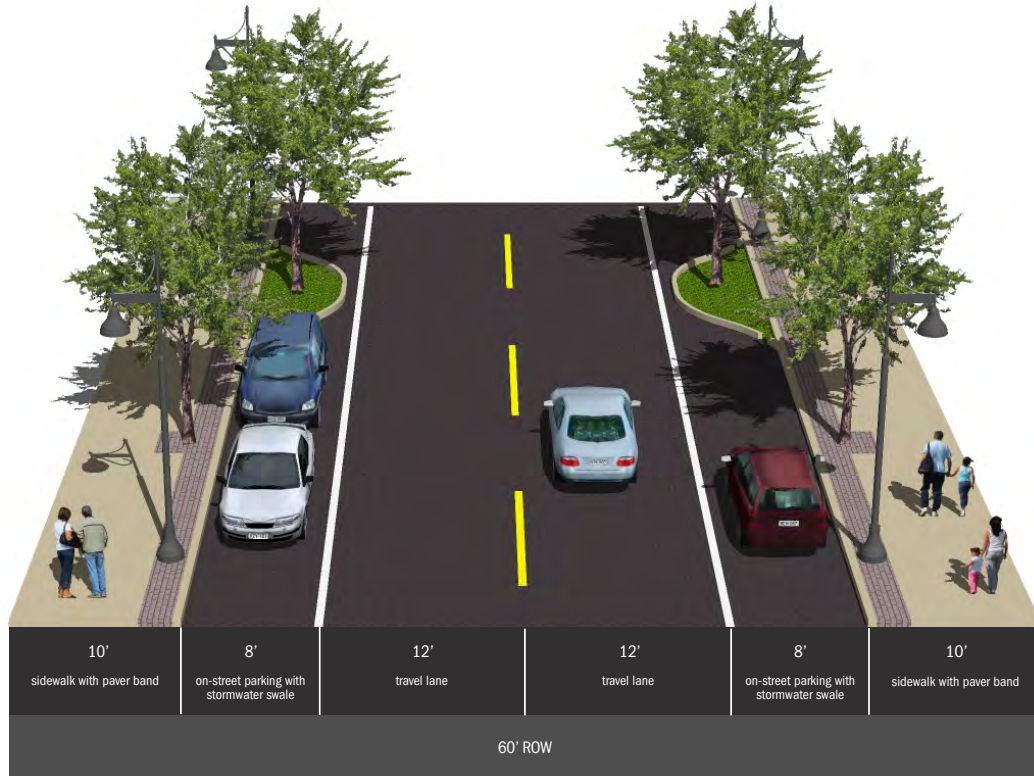


Figure 5. Downtown Street A

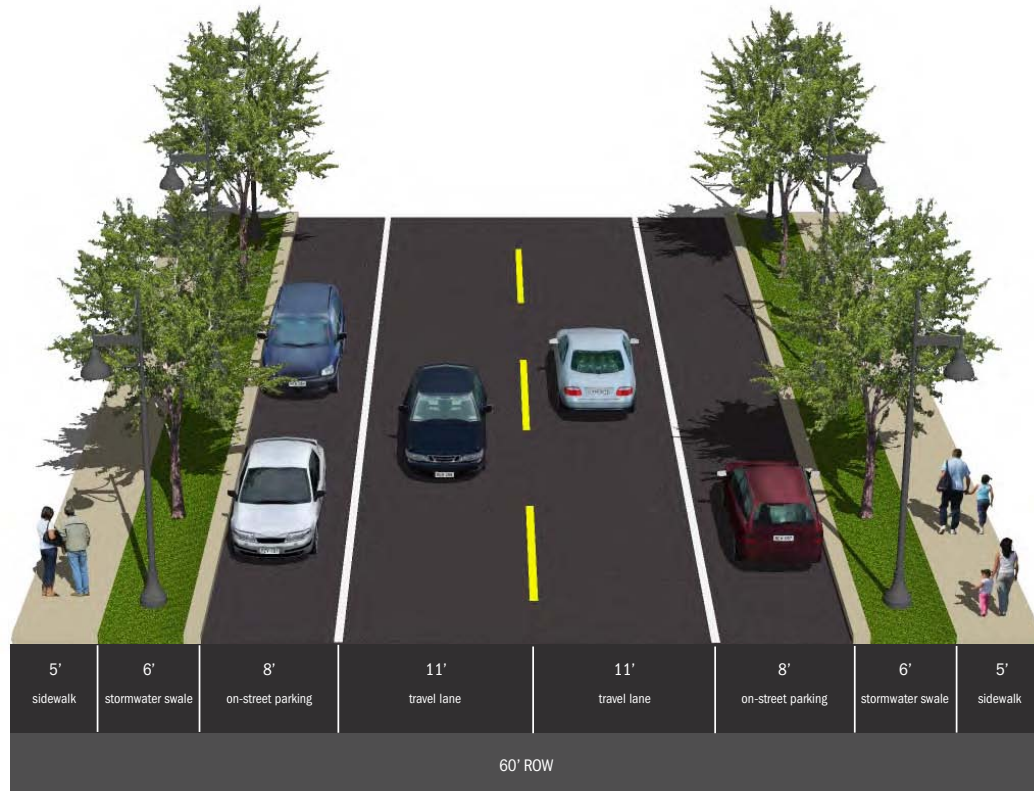


Figure 6. Downtown Street B



Figure 7. Downtown Street C

## Stormwater

The City of Irrigon currently has no piped stormwater system in the Downtown. Stormwater swales were identified as the stormwater facility of choice in the city's Transportation System Plan. As such, future stormwater will be captured and treated locally in stormwater swales - either in linear swales adjacent to the sidewalk (Figure 8), in mid-block detention swales (Figure 9), or in stormwater swales in the curb extensions (Figure 10).

The size and depth of the proposed stormwater facilities will be determined by site soil conditions and percolation rate, frequency and duration of storm events, and slope. A registered landscape architect or civil engineer should analyze the local conditions to determine the design standards for such facilities. A qualified landscape architect will be able to determine an appropriate palette of soil mix, plants, and trees for the stormwater swales which should be used consistently throughout Downtown.

### How Stormwater Swales Work

Stormwater swales are lowered areas of land planted with special grasses, sedges, and trees that collect and convey stormwater runoff from roads, driveways, parking lots, and other impervious surfaces. Stormwater swales slow the runoff water and allows it to temporarily pond. This reduces the water's speed and allows the vegetation to filter and remove sediments, heavy metals, and hydrocarbons, such as oil and grease, from the runoff. Ponding in the swale allows the water to soak into the soil, subsequently reducing the volume and amount of pollutants that flow directly into the Columbia River and other water courses.

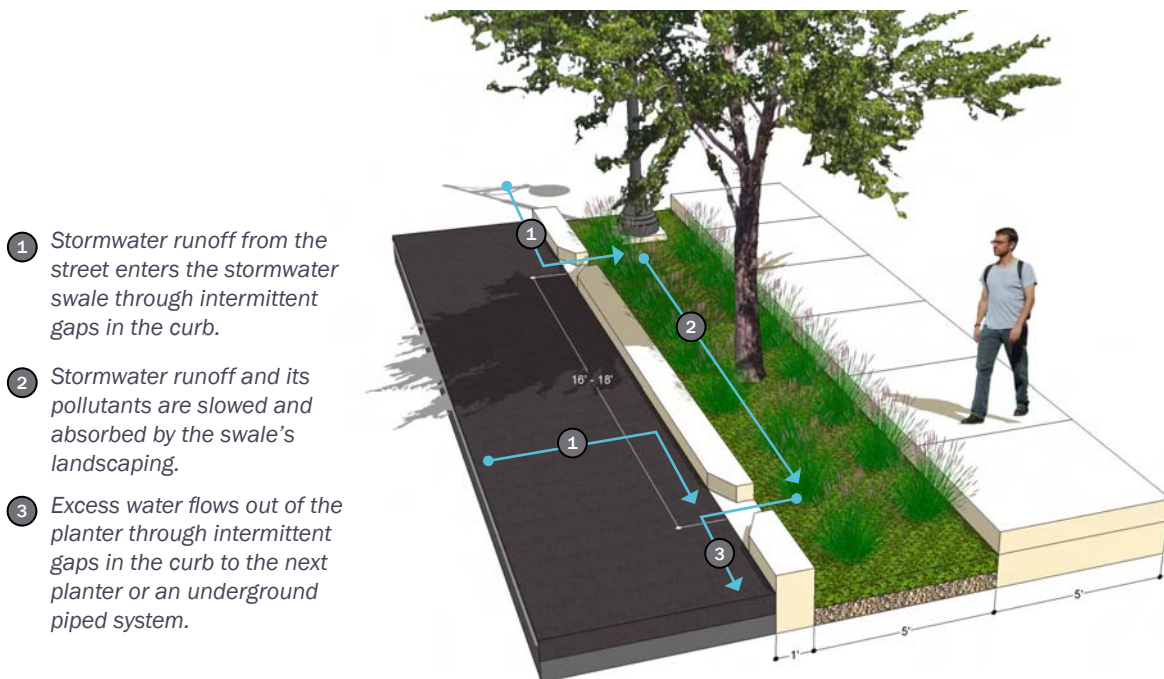


Figure 8. Linear stormwater swale



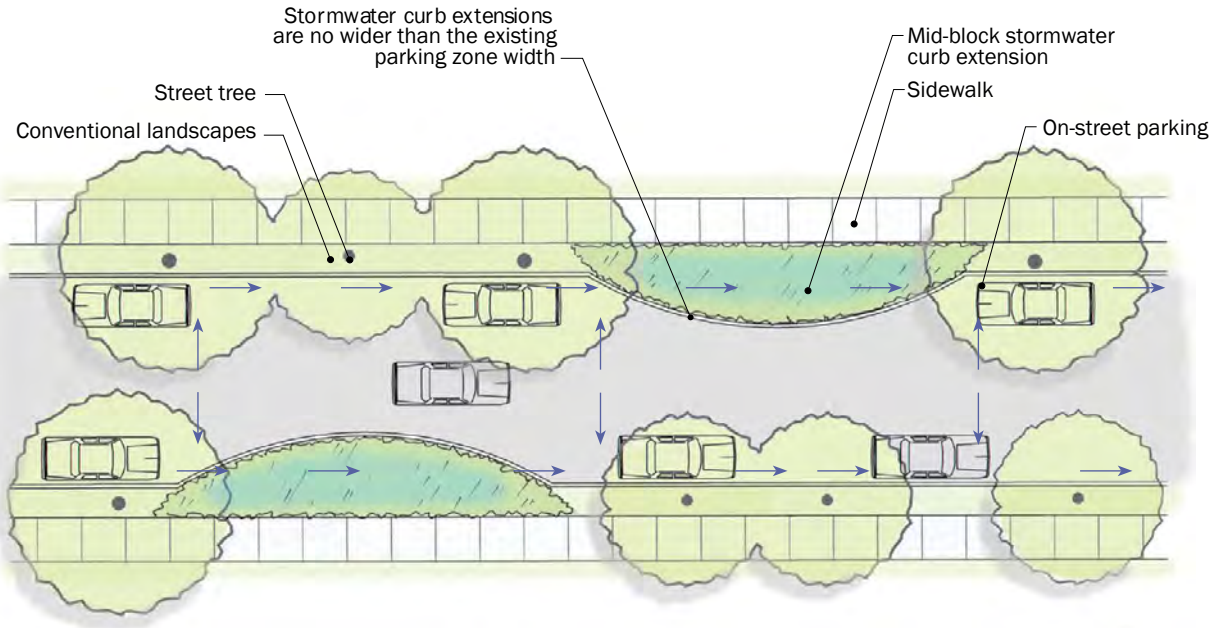
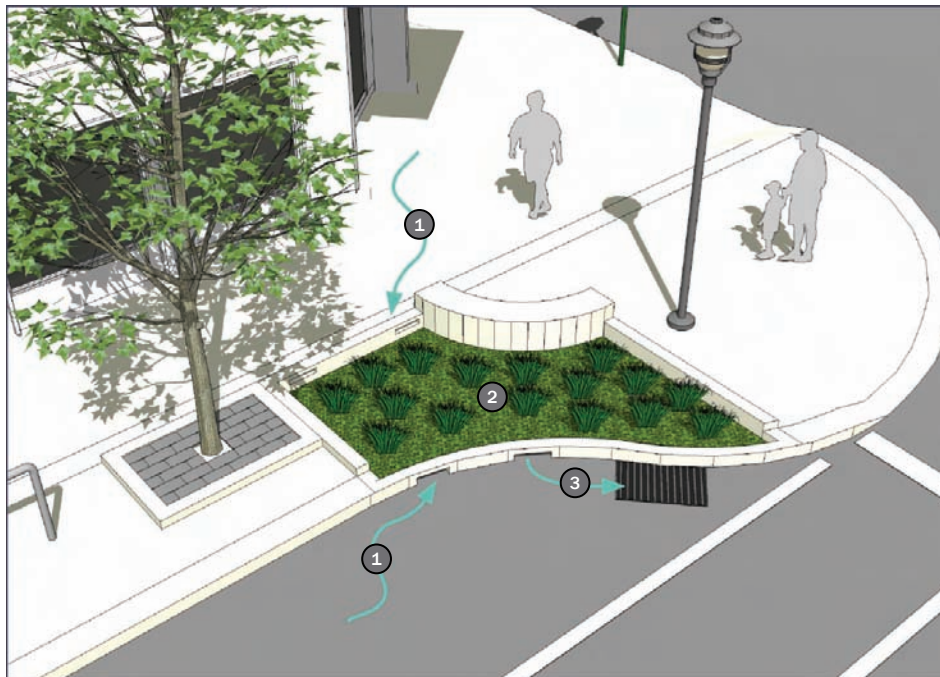
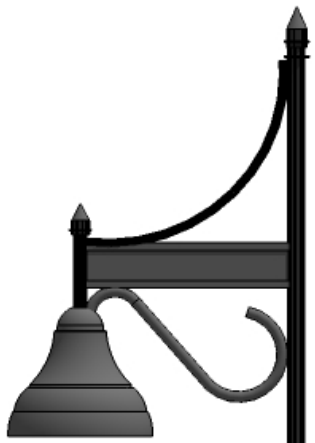


Figure 9. Mid-block stormwater swale  
 Source: San Mateo County Sustainable Green Streets and Parking Lots Guidebook



- 1 Stormwater runoff from the street and sidewalk enters the stormwater swale through intermittent gaps in the curb.
- 2 Stormwater runoff and its pollutants are detained and absorbed by the swale's landscaping.
- 3 Excess water flows out of the planter through intermittent gaps in the curb to the next planter or a piped overflow system.

Figure 10. Curb extension stormwater swale



*Kim Lighting, Era Bell*

## Pedestrian Amenities

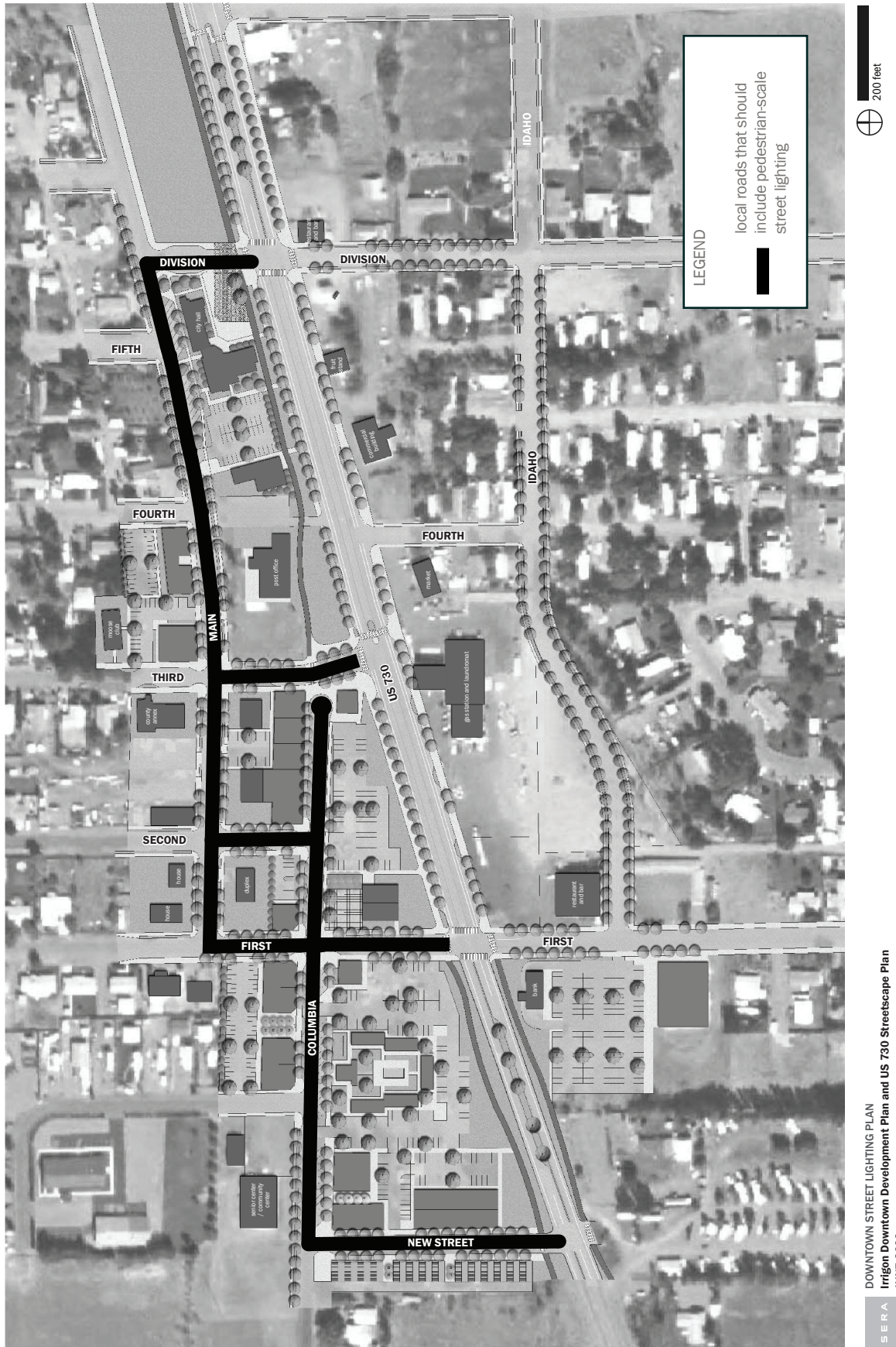
### Pedestrian-Scale Lighting

The City currently uses the Era Bell streetlight from Kim Lighting, which has been used in the landscaping around the new City Hall. The same street light should be used throughout Downtown to provide a cohesive and distinct identity. Streetlight poles should be placed at the intersection corners and - depending on design parameters - spaced between 50 and 60 feet on center in the sidewalk furnishing zone.

Figure 11 shows where electrical conduit and the Era Bell streetlight should be installed throughout Downtown as funding becomes available or adjacent properties redevelop.







**SERA**  
 DOWNTOWN STREET LIGHTING PLAN  
 Irrigon Downtown Development Plan and US 730 Streetscape Plan  
 JUNE 2009

Figure 11. Downtown Street Lighting Plan

### Street Furniture

The street furniture below was chosen to complement the character of the street light, as well as be durable, easy to care for, and long-lasting. Maglin furnishings are aluminum and steel construction and come in a variety of styles and colors.

On the Downtown Street A and Downtown Street C, at least two benches will be placed on each block face in the sidewalk furnishing zone (the first 36 inches from the curb). Benches should face the building - not the street - and provide adequate clearance from other fixed objects, like trees, trash receptacles, and bicycle racks.

### Bicycle Parking

New bicycle racks should be a simple staple design in black, though there are opportunities to incorporate art racks into the streetscape as well. Bike racks installed within the right-of-way are to be installed in the furnishing zone, leaving a minimum 6-foot wide pedestrian way on the sidewalk. Placement of bike racks should be coordinated with business owners to best accommodate entrances, outdoor cafe seating, and/or loading.

*Palmer Group Welle  
Flat Top Bike Rack*



*Maglin, MLB300 Series*



*Maglin, MLWR250 Series*



## Banners

Banners can advertise community events and celebrations, beautify the streetscape and strengthen the community's identity. Streetlight poles must be strong enough to accommodate hanging banners. Banners should be designed to allow air to blow through them so that poles and fixtures are not damaged during high winds. Also, the banners must be placed high enough on the streetlight pole so that they are not damaged by large vehicles (i.e., RVs) in the parking zone. The City may explore options for installation and/or maintenance agreements for these elements with area businesses, property owners, or associations.

## Street Trees and Landscaping

Street trees along Downtown streets should be climate-appropriate species that require little or no irrigation and minimal maintenance. The City will use the following guidelines and recommendations for landscaping in the swales and tree wells throughout Downtown Irrigon:

- Select trees that are well adapted to the local environment, considering soil, wind, sun exposure, and exhaust. Drought-resistant trees should be used in areas with sandy or rocky soil.
- Use deciduous trees for summer shade and winter sun. Drought tolerant and native plant materials are encouraged;
- Provide a broad canopy tree variety unless limited by overhead clearance.
- Select trees that can be “limbed-up” where vision clearance is a concern.
- Use narrow or “columnar” trees where awnings, other building features, or narrow sidewalks limit growth, or where greater visibility is desired between buildings and the street.
- Avoid using trees that are susceptible to insect damage, and avoid using trees that produce excessive seeds or fruit.
- Select trees for their seasonal color, as desired.
- Where possible, landscaping should be designed to effectively enhance existing views or provide new view corridor opportunities. Consideration should be given as to whether or not a single tree species or palette of species should be designated for use in streetscape improvements as an effort to present a unified and coordinated appearance.



*Banners are a relatively inexpensive way to advertise community events, beautify the streetscape, and strengthen the community's identity.*

### Downtown Development Summary

#### Development and Zoning Code Modifications

*The majority of Downtown was recently rezoned to C-1 (Downtown Commercial) and has strong development standards in place to facilitate pedestrian-friendly development. No additional modifications to the Irrigon Development Code are necessary.*

It is important to note that the development program for the Downtown Development Plan is based on existing land availability and redevelopment suitability, not current economic growth forecasts. Preliminary market analyses completed in late 2008 indicated that Irrigon could support up to 18,000 additional square feet of commercial development in the next ten years. The Downtown Development Plan shows significantly more development than this for several reasons: (1) this plan is intended to provide a framework for growth to demonstrate where and how development could occur when local economic conditions can support more growth; (2) the planning horizon for this plan is 20 years, during which a number of things could affect the community, including its ability to absorb more growth; and (3) the development shown in the Downtown Development Plan is intended to illustrate both the benefits and costs of growth.

#### Key Redevelopment Sites

There are a number of redevelopment sites in the Downtown core and adjacent areas (Figure 12). The key redevelopment sites in the Downtown core are shown in orange on the map below; redevelopment sites in the adjacent areas are shown in yellow.

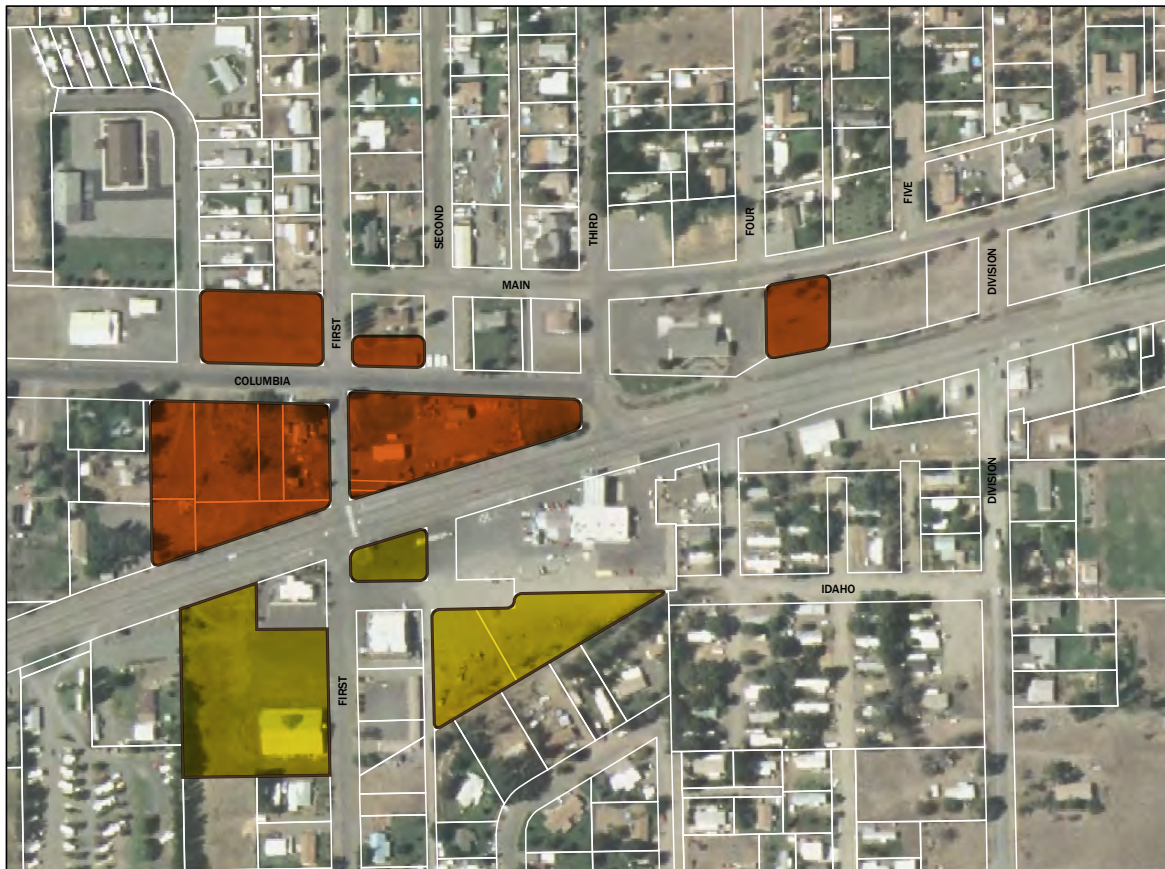


Figure 12. Key redevelopment sites in the Downtown Core and adjacent areas

### Private Downtown Development

Approximately 77,000 sf of new commercial development is oriented to Columbia Avenue and Main in modestly-sized new buildings (between 1,700 sf and 16,000 sf). The existing 12,000 sf vacant grocery store is proposed to redevelop as either another grocery store or some other commercial use, but has not been included in the total commercial square footage because the building is not changing its footprint. Approximately 21,000 sf of new residential (14 units) is proposed at the west end of Downtown to provide a transition between existing residential areas and the new commercial core.

### Public Downtown Development

Three new public buildings are proposed as part of the Downtown Plan, including the already-designed and partially-funded public library (6,600 sf), a new 4,500 sf public building that could share resources with the city hall complex, and a new 5,000 sf pavilion that would serve as the community's year-round outdoor market and covered public space - the Irrigon Public Market. The Irrigon outdoor market is shown on the southeast corner of Columbia Avenue and First. While this lot is not currently publicly owned, it is - from an urban design perspective - a highly-desirable location for a key component of Irrigon's downtown development. The location helps activate the 100 percent corner, has excellent visual access from the highway (to encourage people passing through to stop), and reinforces the rhythm of public development on the north side of US 730, one that is slightly set back from the highway with landscaping and open space in front of the building.

### Plazas and Open Spaces

Other non-streetscape related public investments include a new 65-space public parking lot between First and Third to serve the Irrigon Public Market, a 5,600 sf plaza at the corner of First and Columbia Avenue, an 8,800 sf memorial plaza in front of city hall and the library, and a 39-space public parking lot behind the library to serve the institutional complex.

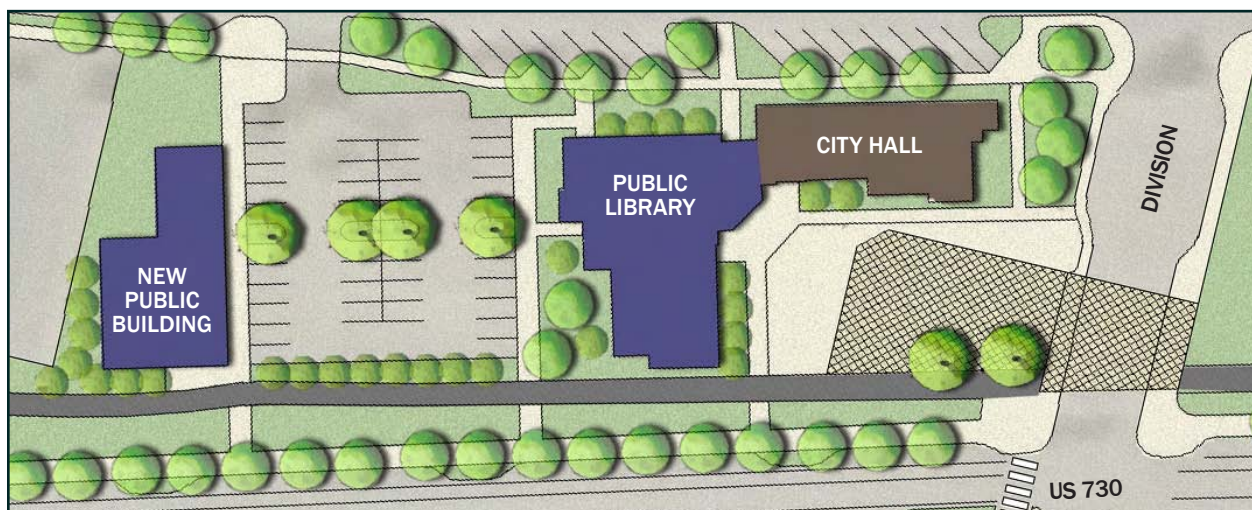


Figure 13. Civic Complex in Downtown Irrigon



A new public plaza and outdoor market is important to the new Downtown Irrigon for several reasons.

One, it anchors the 100% corner of the new Downtown.

Two, the plaza and outdoor market can be developed independently of the private real estate market, which will help set the quality standard for the area and potentially stimulate additional development in the area.

And three, the plaza and market serves as a bookend to the plaza in front of the City Hall and future library, helping communicate the new Downtown boundaries.

- ① special intersection paving
- ② public plaza
- ③ public outdoor market structure

Figure 14. Conceptual rendering of the Irrigon Outdoor Market and Plaza





## Project Costs

Because the Downtown and its related infrastructure currently does not exist, there are substantial costs associated with developing the Downtown core. All of the infrastructure must be built from the ground up, which is costly due to the amount of design expertise, labor and materials needed. That aside, the lack of infrastructure provides an advantage that many other communities do not have: the City can strategically plan and phase improvements to “build it right the first time” without the burdens of limiting existing conditions.

Generally speaking, there are costs associated with bringing the streets up to standard with sidewalks, curbs, parking lanes and stormwater swales, installing lighting and landscaping, acquiring land or easements for new streets and roadway widening, and developing buildings. Due to the conceptual nature of the Downtown Development Plan and the scope of the area, planning-level costs have been provided for three public infrastructure projects: Columbia Avenue from the new north-south street to the circle, North Main Street from First Avenue to Division Street, and all of the north-south streets in the Downtown district north of US 730.

The following tables show planning level cost estimates in 2009 dollars. A 30% contingency has been included in the construction cost estimates to give flexibility for unknown factors that may come up during preliminary and final design. These estimates include engineering design fees, but do not include right-of-way costs, potential environmental permitting or utility relocation costs. Costs will need to be refined before programming into the Capital Improvement Plan, the Statewide Transportation Improvement Plan, and/or being submitted to competitive grant programs (i.e., Transportation Enhancements Program).

<b>Columbia Avenue: 2,000 linear feet</b>	
Demolition, mobilization and traffic control (20%)	\$129,452
Sidewalks	\$80,475
Pavers	\$75,600
Curb extensions	\$18,000
Curbs	\$30,765
Linear swale	\$4,215
Street trees	\$29,000
Stormwater swale landscaping and prep	\$69,600
Asphalt	\$82,040
Mid-Block Swales	\$15,400
Lighting	\$107,678
Benches	\$24,612
Bicycle racks	\$2,000
Sub-Total	\$647,261
Design and Development (15%)	\$97,089
Construction Management (10%)	\$64,726
Contingency (30%)	\$194,178
Total*	\$1,003,255

*2009 dollars - no escalation has been factored into the estimate*

<b>Main Street: 1,700 linear feet</b>	
Demolition, mobilization and traffic control (20%)	\$86,443
Sidewalks	\$47,350
Pavers	\$8,700
Curb extensions	\$7,500
Curbs	\$25,800
Linear swale	\$21,450
Street trees	\$25,000
Stormwater swale landscaping and prep	\$60,000
Asphalt	\$68,800
Mid-Block Swales	\$1,400
Lighting	\$90,300
Benches	\$3,480
Bicycle racks	\$400
Sub-Total	\$432,216
Design and Development (15%)	\$64,832
Construction Management (10%)	\$43,222
Contingency (30%)	\$129,665
Total*	\$669,935

<b>North-South Streets: 2,600 linear feet</b>	
Demolition, mobilization and traffic control (20%)	\$130,414
Sidewalks	\$80,920
Pavers	\$31,290
Curb extensions	\$-
Curbs	\$39,165
Linear swale	\$23,520
Street trees	\$31,000
Stormwater swale landscaping and prep	\$74,400
Asphalt	\$98,640
Mid-Block Swales	\$15,400
Lighting	\$137,078
Benches	\$14,016
Bicycle racks	\$400
Sub-Total	\$652,069
Design and Development (15%)	\$97,810
Construction Management (10%)	\$65,207
Contingency (30%)	\$195,621
Total*	\$1,010,706

2009 dollars - no escalation has been factored into the estimate

## Phasing

Project phasing is important in order to prioritize improvements, leverage and maximize available funding, and establish a clear vision so that projects in the Downtown are competitive for future funding opportunities. Plan improvements are likely to be made as changes in land use occur, as properties redevelop and as sufficient funds are available to construct improvements in Downtown and along US 730.

As logical next steps, the City will implement the following in the near and mid-term:

- Adopt the Downtown Development Plan and US 730 Streetscape Plan and ensure that new development in the Downtown is consistent with the Plan's goals, objectives, and core concepts.
- Use the Plan to guide public frontage improvements when private development occurs in the Downtown and along US 730.
- Identify and seek funding to implement public catalyst projects in the Downtown and along US 730 to help stimulate private investment in the area. Key catalyst projects include:
  - *Outdoor Market and public plaza*
  - *Columbia Avenue streetscape*
  - *Third Street streetscape*
  - *Third/US 730/Columbia intersection realignment, closure, and intersection improvements*
  - *Public library and Memorial Plaza*
  - *Relocate existing recycling center to the west of Downtown*
  - *Develop gateway treatments in the vicinity of First and Tenth along US 730*
  - *US 730 streetscape between First and Division*
- Pursue the recommendations identified in the 2009 Oregon Main Street Program's report.

In the mid- to long-term, the City will implement the following projects to further develop Downtown Irrigon and US 730:

- Construct the Idaho Avenue extension from First to Division.
- Seek funding to design and construct the US 730 streetscape from Second to First and Division to Fourteenth.



## **Part 2: US 730 Streetscape Plan**



## US 730 STREETScape PLAN

The transportation infrastructure within the City of Irrigon primarily centers on US 730, an east-west state highway that bisects the city into northern and southern halves. As a three-lane facility characterized by a wide cross section, wide shoulders, and no bicycle lanes or sidewalks, the planning focus has always centered on the efficient accommodation of through traffic. However, as Irrigon grows and a downtown begins to emerge on the north side of the highway, it will be important to identify and plan for those improvements that will help balance the mobility, accessibility, and safety needs of the community with the accommodation of efficient highway travel.

US 730 from Fourteenth Street to Second Street is envisioned to have a number of characteristics that help to distinguish its purpose as something more than the main east-west travel corridor for regional through traffic. The future US 730 is a multi-modal travel corridor with sidewalks, shoulder lanes that can be used for bicycling, on-street parking, pedestrian crossing treatments, medians, and distinguishing gateway treatments. It should be noted that the corridor streetscape vision is consistent with ORS 366.215 and the needs of Oregon's Freight Commission as documented in the Appendix.

### Streetscape Plan

The US 730 Streetscape Plan is broken into four distinct segments as illustrated in Figures 15 and 16. These segments are a reflection on the need to provide different levels of streetscape amenities in relation to existing right-of-way, land use, and future downtown development.

#### Second Street to First Street

As shown in Figure 15, the Second Street to First Street corridor is designed to be a "gateway" corridor through the inclusion of a raised center median. The median, is envisioned to be landscaped with street trees or other pockets of vegetation that will help reinforce the transitioning role of US 730 through the more urbanized segments of Irrigon. From an access standpoint, left-turn lanes are to be provided at Second Street, a new local street connection to US 730, and First Street. However all other public and private driveways will be limited to right-in/right-out movements. Separated recreational paths will provide multi-modal travel opportunities on both sides of the highway. To maintain at least 21 feet of freight carrying capacity, the 12-foot travel lane is bounded by a 3-foot shy distance (made up of textured concrete) to the median and a 6-foot shoulder lane. All of the streetscape amenities can be provided within the existing 100-foot right-of-way.

The estimated cost to improve this section of US 730 as envisioned is \$915,000.

#### First Street to Third Street

The First Street to Third Street corridor is characterized by a center left-turn lane, a shoulder lane that can be used for bicycling, and on-street parking located along both the north and south sides of the highway. The on-street parking is provided to complement the adjacent commercial and civic use land uses. It is envisioned that the on-street parking would include textured concrete to visually narrow the streetscape though this segment. Stormwater swale curb extensions are placed periodically within the on-street parking lane to serve as both a stormwater facility and provide an element of traffic calming. A 15-foot shared sidewalk/trail is provided along the north side of the highway while a 10-foot sidewalk is provided along the south side of the highway. All of the existing streetscape amenities can be provided within the existing 100-foot right-of-way.

The estimated cost to improve this section of US 730 as envisioned is \$695,000.

### Third Street to Division Street

The Third Street to Division Street travel corridor is similar to the First Street to Third Street segment; however the sidewalk on the north side of the highway is reduced to a 10-foot sidewalk. The sidewalk width reduction is reflective of the existing parallel recreational trail that runs adjacent to the highway. Based on existing right-of-way constraints, the 10 feet for the sidewalk on the north side of the highway would need to be dedicated by the City of Irrigon. A crosswalk and raised refuge island at Third Street would provide for pedestrian crossing safety. The estimated cost to improve this section of US 730 as envisioned is \$861,000

### Division Street to Tenth Street

Division Street to Tenth Street is bordered by City owned park land to the north and residential parcels to the south. The travel corridor design shown in Figure 15 is reflective of this less intensive land use character. In particular, the sidewalk width on the north side of the highway would be reduced to 8 feet to accommodate existing right-of-way constraints. For access management purposes, a raised landscaped median would be constructed between Division Street and Seventh Street. A second raised landscaped median would be constructed between Eighth Street and Tenth Street. To maintain at least 21 feet of freight carrying capacity, the 12-foot travel lane is bounded by a 3-foot shy distance (made up of textured concrete) to the median, a 6-foot shoulder lane that could be used for bicycling, and on-street parking (consisting of textured concrete). A center turn lane would be provided between the two median segments to maintain accessibility to the Seventh Street and Eighth Street corridors. Crosswalks (with a refuge provided in the raised median) would be provided at the Seventh Street, Eighth Street, and Tenth Street intersections for pedestrian crossing safety. The estimated cost to improve this section of US 730 as envisioned is \$1,315,000.

### Tenth Street to Thirteenth Street

As shown in Figure 16, the Tenth Street to Thirteenth Street corridor is bordered by an elementary school to the north and residential/church uses to the south. To continue to provide adequate accessibility to these land uses, a center turn lane would be provided. To facilitate pedestrian crossings along this segment of US 730, crosswalks (with a short median refuge island) would be provided at the Tenth, Twelfth, and Thirteenth Street intersections. The estimated cost to improve this section of US 730 as envisioned is \$615,000.

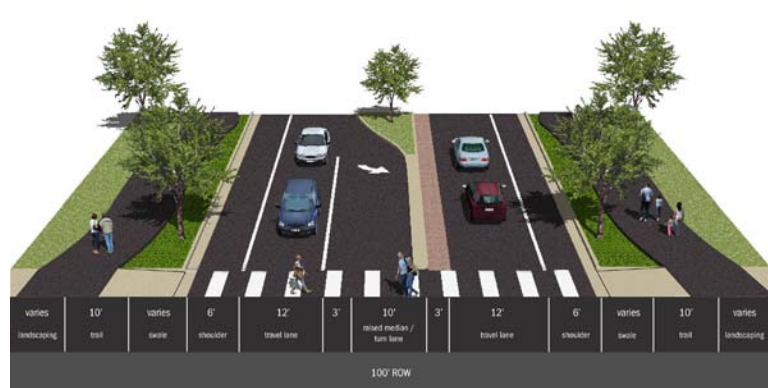
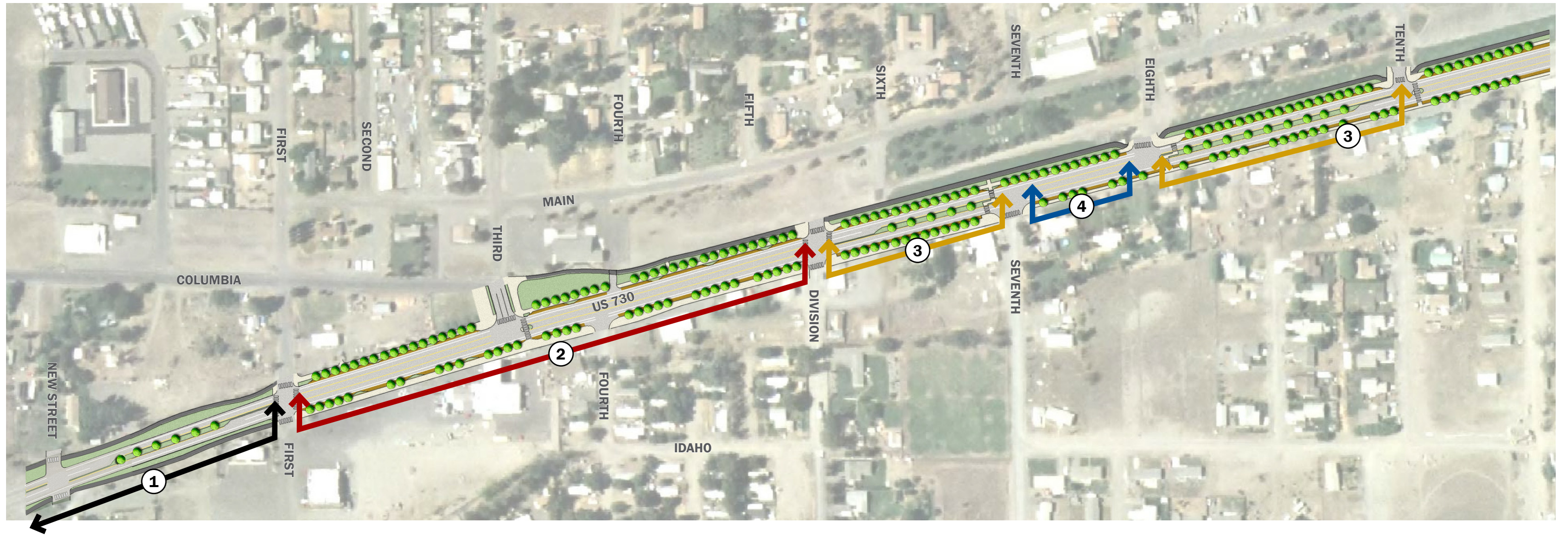
### Thirteenth Street to Fourteenth Street

As shown in Figure 16, the Thirteenth Street to Fourteenth Street corridor is designed to be the complementary “gateway” corridor on the east end of town through the inclusion of a raised center median. On-street parking would be provided only along the north and south sides of the highway with an adjacent curb-tight sidewalk. The estimated cost to improve this section of US 730 as envisioned is \$1,115,000.

#### *US 730 Cost Estimate Summary*

<b>Highway Segment</b>	<b>Estimated Cost</b>
Second Street to First Street	\$915,000
First Street to Third Street	\$695,000
Third Street to Division Street	\$861,000
Division Street to Tenth Street	\$1,315,000
Tenth Street to Thirteenth Street	\$615,000
Thirteenth Street to Fourteenth Street	\$1,115,000
<i>All Segments - Total</i>	<i>\$5,516,000</i>

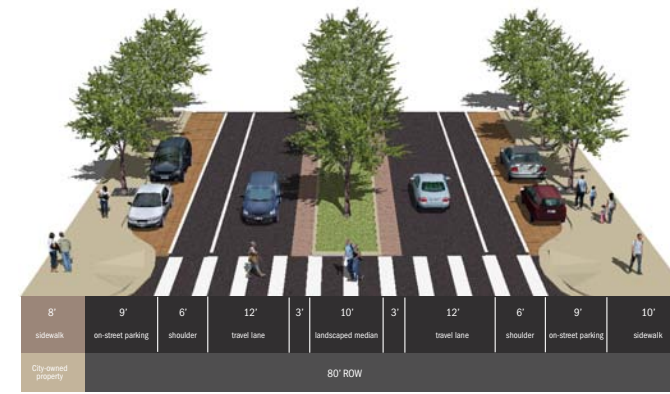




**SEGMENT**  
**1**



**SEGMENT**  
**2**



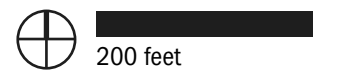
**SEGMENT**  
**3**



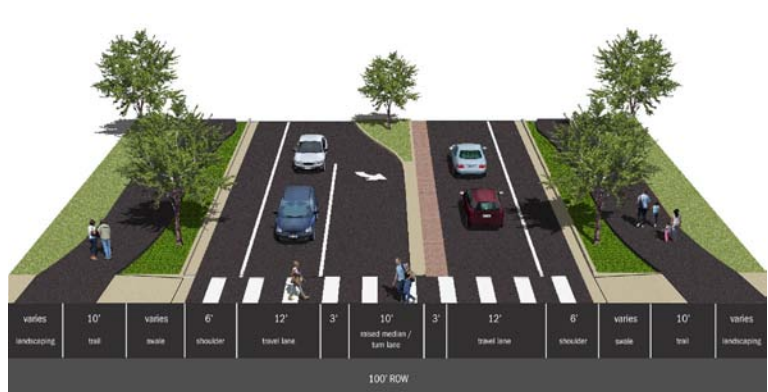
**SEGMENT**  
**4**

**Figure 15. US 730 STREETSCAPE PLAN: SECOND STREET TO TENTH STREET**  
Irrigon Downtown Development Plan and US 730 Streetscape Plan  
DECEMBER 2009

This illustrated plan is a conceptual rendering. Proposed street alignments and development footprints are for planning purposes and do not represent final locations or actual development proposals.



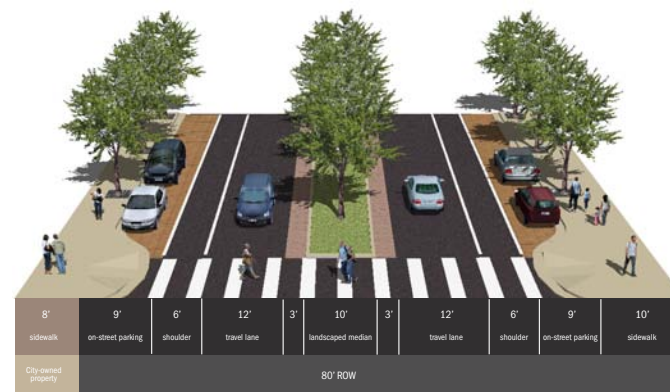




**SEGMENT**  
**1**



**SEGMENT**  
**2**



**SEGMENT**  
**3**



**SEGMENT**  
**4**

**Figure 16. US 730 STREETSCAPE PLAN: TENTH STREET TO FOURTEENTH STREET**  
Irrigon Downtown Development Plan and US 730 Streetscape Plan  
DECEMBER 2009

This illustrated plan is a conceptual rendering. Proposed street alignments and development footprints are for planning purposes and do not represent final locations or actual development proposals.





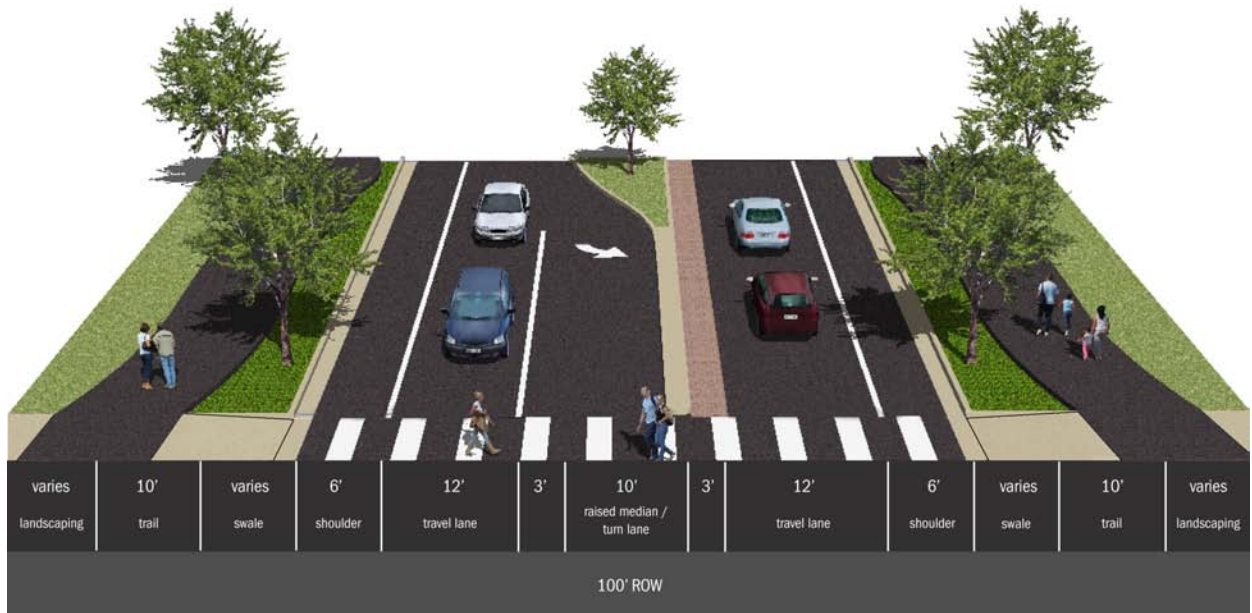


Figure 17. US 730 Cross-Section Detail for Segment 1

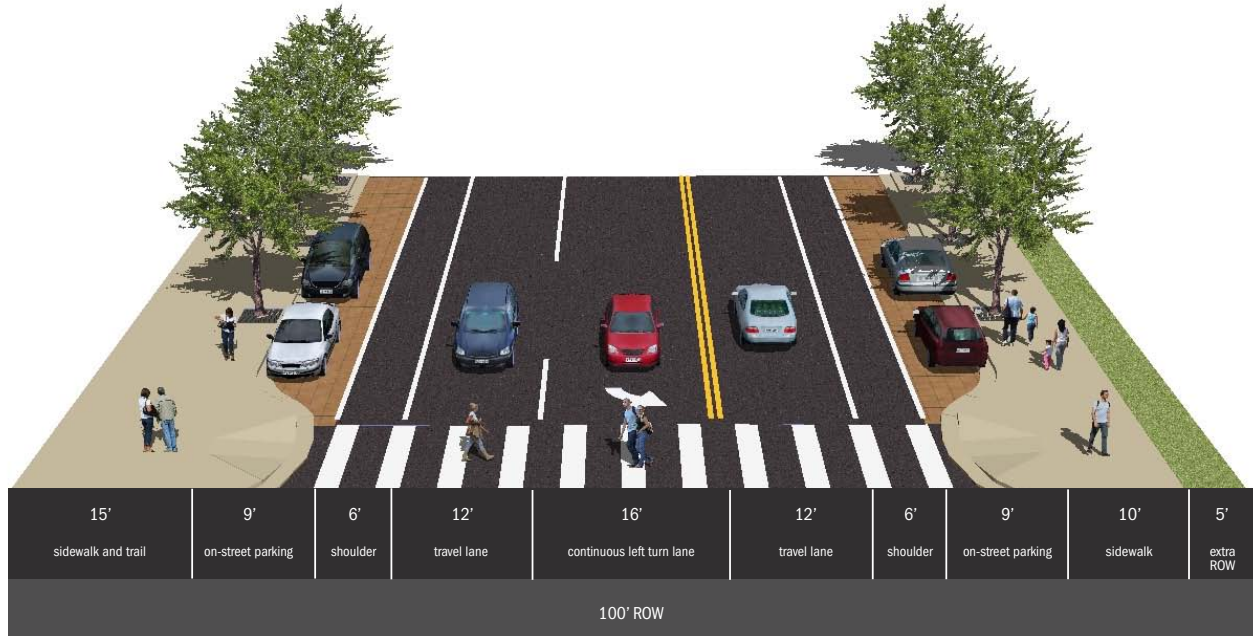


Figure 18. US 730 Cross-Section Detail for Segment 2

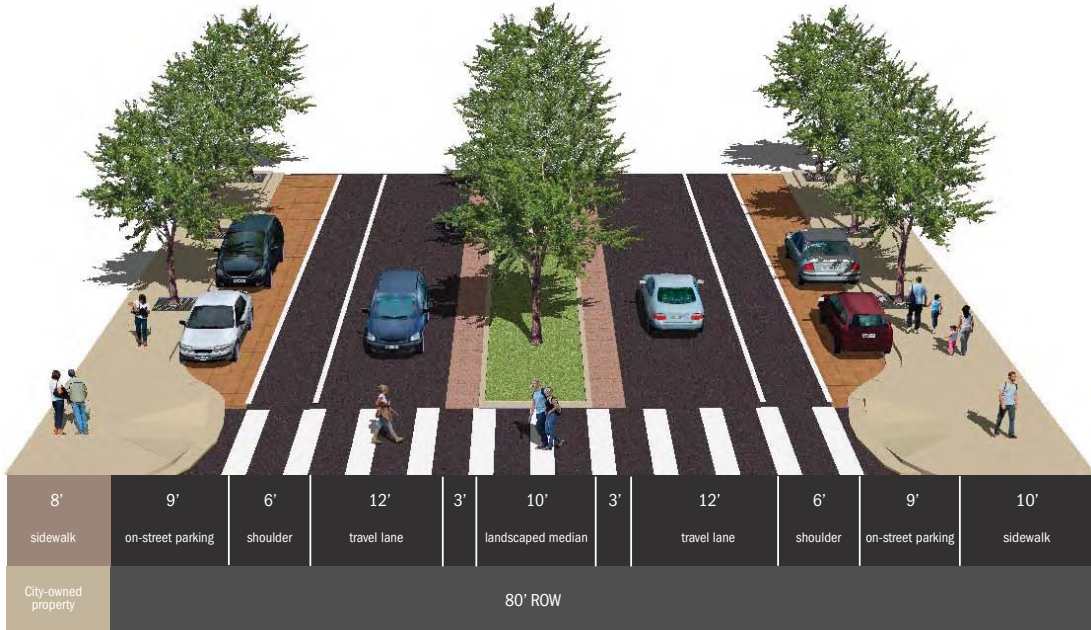


Figure 19. US 730 Cross-Section Detail for Segment 3

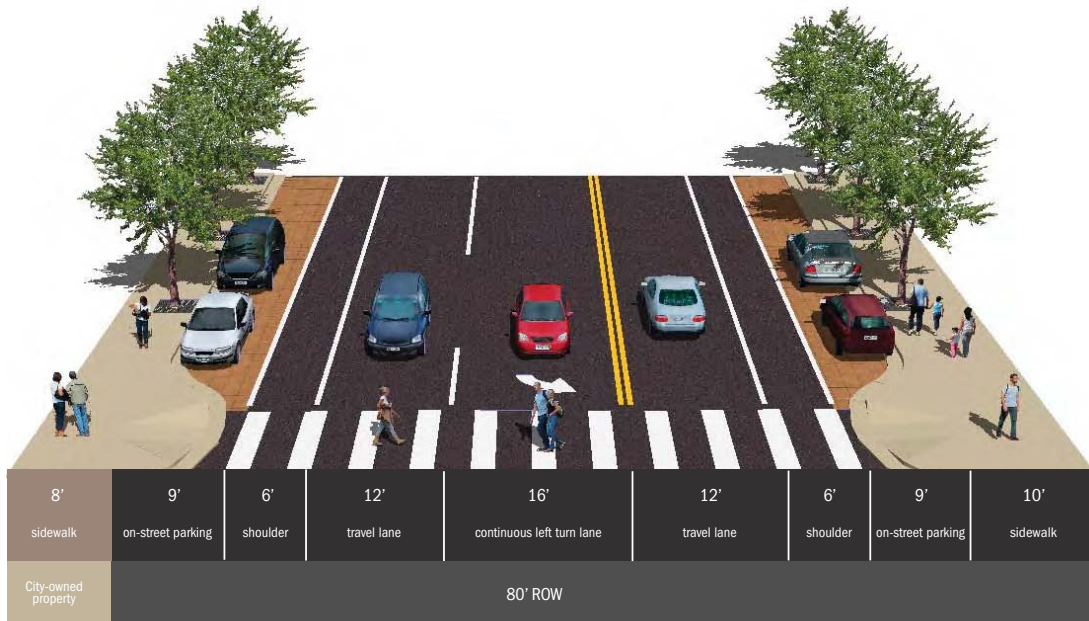


Figure 20. US 730 Cross-Section Detail for Segment 4



## Intersection Improvements

As part of the US 730 Streetscape Plan, improvements are identified at a number of key intersections along the US 730 study corridor. These improvements focus on preserving the long-term capacity and safety for both vehicular movements and pedestrian crossing movements.

### US 730/First Street Intersection Improvements

- In the near/mid-term, capacity based improvements include reconstructing the First Street highway approaches and installing separate left and through/right-turn lanes on both the north and south First Street approaches.
- In the long-term, a more structured form of intersection traffic control will be needed, such as a traffic signal or a roundabout.
- From a pedestrian standpoint, the First Street intersection is and will continue to be a major pedestrian connection between the north and south sides of US 730. As such, bulbouts have been identified to shorten the pedestrian crossing distance of US 730 and improve pedestrian awareness/visibility. The pedestrian bulbouts will be designed to accommodate all typical design vehicles.

### US 730/Third Street Intersection

- In the near/mid-term, Third Street is realigned to intersect US 730 at a right angle.
- In the near/mid-term, capacity based improvements include installing a separate left and right-turn lane on the southbound Third Street approach.
- Pedestrian bulbouts and a median refuge island are identified to shorten the pedestrian crossing distance of US 730 and improve pedestrian awareness/visibility. The refuge island will not restrict any turning movements at the US 730/Third Street intersection.
- The pedestrian bulbouts will be designed to accommodate all typical design vehicles.

### US 730/Division Street Intersection

- From a pedestrian standpoint, the Division Street intersection is and will continue to be a major pedestrian connection between the north and south sides of US 730. As such, bulbouts have been identified in the near term to shorten the pedestrian crossing distance of US 730 and improve pedestrian awareness/visibility.
- The pedestrian bulbouts will be designed to accommodate all typical design vehicles.



Figure 21. US 730/First Street intersection



Figure 22. US 730/Third Street intersection



Figure 23. US 730/Division Street intersection

**US 730/Seventh Street and US 730/Eighth Street Intersections**

- Seventh Street and Eighth Street intersect US 730 along the adjacent City park. Recognizing the potential for the park to generate pedestrian crossings at these intersections, a north side pedestrian bulb out and median refuge island (with a staggered crossing) have been identified in the near-term to shorten the pedestrian crossing distance of US 730 and improve pedestrian awareness/visibility.
- The design of the pedestrian bulbouts will accommodate all typical design vehicles.

**US 730/Tenth Street and US 730/Twelfth Street Intersections**

- Based on their proximity to the adjacent elementary school, Tenth Street and Twelfth Street are focal points for pedestrian crossings along US 730. As such, pedestrian bulbouts and a median refuge island (with a staggered crossing) have been identified to shorten the pedestrian crossing distance of US 730 and improve pedestrian awareness/visibility. The refuge island will not restrict any turning movements at both the US 730/Tenth Street intersection and the US 730/Twelfth Street intersection.
- The design of the pedestrian bulbouts will accommodate all typical design vehicles.

**US 730/Thirteenth Street and US 730/Fourteenth Street Intersections**

- The Thirteenth and Fourteenth Street intersections frame the gateway median along the eastern edge of the US 730 study corridor. In the mid/long-term, bulb-outs have been identified to improve pedestrian awareness/visibility and help establish US 730 through Irrigon as a lower speed multi-modal corridor.

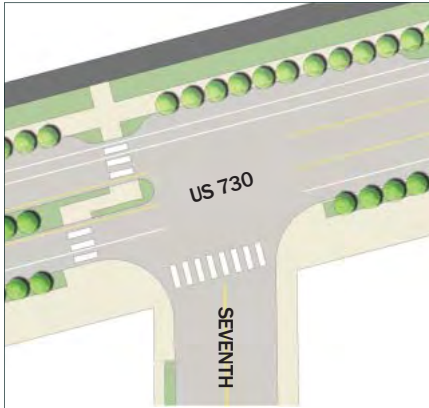


Figure 24. US 730/Seventh Street intersection

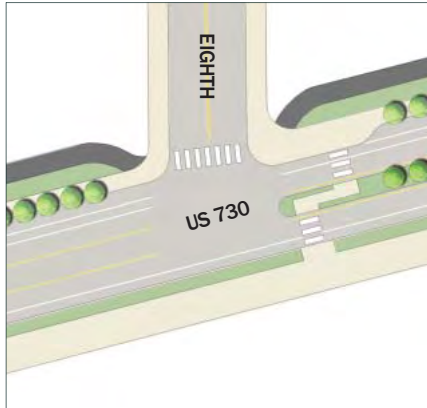


Figure 25. US 730/Eighth Street intersection

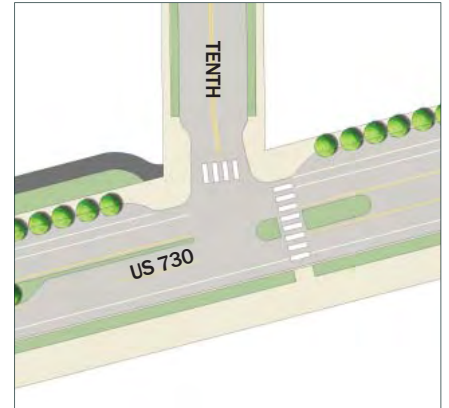


Figure 26. US 730/Tenth Street intersection

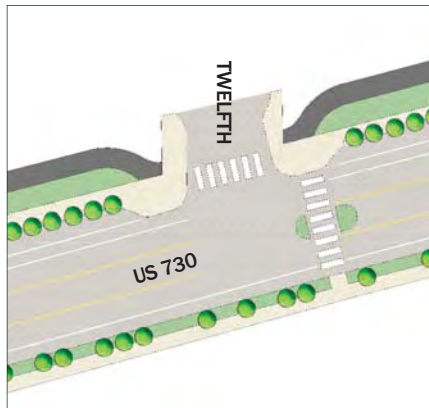


Figure 27. US 730/Twelfth Street intersection

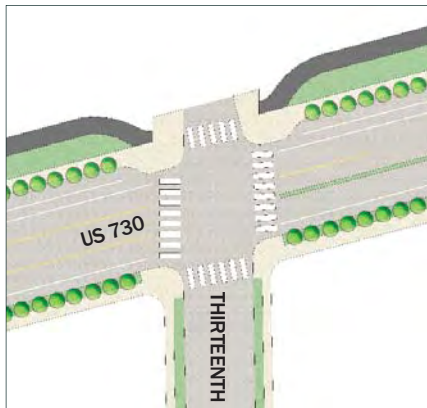


Figure 28. US 730/Thirteenth Street intersection

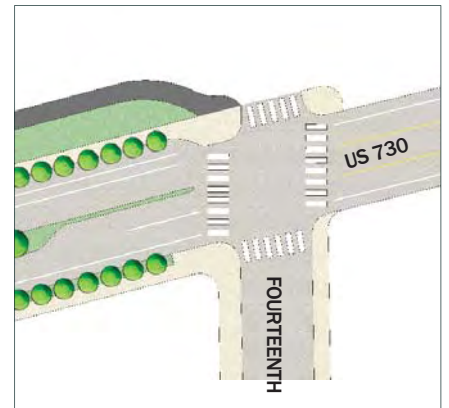


Figure 29. US 730/Fourteenth Street intersection

### **Access Management Strategy**

As part of the US 730 Streetscape Plan, a generalized highway access plan was developed to help identify future access locations and public circulation routes along the study corridor. The plan shall be used by the City of Irrigon and ODOT in future land use decisions involving the properties located within and along the US 730 study corridor.

### **US 730 Access Plan**

Access spacing standards along US 730 are currently regulated by the 1999 Oregon Highway Plan. Although it is inherently difficult to modify existing roadway sections to meet these exact access management standards, under the guidance of the planning process, an access management plan has been developed for the US 730 study corridor. The resulting access management plan contains strategies and future access plans that balance the need to provide reasonable access to the highway while still efficiently accommodating through traffic. Together with the recommended circulation improvement projects, the access management plan will enhance the safety, function, and capacity of the US 730 study corridor. The following section outlines details of the access management plan for US 730.

### **US 730 from Second Street to First Street**

This section of the study corridor is consistent with Segment “A” in Figure 30. The focus of the access management plan on this section of the study corridor is to consolidate the overall number of private access driveways in the near-term and plan to limit access and turning movements in the long-term through highway median controls. To achieve this, the following access plan and management strategies have been developed:

- **Near-Term:** Work to consolidate the existing driveways and highway approach permits through implementation of the following strategies:
  - *Identify illegal approaches and close (those driveways constructed since 1949 without a permit from ODOT) or if appropriate, place under permit. For legal approach permits, condition the permit to state that private access will be eliminated when other alternate, reasonable access becomes available to the property.*
  - *Identify locations where adjacent properties can share access to US 730 and relocate (indenture) existing highway approaches to the new shared locations.*
  - *Where properties have multiple highway approaches, identify situations where approaches can be consolidated.*
  - *Where properties have alternate reasonable access by some means other than US 730 such as an adjacent City street, establish a new secondary*



access, purchase remaining rights of access to the highway, and close the driveway.

- Purchase access control along those portions of the corridor where it hasn't already been acquired and where future development potential exists.
- **Mid/Long-Term:** Establish public access to the north side of US 730 as outlined below:
  - As part of private property redevelopment or capital improvement projects, establish a public access and approach approximately 500 feet west of First Street. This connection will provide secondary access between US 730 and Columbia and improve downtown circulation.
  - Upon development of the new public roadway, redirect adjacent properties with direct access to US 730 to the new roadway, purchase access reservations, and close the highway approaches.
- **Mid/Long-Term:** Construct a raised median along US 730 between Second Street and First Street. All remaining points of access including the new public street described above will have right-in/right-out access.

The following table provides an approach to accommodating the short term and the mid/long term access management strategy for Second to First.

Second Street to First Street			
Timeframe	Map Reference Number	Circulation/Access Project	Implementation Threshold
Near-Term	A1	Close non-permitted approaches to US 730.	When redevelopment occurs or as part of a highway improvement project.
	A2	Purchase and close existing reservations of access in which the affected property has reasonable alternative access to a public street or other legal approach.	
	A3	Purchase access control.	
Mid/Long-Term	A4	Development of Local Transportation Network	As part of a city improvement project or as redevelopment occurs.
	A5	Install a raised median from Second Street to First Street.	As part of a highway improvement project.

**US 730 from First Street to Division Street**

This section of the study corridor is consistent with Segment “B” in Figure Maintaining near- to mid-term access to the existing commercial properties and long-term access to the Third and Fourth Street corridors has been the identified access goal along this segment of the highway. As such, the focus of the access management plan is to consolidate the overall number of private access driveways in the near- to mid-term and plan to limit any new access in the long-term. To achieve this, the following access plan and management strategies have been developed:

- **Near-Term:** Work to consolidate the existing driveways and highway approach permits through implementation of the following strategies:
  - *Identify illegal approaches and close (those driveways constructed since 1949 without a permit from ODOT) or if appropriate, place under permit. For legal approach permits, condition the permit to state that private access will be eliminated when other alternate, reasonable access becomes available to the property.*
  - *Identify locations where adjacent properties can share access to US 730 and relocate (indenture) existing highway approaches to the new shared locations.*
  - *Where properties have multiple highway approaches, identify situations where approaches can be consolidated.*
  - *Where properties have alternate reasonable access by some means other than US 730 such as an adjacent City street, establish a new secondary access, purchase remaining rights of access to the highway, and close the driveway.*
  - *Purchase access control along those portions of the corridor where it hasn't already been acquired and where future development potential exists.*
- **Mid/Long-Term:** Establish a continuous Idaho Avenue connection between First and Division Streets.
  - *With the Idaho Avenue corridor established, close individual property driveways with direct access to US 730 and reconnect them to Idaho Avenue.*

The table below provides an approach to accommodating the short term and the mid/long term access management strategy for First to Division.

First Street to Division Street			
Timeframe	Map Reference Number	Circulation/Access Project	Implementation Threshold
Near-Term	B1	Close non-permitted approaches to US 730.	When redevelopment occurs or as part of a highway improvement project.
	B2	Purchase and close existing reservations of access in which the affected property has reasonable alternative access to a public street or other legal approach.	
	B3	Purchase access control.	
Mid/Long-Term	B4	Development of Local Transportation Network	As part of a city improvement project or as redevelopment occurs.

### US 730 from Division Street to Tenth Street

This section of the study corridor is consistent with Segment “C” in Figure 30. Compared with the previous two segments, there are significantly more highway approaches and less potential for the development of a supporting parallel local street network on the south side of the highway. Given these conditions, the focus of the access management plan on this section of the study corridor is to consolidate the overall number of private access driveways in the near/mid-term and work towards the development of a series of raised medians in the long-term. To achieve this, the following access plan and management strategies have been developed:

- **Near/Mid-Term:** Work to consolidate the existing driveways and highway approach permits through implementation of the following strategies:
  - *Identify illegal approaches and close (those driveways constructed since 1949 without a permit from ODOT) or if appropriate, place under permit. For legal approach permits, condition the permit to state that private access will be eliminated when other alternate, reasonable access becomes available to the property.*
  - *Identify locations where adjacent properties can share access to US 730 and relocate (indenture) existing highway approaches to the new shared locations.*
  - *Where properties have multiple highway approaches, identify situations where approaches can be consolidated.*
  - *Where properties have alternate reasonable access by some means other than US 730 such as an adjacent City street, establish a new secondary access, purchase remaining rights of access to the highway, and close the driveway.*
  - *Purchase access control along those portions of the corridor where it hasn't already been acquired and where future development potential exists.*
- **Long-Term** – Construct a raised median from Division Street to Seventh Street and another raised median from Eighth Street to Tenth Street.

The table below provides an approach to accommodating the short term and the mid/long term access management strategy for First to Division.

Division Street to Tenth Street			
Timeframe	Map Reference Number	Circulation/Access Project	Implementation Threshold
Near/Mid-Term	C1	Close non-permitted approaches to US 730.	When redevelopment occurs or as part of a highway improvement project.
	C2	Purchase and close existing reservations of access in which the affected property has reasonable alternative access to a public street or other legal approach.	
	C3	Purchase access control.	
Long-Term	C4	Development of Local Transportation Network	As part of a city improvement project or as redevelopment occurs.
	C5	Construct a raised median from Division Street to Seventh Street. Construct a raised median from Eighth Street to Tenth Street.	As part of a highway improvement project.

**US 730 from Tenth Street to Fourteenth Street**

This section of the study corridor is consistent with segment “D” and “E” in Figure 31. The focus of the access management plan on this section of the study corridor is to consolidate the overall number of private access driveways in the near-term and plan to limit access and turning movements in the long-term through highway median controls. To achieve this, the following access plan and management strategies have been developed:

- **Near/Mid-Term:** Work to consolidate the existing driveways and highway approach permits through implementation of the following strategies:
  - *Identify illegal approaches and close (those driveways constructed since 1949 without a permit from ODOT) or if appropriate, place under permit. For legal approach permits, condition the permit to state that private access will be eliminated when other alternate, reasonable access becomes available to the property.*
  - *Identify locations where adjacent properties can share access to US 730 and relocate (indenture) existing highway approaches to the new shared locations.*
  - *Where properties have multiple highway approaches, identify situations where approaches can be consolidated.*
  - *Where properties have alternate reasonable access by some means other than US 730 such as an adjacent City street, establish a new secondary access, purchase remaining rights of access to the highway, and close the driveway.*
  - *Purchase access control along those portions of the corridor where it hasn't already been acquired and where future development potential exists.*
  
- **Long-Term:** Construct a raised median along US 730 between Thirteenth Street and Fourteenth Street. All remaining points of access will have right-in/right-out access.

The table below provides an approach to accommodating the short term and the mid/long term access management strategy for First to Division.

Tenth Street to Fourteenth Street			
Timeframe	Map Reference Number	Circulation/Access Project	Implementation Threshold
Near/Mid-Term	D1/E1	Close non-permitted approaches to US 730.	When redevelopment occurs or as part of a highway improvement project.
	D2/E2	Purchase and close existing reservations of access in which the affected property has reasonable alternative access to a public street or other legal approach.	
	D3/E3	Purchase access control.	
Long-Term	E4	Construct a raised median from 13th Street to 14th Street	As part of a highway improvement project.



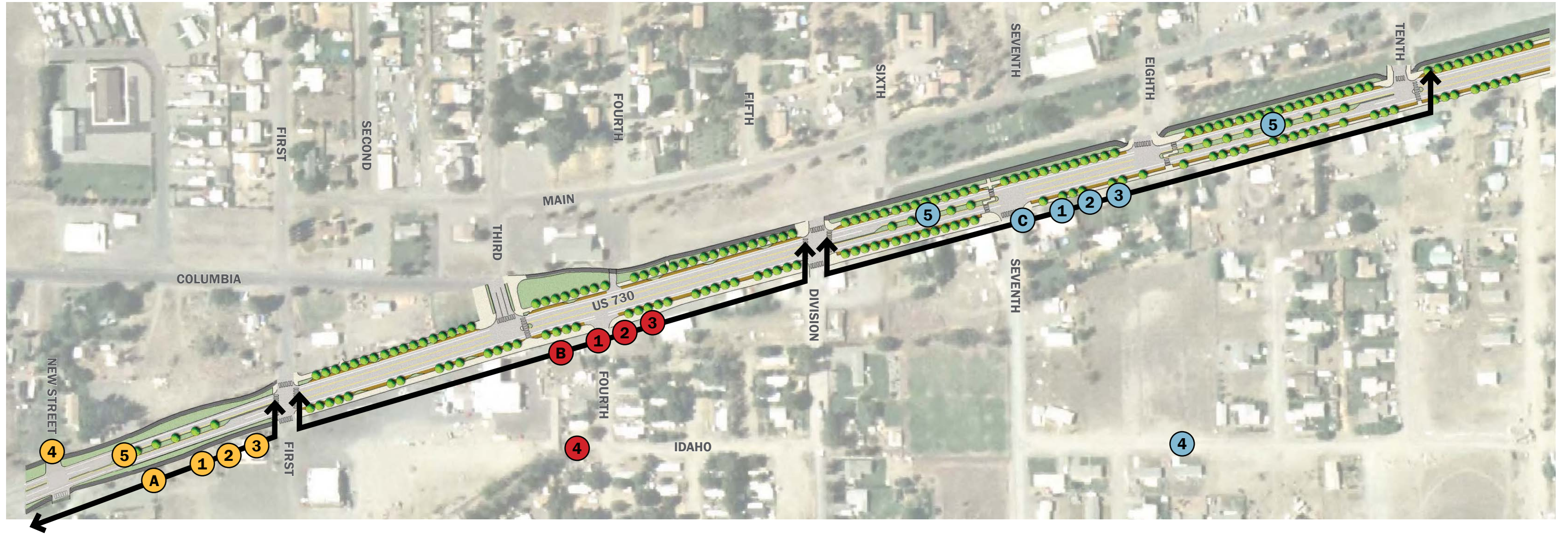
## Special Transportation Area (STA) Designation

As defined by the Oregon Highway Plan, a Special Transportation Area (STA) is a highway segment designation that may be applied to a highway segment when an existing downtown or planned downtown, business district or community center straddles the state highway in existing or certain planned urban centers. In other words, an STA designation is ODOT's formal recognition that specific sections of state highways need to balance regional through movements with the multi-modal circulation and local access needs of the adjacent jurisdiction.

Based on the improvements identified in the Irrigon Downtown Development Plan and US 730 Streetscape Plan, an STA designation is an important component in the foundation of a highway corridor that accommodates through movements while still fostering a corridor with adequate public street access, slower travel speeds, and safe multi-modal access. The Irrigon Downtown Development Plan and US 730 Streetscape Plan has developed the following series of circulation and access components that support the designation of an STA along US 730 from First Street to Tenth Street.

- An identification of a compact urban core that focuses a formal downtown district on the north side of US 730 and an enhanced commercial zone along the south side of US 730.
- An interconnected network of existing and planned local streets to serve the downtown core and commercial zones.
- An identified series of formal pedestrian crossing treatments (intersection bulb outs and pedestrian refuge islands), bicycle lanes, and on-street parking to accommodate local multi-modal transportation needs with through traffic along the highway.
- A series of sidewalks and combined recreational paths located adjacent to the north and south sides of the highway.
- A streetscape plan for US 730 that will help to change the focus from the efficient accommodation of through traffic to one that fosters slower travel speeds, accommodates side-street turning movements, and emphasizes local pedestrian and bicycle travel.
- An access management plan that strives to reduce direct private access to the highway and emphasize future access via a series of public street connections.

PART 2: US 730 STREETScape PLAN



**A**

**Near-Term**

- ① • Close non-permitted approaches to US 730
- ② • Purchase and close existing reservations of access in which the affected property has reasonable alternative access to a public street or other legal approach.
- ③ • Purchase access control.

**Mid/Long-Term**

- ④ • Development of Local Transportation Network
- ⑤ • Install a raised median from Second Street to First Street.

**B**

**Near-Term**

- ① • Close non-permitted approaches to US 730
- ② • Purchase and close existing reservations of access in which the affected property has reasonable alternative access to a public street or other legal approach.
- ③ • Purchase access control.

**Mid/Long-Term**

- ④ • Development of Local Transportation Network

**C**

**Near-Term**

- ① • Close non-permitted approaches to US 730
- ② • Purchase and close existing reservations of access in which the affected property has reasonable alternative access to a public street or other legal approach.
- ③ • Purchase access control.

**Mid/Long-Term**

- ④ • Development of Local Transportation Network
- ⑤ • Construct a raised median from Eighth Street to Tenth Street.

**Figure 30. US 730 ACCESS MANAGEMENT STRATEGY - SECOND STREET TO TENTH STREET**  
 Irrigon Downtown Development Plan and US 730 Streetscape Plan  
 JUNE 2009 / DECEMBER 2009

This illustrated plan is a conceptual rendering. Proposed street alignments and development footprints are for planning purposes and do not represent final locations or actual development proposals.







**Near-Term**

- 1 • Close non-permitted approaches to US 730
- 2 • Purchase and close existing reservations of access in which the affected property has reasonable alternative access to a public street or other legal approach.
- 3 • Purchase access control.



**Near-Term**

- 1 • Close non-permitted approaches to US 730
- 2 • Purchase and close existing reservations of access in which the affected property has reasonable alternative access to a public street or other legal approach.
- 3 • Purchase access control.

**Mid/Long-Term**

- 4 • Construct a raised median from Thirteenth Street to Fourteenth Street

Figure 31. US 730 ACCESS MANAGEMENT STRATEGY - TENTH STREET TO FOURTEENTH STREET  
 Irrigon Downtown Development Plan and US 730 Streetscape Plan  
 S E R A  
 JUNE 2009 / DECEMBER 2009

This illustrated plan is a conceptual rendering. Proposed street alignments and development footprints are for planning purposes and do not represent final locations or actual development proposals.

