

## Summary of Written Comments Received by July 21, 2016

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## **Comments Received**

**Project # 1A0398**

**Port of Hood River**

**Aviation Technology & Emergency Response Center**



Scott Turnoy  
Freight Planning Program Manager  
Oregon Department of Transportation  
555 13th Street NE, Suite 2  
Salem, OR 97301

Dear Scott,

The Mid-Columbia Economic Development District (MCEDD) submits this letter in support of the Port of Hood River's ConnectOregon VI project proposal, Aviation Technology & Emergency Response Center to the Oregon Department of Transportation.

Annually, MCEDD develops a regional Comprehensive Economic Development Strategy or CEDS. In the 2016 CEDS update, the need to address transportation infrastructure emerged as a key element to economic stability and growth as an important part of our economic development strategy.

Infrastructure improvements at the Ken Jernstedt Airfield will help address diverse aviation transportation needs in our region. Most notable will be the ability to service the growing aviation technology industry, a fast growing sector of our economy. The Airfield improvements are also key to emergency response infrastructure and resiliency planning as the Port states the proposed improvements will enable aircraft fire-fighting crews to safely operate out of a dedicated location.

MCEDD supports the Port of Hood River in seeking a ConnectOregon VI grant for its proposed Aviation Technology & Emergency Response Center.

Sincerely,

A handwritten signature in blue ink, appearing to read "A Hoey", is positioned above the printed name.

Amanda Hoey  
Executive Director

**MCEDD is an equal opportunity employer, lender and provider.  
Contact MCEDD at 541-296-2266; TTY 711**

## **Comments Received**

**Projects # 2A0397**

**City of Eugene**

**Eugene Airport Roadway Improvements**



Eugene Area  
Chamber of  
Commerce

April 14, 2016

To whom it may concern:

Throughout our organization's 100-year history the Eugene Area Chamber of Commerce has believed that community infrastructure is crucial to economic success. And just as railroads were essential to economic development in the 19th century, air service is fundamental to economic development success today. With a market of over 600,000 the Eugene Airport serves as a key element in the future economic health of our region. That is why air service development has been and remains a key priority for our organization and the local business community.

The Eugene Chamber enthusiastically supports the Eugene Airport's *ConnectOregon* project to improve the condition of the access roads around the airport. Over the past 5 years, the airport has seen record numbers of passengers in and out of the area, with total passengers approaching one million per year. This reflects local companies going to other communities to further expand their business, local residents recognizing the convenience of flying locally, and travelers from all over the world coming to our community in ever-increasing numbers.

In 2015 the Eugene Airport added new direct service to San Jose, California. In June of 2016, United Airlines has announced movement to larger aircraft serving San Francisco, increasing seat capacity by over 30%. Both of these moves connects our community to many of the premier technology companies in the world.

More options also improves access for other business and leisure travel to and from the area. The airport now has 10 direct connections which allows extensive opportunities for outreach to promote the wonderful cultural events, food/dining, and opportunities that our community provides.

Quality air service drives economic investment in regions like ours. The Eugene Airport's grant request can help make that happen. The roadway improvement project will help improve safety to and from the airport. With increasing traffic, the roads are showing their age. There are more heavy vehicles, busses, taxis, and personal cars using this route. It is our belief that repairing and resurfacing these roadways now is a cost-effective way to ensure continued economic growth in the community and the new life of the roads will help accommodate the incredible traffic anticipated for the 2021 World Track and Field competition.

Best regards,

Dave Hauser  
President & CEO

1401 Willamette Street  
P.O. Box 1107  
Eugene, OR 97440-1107  
TEL: 541.484.1314  
FAX: 541.484.4942

## **Comments Received**

**Project # 4A0383**

**City of Bend**

**Bend Airport Helicopter Operations Area Phase 2**



*Professional Air*  
Travel that works *for you*

Connect Oregon VI

via email

11/13/15

Chris Cummings, Freight Program Manager

[ConnectOregonApps@odot.state.or.us](mailto:ConnectOregonApps@odot.state.or.us)

[Christopher.J.CUMMINGS@odot.state.or.us](mailto:Christopher.J.CUMMINGS@odot.state.or.us)

Re: Proposal for funding a Helicopter Operations Area at the Bend Airport

Dear Connect Oregon VI:

We are a business and employer of 30 at the Bend Municipal Airport. The airport has applied for Connect Oregon VI to receive funding to build a Helicopter Operations Area (HOA, or heliport) in the northeast corner of the airport. We are strongly opposed to this project, for the following reasons:

The project being proposed, while be applied for by a public entity, the City of Bend, is in fact a project that will benefit only one private entity, Leading Edge Aviation (LEA), a helicopter school currently located at the airport. The main purpose for this HOA is not to build "a critical link connecting elements of Oregon's transportation system that will measurably improve utilization and efficiency of the system," as defined in the Connect Oregon Legislation Rules. Despite the representations of the City and its private entity partner, Leading Edge, this HOA is designed specifically for allowing one helicopter school to re-locate its operation to this new area, and allowing it to expand fairly dramatically. This is not the purpose of Connect Oregon funds. The City will likely state in its application that this is a critical project for improving the safety of air traffic at the airport. In fact the opposite is true.

The first priority for improving safety and efficiency at the Bend Airport should be an Air Traffic Control Tower. This would benefit ALL users of the airport, not just a single business entity, Leading Edge, which is the case with the proposed HOA. The cost of a control tower is less than half the cost of the proposed HOA.

An HOA does not solve the biggest problem at the Bend Airport - too many operations occurring in too small an airspace, and this HOA and the resulting increase in operations will actually decrease safety. It will result in an even greater number of helicopters operating in our small airspace. It also will allow larger helicopters to be used in the LEA training program, with more congestion, and more noise.

This proposed project comes with no guarantees that LEA or anyone else will build facilities next to or on this HOA. For LEA, this means that their training helicopters will hop back and forth across the runway to their current home base and hangars even more frequently than they do now. These movements

cause constant conflict with fixed wing aircraft on the ground as well as in the runway/taxiway environment.

To reiterate - this entire HOA project is being built to benefit one company, LEA – anyone who argues that other helicopters will use that landing or parking area is not being honest. Other commercial helicopter operators such as AirLink, LifeFlight, the National Guard, US Forest Service, and Fish & Game – they all come to Bend for FBO services and fuel. They will not utilize this HOA until such services and fuel are available at this HOA facility. IF LEA (or anyone else) builds FBO services and fuel over there, *then* it makes sense. But Connect Oregon would be betting on a private entity building these additional facilities. We doubt very much they are going to spend the millions of dollars necessary to put in all of the infrastructure and facilities needed to make this a viable investment - hangars, fuel, and an FBO building at a minimum. These facilities are why pilots and customers come to Bend.

The proposed HOA comes with a proposed split of the airport’s airspace and new traffic patterns. We believe these new patterns will create very dangerous conditions: the possibility of 2 aircraft landing simultaneously at opposite ends of our single runway; aircraft on downwind legs electing to choose (always at pilot’s discretion) the opposite runway from another aircraft may find themselves head-to-head in the pattern; 2 aircraft could end up trying to land simultaneously on the same runway – one from the east and one from the west (one heli and one fixed); the dual patterns will cause a pilot in one pattern on one half of the field to have to have vision and knowledge of where aircraft are in the pattern on the other side of the field – it is already difficult as it is with all of the “targets” currently in and around our airspace, and hovering on the taxiways.

Of lesser concern to Connect Oregon, the proposed location of this HOA moves the helicopter approaches and takeoffs a half mile closer to Cimarron City, long a contentious issue with its residents. It increases the potential noise (more and larger helicopters), moves that noise contour much closer to this residential area, and the new flight patterns will most certainly fly over more houses to the east of the airport.

Thank you for your consideration.

Sincerely,

A handwritten signature in black ink, appearing to read "Gwil Evans". The signature is fluid and cursive, with a long horizontal stroke at the end.

Gwil Evans

President and General Manager, Professional Air

## **Comments Received**

**Projects # 1B0392**

**City of Portland**

**Red Electric Trail – Off Street Section (Portland, OR)**

## **TURNROY Scott**

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**From:** Sen Burdick <sen.ginnyburdick@state.or.us>  
**Sent:** Friday, April 22, 2016 2:21 PM  
**To:** TURNROY Scott  
**Subject:** Letter of support for the Red Electric Trail Connect Oregon grant  
**Attachments:** Letter of Support - Red Electric Trail (Sen Burdick).docx

Dear Mr. Turnoy,

Attached is a letter of support signed by Senate Majority leader Ginny Burdick and House Majority Leader Jennifer Williamson for the Connect Oregon grant that would provide funding to construct a segment of the Red Electric Trail. If this trail is completed, it will link the downtown area and SW Portland with the Fanno Creek Regional trail in Washington county. Doing so would provide much needed pedestrian and cycling infrastructure to the area.

Please let me know if you have any questions or concerns! I've mailed a hard copy of the letter as well.

Best,

Kate

Kate Grosswiler, Legislative Aide  
Office of Senator Ginny Burdick  
Senate Majority Leader  
Serving District 18  
[sen.ginnyburdick@state.or.us](mailto:sen.ginnyburdick@state.or.us)  
503-986-1718

Dear Mr. Turnoy,

As legislators representing Southwest Portland, we urge your support of the Red Electric Trail application for Connect Oregon VI funds.

The Red Electric Trail has been on the City of Portland's "to do list" since the concept first emerged as a partnership with the City of Beaverton and Washington County, among other local governments, in 2003. The Fanno Creek Trail, which runs along the creek in Washington County and is well-used for transportation, recreation, and exercise, ends at the county line. Meanwhile, progress has been made on the Red Electric Train that could eventually connect the Fanno Creek Trail to the Willamette River – creating a connection that will increase multimodal transportation options in a time of dramatically increasing traffic.

The portions of the Red Electric Trail that would be funded by Connect Oregon VI run through a part of Southwest Portland that has very little pedestrian and cycling infrastructure. Pedestrians have limited and often indirect routes to leave the neighborhood, as sidewalks are few and far between – even on busy streets. Bike commuters are forced to ride on busy streets without shoulders, meaning that only the most experienced and bold riders feel comfortable leaving the neighborhood by bike. Further, this part of Southwest Portland has very limited transit service. The Red Electric follows an old rail line that would eventually create a direct route to the Willamette River, and that avoids both the busy streets and the hills that make walking and biking difficult and even dangerous.

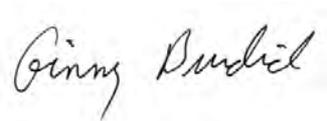
The public health benefits are easy to imagine – even for the small portion of the larger Red Electric Trail project proposed for Connect Oregon VI. Families with children could enjoy a ride through the neighborhood, helping to create lifelong habits and hobbies. Currently, local kids learn to ride their bikes in parking lots because there are no sidewalks on which to safely practice. The Red Electric Trail will also connect the neighborhoods to local public schools, all of which are on the proposed trail route. The segment that would be funded by Connect Oregon VI includes improving pedestrian and bike access to Hayhurst School and Pendleton Park.

While this project would certainly benefit residents of Southwest Portland, it would also benefit residents of Washington County and Beaverton. Fanno Creek Trail users could add just a few miles onto their ride to enjoy shopping and recreation in Hillsdale, for example, when the project is complete. Eventually, it would make it possible for one to bike commute from Washington County to Downtown Portland without braving unprotected bike lanes on busy, high-speed corridors – or no bike lines at all.

In the 13 years since the Red Electric Trail conversations began in earnest, the project has attracted broad support from neighborhood groups. Federal and state funding has been

identified for portions of the trail, including a bridge in the Hillsdale neighborhood. This is an area of the state that is well-populated, but underserved by infrastructure. This project would benefit thousands of people. I hope that you will be part of this exciting opportunity to make a real difference for a neighborhood in need of safer routes to schools, work, shopping, and recreation.

With gratitude for your consideration,

A handwritten signature in cursive script that reads "Ginny Burdick".

Senate Majority Leader Ginny Burdick

A handwritten signature in cursive script that reads "Jennifer Williamson".

House Majority Leader Jennifer Williamson

## **Comments Received**

**Project # 1B0402**

**City of Portland**

**Flanders Crossing Active Transportation Bridge**

April 26, 2016

Commissioner Steve Novick  
1221 SW 4th Ave., Suite 210  
Portland, OR 97204  
[novick@portlandoregon.gov](mailto:novick@portlandoregon.gov)

Dear Commissioner Novick,

We at SERA Architects wish to express our strong support for the NW Flanders Street Greenway project. We encourage you to lend your efforts to making these improvements real by supporting the *ConnectOregon* grant to construct safer crossings at Naito Parkway and a pedestrian and bicycle bridge over I-405. Ever since our founder, Bing Sheldon, helped create the 1972 Downtown Plan, which foresaw the importance of reinvigorating downtown as a place for people first, SERA has firmly advocated for investments and programs to **make walking and biking the easiest and safest means** of moving about the Central City. Indeed NW Flanders Street has been identified for walking and bicycling improvements ever since that 1972 Downtown Plan.

Our employees have embraced walking and biking to the office, but far too often find that a convenient and intuitive route from NE or SW Portland suddenly ends upon reaching downtown. NW Flanders, with easy access to and from the Steel Bridge, can be the premier east/west route for people on foot and bike to travel from the West Hills through the bustling Alphabet District and Pearl to Old Town and connections to the Willamette River and Waterfront Park. Through the several Downtown and Central City plans, the City has acknowledged the importance of Flanders for connecting homes, businesses, hospitals, schools, social services, and parks, and now is the time to realize that goal by making these improvements. Two specific projects, both under consideration by *ConnectOregon*, are key to allowing NW Flanders to achieve its full potential:

- **A legal and safe crossing of Naito Parkway** and the railroad tracks to connect to the Tom McCall Waterfront and across the Steel Bridge to the Eastbank Esplanade and Lloyd District.
- **A pedestrian and bicycle bridge across the gaping I-405 cut.** Everett and Glisan Streets are both uncomfortable one-way options with inadequate sidewalks, dangerous intersections, and numerous on/off-ramp conflicts. NW Flanders is the ideal location for this crossing and like so many well-conceived infrastructure projects serves many users, not only walkers and bicyclists but also emergency responders who could use it as a seismically robust crossing of I-405 in case of a major earthquake.

Our office is on NW Flanders Street and it is encouraging imagining just a few years from now so many more people traveling past our windows on a safe, comfortable, and convenient route through the heart of NW Portland. We hear about the need to improve this route not just from our SERA coworkers but also from our development partners constructing the projects that will bring thousands more residents to NW Portland and further underscores the need for strong connectivity options. The NW Flanders Greenway is a top priority of ours and we urge you to support the grant proposals for both the Naito Crossing and I-405 Bridge.

Thank you for your leadership and vision on this project and so many others in the city.

Sincerely,

  
Joe Pinzone

  
Kurt Schultz

cc: Reza Farhoodi, Pearl District Neighborhood Association – [rmichael87@gmail.com](mailto:rmichael87@gmail.com)  
Mark Lear, Portland Bureau of Transportation – [mark.lear@portlandoregon.gov](mailto:mark.lear@portlandoregon.gov)  
Zef Wagner, Portland Bureau of Transportation – [zef.wagner@portlandoregon.gov](mailto:zef.wagner@portlandoregon.gov)

The Honorable Steve Novick  
Portland City Council  
1221 SW 4th Avenue, Suite 210  
Portland, OR 97205

Dear Commissioner Novick;

I am writing to add our endorsement to the chorus of business, neighborhood advocates and individuals who have voiced strong support for the Connect Oregon application of the NW Flanders Neighborhood Greenway Project and Active Transportation Bridge. We respectfully urge you to prioritize this significant proposal and work for its full funding.

While there are many reasons to support a proposal like this which supports and advances important Portland values, here are just a few of the reasons for our support:

- 1. NW Flanders Greenway strengthens and enhances Portland Neighborhoods.** The project would create an important piece of urban infrastructure that would improve the local connection between dense and growing Portland Neighborhoods. For 10 years, Zoom+ has located its on-demand clinics in Portland's neighborhoods and we have come to see how Portland's families have responded to local improvements that enhance their neighborhood livability.
- 2. NW Flanders Greenway encourages mobility and healthy lifestyles.** At Zoom+ we have a fundamental commitment to helping people achieve their top health potential. Movement and active lifestyles are essential to today's primary care and we applaud the project's promotion of the shared agenda of transportation mobility and healthy lifestyles.
- 3. NW Flanders Greenway supports Urban employment goals.** Safe Transportation options for employees who prefer walking and biking allows Zoom+ and other urban employers to attract and retain valued employees. By the end of this year, Zoom+ will move its company headquarters -- we call it our Base Camp -- to the Pearl West building in the Pearl District and the development of the NW Flanders Neighborhood Greenway will have tremendous appeal to the creative and engaged workers who will help make Portland the national leader in the movement to reshape the delivery of health care services.

In sum, we are happy to endorse this project and the multiple benefits it will bring in connecting urban neighborhoods and providing safe and accessible transportation options for people who live, work and play in Portland. We respectfully urge your support for this application as a high priority Connect Oregon project.

Sincerely

**Dave Sanders, MD**  
Co-founder and CEO  
503-449-8964 (m)  
503-819-4134 (Adriana Daoust)



Received May 3, 2016

OREGON COLLEGE *of* ORIENTAL MEDICINE

ocom.edu | 75 NW Couch Street, Portland, OR 97209 | 503-253-3443

Commissioner Steve Novick  
1221 SW 4th Ave., Suite 210  
Portland, OR 97204

April 20, 2016

Dear Commissioner Novick,

Oregon College of Oriental Medicine (OCOM) strongly supports the Flanders Street Greenway project and urges you to prioritize efforts to make it a reality by supporting the Connect Oregon grant applications that would construct a safer crossing of Naito Parkway and a bicycle/pedestrian-only bridge across Interstate 405. OCOM draws 400 people every day to our campus at NW 1st and Couch. Of these, 16% report commuting by bicycle and 7% by foot. The Greenway project would facilitate increases in biking and walking and allow our college to attract and retain students, staff, faculty and patients.

Northwest Portland hosts a diverse and dense population that provides key local and regional services, employers, educational institutions, and housing for all income ranges. Improved bike and pedestrian safety is desperately needed to support this community. With the high concentration of residents and businesses, it is essential for the entire city to have safe multimodal connections within and to the Northwest. The reality is that commuters from the east side of our city are able to follow safe routes until they reach the west side of the river where they must contend with busy and dangerous city traffic.

Currently there is no safe east-west route for people to walk and bike from the River to the West Hills. This lack is why multiple city plans, going back to the 1972 Downtown Plan, have designated NW Flanders Street as a priority neighborhood greenway. To achieve its potential, though, Flanders needs critical improvements, including:

- A safe, at-grade, legal crossing of Naito Parkway to facilitate access to the bicycle/pedestrian path on the Steel Bridge and the regional trail system at Tom McCall Waterfront Park and the Eastbank Esplanade. This is especially important to the college given our high number of bicycle commuters and our location to the immediate west of Naito.
- A bridge that crosses the I-405 freeway canyon for both pedestrians and bicycles. Current sidewalks on Everett and Glisan Street are grossly inadequate and lead to dangerous crossings across speeding traffic. Existing bicycle facilities on those streets also have dangerous conflicts with freeway on- and off-ramps.



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ocom.edu | 75 NW Couch Street, Portland, OR 97209 | 503-253-3443

The proposed improvements help create a unified east-west corridor that is comfortable for both pedestrians and bicyclists, and provide a direct connection for thousands of bicycle commuters stretching from NW 23<sup>rd</sup> Avenue to Tom McCall Waterfront Park and the Eastbank Esplanade. This will create a safe and attractive regional route that stretches from the West Hills to Boring, Oregon via the Springwater Corridor trail.

In addition, the Flanders bridge at I-405 would be built to modern seismic design standards and provide a lifeline for emergency vehicles in case of a major earthquake. For those in the OCOM community with family west of I-405, this bridge would provide a critical link to loved ones when the next Cascadia subduction zone quake occurs.

This project is a top priority for us. We urge you to support the grant proposals for both the at-grade crossing at Naito Parkway, as well as the Flanders Crossing bridge across I-405.

Thank you for your leadership and support.

Sincerely,

Chris Chiacchierini  
Vice President for Planning and Operations

Commissioner Steve Novick  
1221 SW 4th Ave., Suite 210  
Portland, OR 97204

Received May 3, 2016

April 19, 2016

Dear Commissioner Novick,

I wish to express my strong approval for the Flanders Street Greenway project and urge you to prioritize efforts to make it a reality by supporting the Connect Oregon grant applications that would construct a safer crossing of Naito Parkway and a bicycle/pedestrian-only bridge across Interstate 405. As more commuters embrace alternative forms of transportation, these improvements would help facilitate greater biking and walking and allow our company to attract and retain top employees.

Northwest Portland – from Old Town/Chinatown, to the Pearl District, and to the Northwest District – hosts a diverse and dense population that provides key local and regional services, employers, educational institutions, and housing for all income ranges. Improved bike and pedestrian safety is desperately needed to support these populations, businesses, and institutions. With the high concentration of residents and businesses, it is essential for the entire city to have safe multimodal connections to Northwest. The reality is that commuters from the east side of our city are able to follow safe routes until they reach the west side of the river and are dumped directly into busy city traffic.

Currently there is no safe east-west route for people to walk and bike from the River to the West Hills. This lack is why multiple city plans, going back to the 1972 Downtown Plan, have designated NW Flanders Street as a priority neighborhood greenway. To achieve its potential, though, Flanders needs critical improvements, specifically:

- A safe, legal crossing of Naito Parkway to the bicycle/pedestrian path on the Steel Bridge and to the regional trail system at Tom McCall Waterfront Park and the Eastbank Esplanade.
- A bridge that crosses the I-405 freeway canyon for both pedestrians and bicycles. Current sidewalks on Everett and Glisan Street are grossly inadequate and lead to dangerous crossings across speeding traffic. Existing bicycle facilities on those streets also have dangerous conflicts with freeway on- and off-ramps.

These improvements help create a unified east-west corridor that is comfortable for both pedestrians and bicyclists, and provide a direct connection for thousands of bicycle commuters stretching from NW 23<sup>rd</sup> Avenue to Tom McCall Waterfront Park and the Eastbank Esplanade. This will create a safe and attractive route that stretches from the West Hills to Boring, Oregon via the Springwater Corridor trail.

In addition, the Flanders bridge at I-405 would be built to modern seismic design standards and provide a lifeline for emergency vehicles in case of a major earthquake.

This project is a top priority for us. I urge you to support the grant proposals for both the at-grade crossing at Naito Parkway, as well as the Flanders Crossing bridge across I-405.

Thank you in advance for your leadership and support.

Sincerely,

Michael Starzec  
10 Barrel Brewing Co.  
General Manager, Portland Pub  
1411 NW Flanders St., Portland, OR 97209  
503-224-1700



Commissioner Steve Novick  
1221 SW 4th Ave., Suite 210  
Portland, OR 97204

April 15, 2016

Dear Commissioner:

Mercy Corps has recently learned about the Flanders Street Greenway project and would like to express our support. We understand that Connect Oregon grant applications are pending that would construct safe and access across of Naito Parkway and a bicycle-and-pedestrian-only bridge across I-405.

Mercy Corps has an avid and active year-round bike commuter community, as do many businesses in our area. We endorse efforts that support environmentally sensitive commuting options, which is one of the reasons we located our Global Headquarters in Portland.

We're excited about the possibility of a safer pedestrian and bicycle route from Naito Parkway to the west hills. We urge your support of the grant proposals for the safe crossing at Naito Parkway and the Flanders Crossing bridge across I-405.

Thank you in advance for your consideration.

Sincerely,

A handwritten signature in blue ink, appearing to read "H Donnelly".

Hugh Donnelly  
Director  
Administration + Facilities



Commissioner Steve Novick  
1221 SW 4th Ave., Suite 210  
Portland, OR 97204

April 25, 2016

Dear Commissioner Novick,

At DeskHub we value easy access for our clients and employees. We are a co-working facility of over 150 people, many of whom are commuters and individuals who walk from outside and within the Pearl District.

We are excited to see the possibility of creating additional access through the Flanders Street Greenway project and urge you to prioritize efforts to make it a reality by supporting the Connect Oregon grant applications that would construct a safer crossing of Naito Parkway and a bicycle/pedestrian-only bridge across Interstate 405. This allows us to continue to grow our community and co-working site, which will attract and retain top employees.

Currently there is no safe east-west route for people to walk and bike from the River to the West Hills. This lack is why multiple city plans, going back to the 1972 Downtown Plan, have designated NW Flanders Street as a priority neighborhood greenway. To achieve its potential, though, Flanders needs critical improvements, specifically:

- A safe, legal crossing of Naito Parkway to the bicycle/pedestrian path on the Steel Bridge and to the regional trail system at Tom McCall Waterfront Park and the Eastbank Esplanade.
- A bridge that crosses the I-405 freeway canyon for both pedestrians and bicycles. Current sidewalks on Everett and Glisan Street are grossly inadequate and lead to dangerous crossings across speeding traffic. Existing bicycle facilities on those streets also have dangerous conflicts with freeway on- and off-ramps.

These improvements help create a unified east-west corridor that is comfortable for both pedestrians and bicyclists, and provide a direct connection for thousands of bicycle commuters stretching from NW 23<sup>rd</sup> Avenue to Tom McCall Waterfront Park and the Eastbank Esplanade. This will create a safe and attractive route

that stretches from the West Hills to Boring, Oregon via the Springwater Corridor trail.

In addition, the Flanders Bridge at I-405 would be built to modern seismic design standards and provide a lifeline for emergency vehicles in case of a major earthquake.

This project is a top priority for us. We urge you to support the grant proposals for both the at-grade crossing at Naito Parkway, as well as the Flanders Crossing Bridge across I-405.

Thank you in advance for your leadership and support!

Sincerely,



Ronda Closner  
DESKHUB  
334 NW 11<sup>th</sup> Avenue  
Portland, OR 97209



Received May 3, 2016

Commissioner Steve Novick  
Portland City Council  
1221 SW 4th Ave., Suite 210  
Portland, Oregon, 97204  
USA

April 20, 2016

Flanders Street Greenway Project

Dear Commissioner Novick,

We wish to express our strong approval for the Flanders Street Greenway project and urge you to prioritize efforts to make it a reality by supporting the Connect Oregon grant applications that would construct a safer crossing of Naito Parkway and a bicycle/pedestrian-only bridge across Interstate 405. As more commuters embrace alternative forms of transportation, these improvements would help facilitate greater biking and walking and allow our company to attract and retain top employees.

Northwest Portland – from Old Town/Chinatown, to the Pearl District, and to the Northwest District – hosts a diverse and dense population that provides key local and regional services, employers, educational institutions, and housing for all income ranges. Improved bike and pedestrian safety is desperately needed to support these populations, businesses, and institutions. With the high concentration of residents and businesses, it is essential for the entire city to have safe multimodal connections to Northwest. The reality is that commuters from the east side of our city are able to follow safe routes until they reach the west side of the river and are dumped directly into busy city traffic.

Currently, there is no safe east-west route for people to walk and bike from the River to the West Hills. This lack is why multiple city plans, going back to the 1972 Downtown Plan, have designated NW Flanders Street as a priority neighborhood greenway. To achieve its potential, though, Flanders needs critical improvements, specifically:

-A safe, legal crossing of Naito Parkway to the bicycle/pedestrian path on the Steel Bridge and to the regional trail system at Tom McCall Waterfront Park and the Eastbank Esplanade.

888 Brannan Street  
Fourth Floor  
San Francisco, California 94103  
United States



-A bridge that crosses the I-405 freeway canyon for both pedestrians and bicycles. Current sidewalks on Everett and Glisan Street are grossly inadequate and lead to dangerous crossings across speeding traffic. Existing bicycle facilities on those streets also have dangerous conflicts with freeway on- and off-ramps.

These improvements help create a unified east-west corridor that is comfortable for both pedestrians and bicyclists, and provide a direct connection for thousands of bicycle commuters stretching from NW 23rd Avenue to Tom McCall Waterfront Park and the Eastbank Esplanade. This will create a safe and attractive route that stretches from the West Hills to Boring, Oregon via the Springwater Corridor trail.

In addition, the Flanders Crossing bridge at I-405 would be built to modern seismic design standards and provide a lifeline for emergency vehicles in case of a major earthquake.

This project is a top priority for us. We urge you to support the grant proposals for both the at-grade crossing at Naito Parkway, as well as the Flanders Crossing bridge across I-405.

Thank you in advance for your leadership and support.

Sincerely,

A handwritten signature in blue ink that reads 'Cyn Wang'.

Cyn Wang

# PBOT

PORTLAND BUREAU OF TRANSPORTATION

1120 SW Fifth Avenue, Suite 800 Portland, OR 97204 503.823.5185  
Fax 503.823.7576 TTY 503.823.6868 www.portlandoregon.gov/transportation

Steve Novick Commissioner Leah Treat Director

Date: April 1, 2016

To: Region 1 ACT Members

From: Director Leah Treat, Portland Bureau of Transportation

CC: Commissioner Steve Novick, City of Portland

Re: City of Portland *ConnectOregon* Flanders Crossing Project Technical Score

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Thank you for taking the time to review the *ConnectOregon* project applications. I am writing to address a misunderstanding that negatively impacted the City of Portland's technical score for the Flanders Crossing Active Transportation Bridge. After the modal committees received additional information, they recommended that Flanders rank as the third highest bike/ped project in the state.

The material provided to you so far does not correct for this misunderstanding, and reflects a technical score of sixty-nine, which is just below the Tier 1 cut-off. We were able to receive this high of a score, despite receiving a negative five in the project readiness category because of the many significant benefits of the project. As detailed below, we do not believe this negative five in the project readiness category was merited.

- 1. The technical score comments stated that there was no acknowledgement of written support from ODOT even though there has been extensive collaboration between ODOT and PBOT on this project.**

Since receiving the technical score two additional letters were added to the application packet to address this. One of the letters was from the ODOT Region 1 Planning Manager stating that ODOT has had extensive consultation with PBOT. We had not included this information in the original application because the application did not ask for it and we assumed that ODOT was already aware of this collaboration. The *ConnectOregon* staff person helped us to add this information to the packets. However, our technical scores were not updated, so they still reflect this misunderstanding.

- 2. The technical score comments question PBOT's ability to deliver projects. PBOT has a long history of successful delivery of state and federally funded projects. We have also recently taken some additional steps to ensure successful and on time delivery.**

PBOT is confident that we would be able to deliver the Flanders project. We have a large volume of projects, and have a couple of legacy projects that we inherited with specific



*The Portland Bureau of Transportation fully complies with Title VI of the Civil Rights Act of 1964, the ADA Title II, and related statutes and regulations in all programs and activities. For accommodations, complaints and information, call (503) 823-5185, City TTY (503) 823-6868, or use Oregon Relay Service, 711.*

challenges that caused delays. We have taken some additional significant steps to ensure that we are able to continue to deliver projects in a timely manner:

- We have evaluated the schedules for all of our federal projects and we are prioritizing the completion of these projects.
- We have two projects that will start construction this spring, two other projects that will go to construction as soon as right of way is completed, and another project that is awaiting a federal permit and will go to construction as soon as the permit is obtained.
- We are also working with ODOT to finalize several IGAs and appreciate that ODOT has helped to expedite that process.
- Finally, we will be focused on completing and tracking projects with monthly progress reports to ensure that ODOT fully understands the ongoing progress of each project.

It is my sincere hope that you will consider this additional information in your future deliberations. If you have any questions, please contact me at 503-823-5085.

To: Christopher Cummings, ODOT  
From: Dan Layden, Capital Program Manager, PBOT  
Date: February 10, 2016  
RE Coordination with ODOT on Flanders Crossing Project

The City of Portland has coordinated extensively with the ODOT Region One on the Flanders Crossing project. We have coordinated both on the potential traffic impacts of a new signalized crossing and potential impacts of construction of a new bridge on the retaining walls for Interstate 405.

PBOT and ODOT staff met several times to discuss the need for modeling traffic impacts of the potential crossing at NW 16<sup>th</sup> and Flanders. The City of Portland is currently conducting traffic counts and origin destination studies in order to construct the analytical tools necessary to evaluate the crossing of NW 16<sup>th</sup> and Flanders. We anticipate completing this work before we begin design of the Flanders Crossing if the project is funded.

PBOT staff also met with Region One structural engineers to discuss the requirement for constructing bridge foundations. This work is documented in the Type, Size and Location report prepared for the project (attached).

The City of Portland is committed to delivering this project in cooperation with ODOT Region One and will continue to coordinate our efforts as we move forward on the project. We understand the timeline for this project is somewhat longer than the typical timeline for the Connect Oregon program. However, this is a significant project that will require some time to design and construct. The City is committed to completing the project in the timeline specified in our application. We have completed 30% design and are poised to move into final design if funding is approved.



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**From:** [MAKLER Jon](#)  
**To:** [Cummings Christopher J](#)  
**Cc:** [art.pearce@portlandoregon.gov](mailto:art.pearce@portlandoregon.gov); [BROOKS Kelly S](#); [MITCHELL Dennis J](#)  
**Subject:** PBOT, Connect Oregon, Flanders Bridge  
**Date:** Friday, February 05, 2016 9:59:48 AM  
**Attachments:** [image001.jpg](#)

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Chris,

I'm writing to affirm that Region 1 has had extensive consultation with PBOT regarding their proposal for a bike/ped bridge over I-405 at Flanders. As all parties would acknowledge, the issue is not fully resolved. But the City has been a forthcoming and earnest partner in responding to ODOT concerns, including commitment to and performance of traffic analysis specified by Region 1 staff. The evaluation of their application should absolutely reflect consultation with ODOT on this issue.

Respectfully,

Jon

Jon Makler, AICP  
Region 1 Planning Manager  
Oregon Dept of Transportation  
[jon.makler@odot.state.or.us](mailto:jon.makler@odot.state.or.us)  
Direct: (503) 731 – 4753  
Mobile: (971) 322 – 5633

crosswalk





# Flanders Crossing

Feasibility Study + Alternatives Analysis

PBOT Bridges and Structures  
November 2015



**PBOT**  
PORTLAND BUREAU OF TRANSPORTATION

# Flanders Crossing

Feasibility Study + Alternatives Analysis

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NOVEMBER 2015



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## Executive Summary

PBOT has undertaken a feasibility study and alternatives analysis for a new active pedestrian and bicycle bridge over I-405 at NW Flanders Street in Portland. The bridge is required to provide the residents of the NW District and the Pearl District a safer active crossing over I-405.

Six alternative designs were developed and assessed by both PBOT and Community stakeholders using the Sustainable Development principles of the Triple Bottom Line (TBL). This approach places equal emphasis on how the bridge design affects the equity of its constituents, the natural and built environment, and short and long term economics of the project. This approach is also known as balancing the needs of the 3 P's: People, Planet and Prosperity.

It was recognized that in addition to selecting a bridge design that best satisfies the established TBL criteria, the bridge should also be able to function as an alternate seismic resilient route for emergency vehicles following an earthquake.



*Artist rendering of Flanders Crossing Preferred Alternative C*

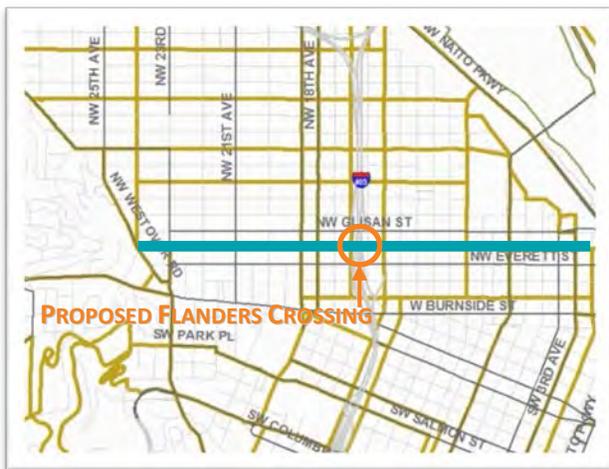
After analyzing the feedback from the multi-disciplined evaluation team, the preferred alternative was identified as ALTERNATIVE C – a 24 ft. wide, single span custom steel bridge as shown above, at an estimated project cost of \$6,009,656.

This bridge was generally seen as the ideal model for complying with Vision Zero safety principles, while also minimizing the disruptive impacts on the public during construction. The bridge also has the potential to be customized for local aesthetic appeal while still being relatively economical to build and maintain over the long term. Lastly, the configuration can be planned to allow for use by both active user and emergency vehicle responders during post-earthquake recovery operations.

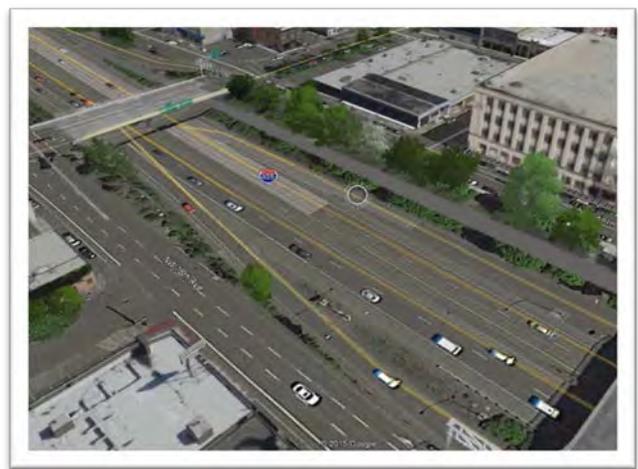
## Project Background + Location

Almost since the construction of the I-405 freeway in 1969, a safer connection for pedestrians and bicyclists from NW District to Pearl District was warranted. The existing crossings over I-405 at NW Everett St. and NW Glisan St. are highly congested at peak hours, do not provide adequate pedestrian and bicycle facilities, and are located in the heart of a busy freeway ramp network.

In 2007, the City of Portland identified the NW Flanders St. (and its crossing over I-405) as a bike corridor to serve an area with high potential for future non-motorized trips. The project is now envisioned as a neighborhood greenway that will extend on Flanders St. from NW 24<sup>th</sup> Ave. to Naito Parkway. This Feasibility Study & Bridge Alternatives analysis will solely focus on the crossing of NW Flanders at I-405.



*Project location map*



*View of existing project site*

## Site Constraints

There are a number of unique challenges at this site that affect the type, size and location of the bridge design selected. The following constraints were identified early on in the scoping process to avoid the expenditure of time and resources on alternatives that would not be feasible:

- Proposed bridge crosses over interstate freeway with approximately 100,000 vehicles per day
- Traffic impacts during construction could adversely affect the freeway and local street users
- Site requires approximately 200 foot long bridge
- Required 17'-4" vertical clearance to freeway below limits structure types
- New bridge should be built to current seismic design standards
- Available "landing space" at NW 16<sup>th</sup> is limited for safe connections to transportation network
- Partial demolition of existing ODOT retaining walls will be required

## Guiding Principles & Evaluation Criteria

In order to evaluate the proposed six bridge design alternatives, PBOT has used the guiding principles of Sustainable Development, which was first explored in 1987 by the United Nations in a report called Our Common Future. Initially defined as *development that meets the needs of the present without compromising the ability of future generations to meet their own needs*, sustainable development has evolved into the more defined Triple Bottom Line (TBL). The aim is to meet, with equal parity, the following:

- Equity of all constituents
- Environmental concerns
- Economic prosperity

They are presented in this study as the Three P's: People, Planet, and Prosperity. Where these three areas converge on an balanced solution, is likely to have a lasting sustainable design.

<b>PEOPLE</b> Alternatives evaluation will include factors that consider community safety and construction impacts	<b>PLANET</b> Environmental & aesthetic impacts will be considered during the alternatives evaluation process	<b>PROSPERITY</b> Both short and long term costs and financial risks will be included in the evaluation criteria
---	--	---

The Triple Bottom Line criteria as outlined below, were used on this project as the basis for comparison and evaluation of alternatives, and the selection of the locally preferred alternative.

### PEOPLE

**Bridge type embraces Vision Zero transportation safety principles:** PBOT aims to make Portland's transportation system the safest possible and to move towards zero traffic-related deaths or serious injuries. The Flanders Crossing Bridge should be designed to embrace Vision Zero principles by incorporating crosswalk strategies and alignments to protect the most vulnerable transportation users.

**Bridge type reduces construction delays and impacts on community:** Traffic disruptions during construction can increase travel times for users, as well as cause excessive noise and emissions locally. The project aims to reduce the impacts to the traveling public during construction of the bridge.

**Bridge type provides positive user experience and/or valuable use of public space:** The Flanders Crossing Bridge will not be designed for automobile use. It is desired that the design of this rare urban public space be enhanced for its users, rather than simply designed to convey bicyclists and pedestrians over the freeway.

## PLANET

**Bridge type provides aesthetic value:** Not every bridge is considered equal when it comes to its appearance. The look and feel of the bridge and how it interacts with the surrounding environment is an important factor in alternatives development for the Flanders Crossing.

**Bridge type limits carbon footprint and greenhouse gas emissions during construction:** Alternatives will be evaluated against their relative carbon footprint and greenhouse gas emissions during construction. Materials selection and construction-induced congestion would have direct impacts to greenhouse gas emissions and project carbon footprint.

**Bridge type preserves local character:** The ability of the bridge to suit the context and existing character of the area is important. It should be designed and built to avoid incompatibility with character of the locale.

## PROSPERITY

**Bridge type limits initial construction cost and risk:** Ultimately, the economic bottom line is an important factor in most transportation projects. As many transportation professionals aspire to design and build signature type bridge projects, it is important to balance the project cost with the need. Additionally, conventional design and construction methods generally result in lower risk to cost increases during construction.

**Bridge type minimizes future operation and maintenance costs:** The ever increasing costs to operate and maintain infrastructure assets to the end of their useful life is often not considered during the design phase. Given the reduced budgets for the operation and maintenance of bridges, the cost to perform such work should be considered during the alternatives evaluation process.

**Bridge type provides capacity for sustainable growth:** It is important that the bridge design selected have sufficient capacity to accommodate user growth over the next century of continual use, as people turn to active transportation to avoid the congestion of gridlocked streets.





Estimated Project Cost | \$4,046,606

Alternative A  
16 FT WIDE SINGLE SPAN STANDARD TRUSS BRIDGE



Estimated Project Cost | \$4,166,155

ALTERNATIVE B  
16 FT WIDE TWO SPAN CONCRETE BRIDGE



Estimated Project Cost | \$6,009,656

**PREFERRED ALTERNATIVE**  
ALTERNATIVE C  
24 FT WIDE SINGLE SPAN CUSTOM STEEL BRIDGE



Estimated Project Cost | \$6,917,414

ALTERNATIVE D  
24 FT WIDE SINGLE SPAN CABLE-STAYED BRIDGE



Estimated Project Cost | \$6,493,151

ALTERNATIVE E  
14 FT WIDE SINGLE SPAN PARALLEL TRUSS BRIDGES



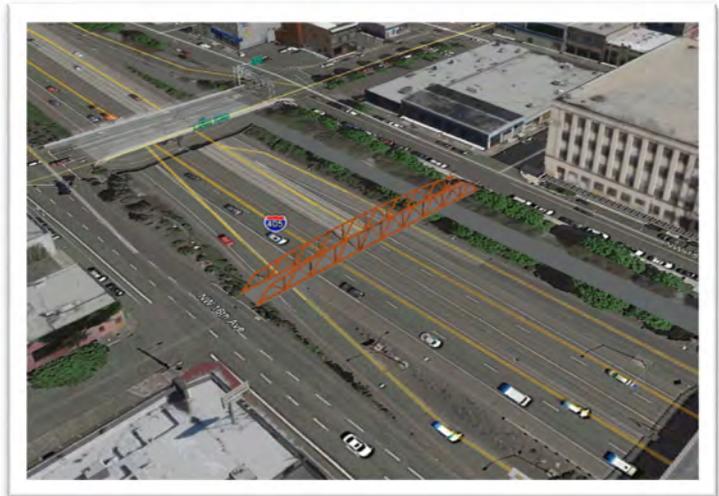
Estimated Project Cost | \$8,123,937

ALTERNATIVE F  
60 FT WIDE TWO SPAN CONCRETE PLAZA BRIDGE

### ALTERNATIVE A

#### 16 FT WIDE SINGLE SPAN STANDARD TRUSS BRIDGE

The relatively narrow single span 16 foot wide steel truss bridge consists of two steel trusses on the outside of the bridge. There are a number of examples of this bridge type around Portland, such as along the Springwater Corridor and at Chimney Park. These bridges are often made using weathering steel. This prefabricated bridge could be built quickly and at a low cost.



### ALTERNATIVE B

#### 16 FT WIDE TWO SPAN CONCRETE BRIDGE

The most common bridge type built in the region is a precast prestressed concrete girder bridge. These bridges can be built quickly and inexpensively. However, prestressed girder bridges are generally only adequate for shorter spans. Given this constraint, the Flanders Crossing Bridge would require a pier to be constructed in the freeway median below. The construction of the pier would be a major disruption to traffic on I-405.



### ALTERNATIVE C

#### 24 FT WIDE SINGLE SPAN CUSTOM STEEL BRIDGE

This alternative aims to provide the optimal target width for active users on the bridge and to clear span the freeway. The 24 foot wide configuration would allow for parallel 6 foot wide sidewalks on the outside of the bridge, and two 6 foot wide opposing bicycle lanes in the center of the bridge. The structure could be customized with aesthetic features or simple elegance to enhance its presence in the community.



## ALTERNATIVE D

### 24 FT WIDE SINGLE SPAN CABLE-STAYED BRIDGE

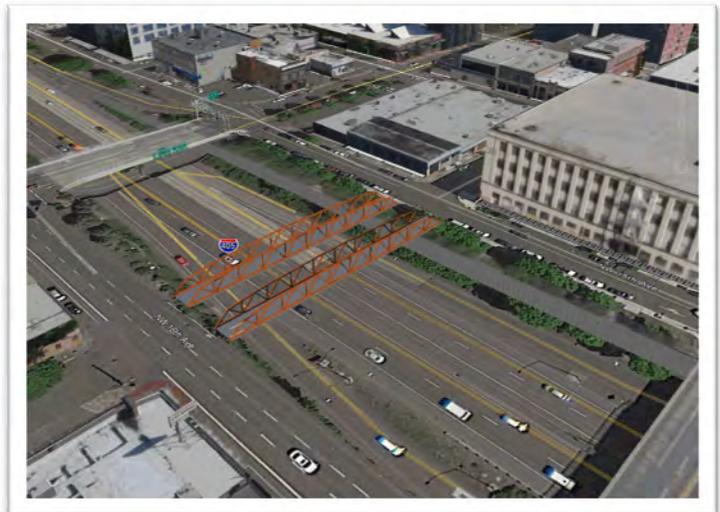
A signature bridge with iconic aesthetic features can define the spirit of a location and provide inspiration to those who witness its grandeur. A cable stayed alternative is presented in this study as the most suitable signature bridge type given the project constraints. It would complement the new Tilikum Crossing. It would provide similar safety benefits to Alternative C.



## ALTERNATIVE E

### 14 FT WIDE SINGLE SPAN PARALLEL TRUSS BRIDGES

The key feature to this alternative is the twin, or parallel, bridge concept. The two bridge configuration provides a directional separation of bicycles that would improve safety for all active users. Additionally, the proposed pedestrian area would line up with the approach sidewalks, which results in the most ADA compatible layout. The bridges could be off-the-shelf prefabricated bridges to reduce cost and construction time.



## ALTERNATIVE F

### 60 FT WIDE TWO SPAN CONCRETE PLAZA BRIDGE

In an increasingly hectic world, accessible public spaces become even more valuable. The plaza alternative aims to provide a worthwhile public space in addition to a transportation facility. It is envisioned as a respite for the neighborhood members from the chaos of urban life. The 60 ft. width would provide ample room for a safe crossing for all active users.

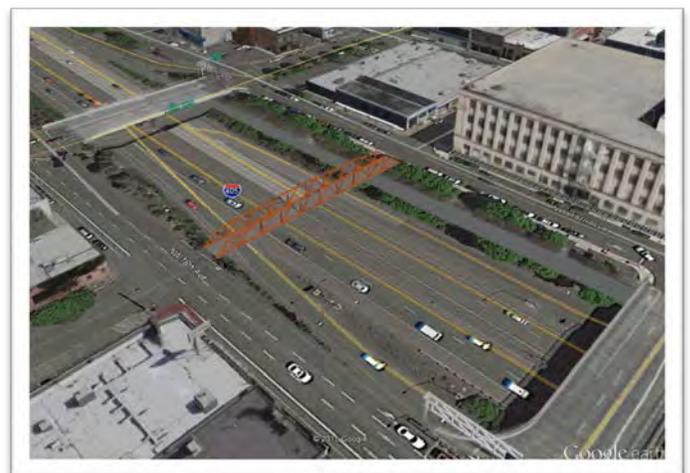
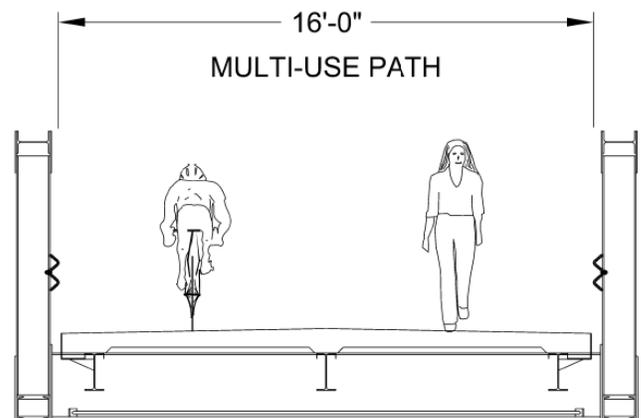
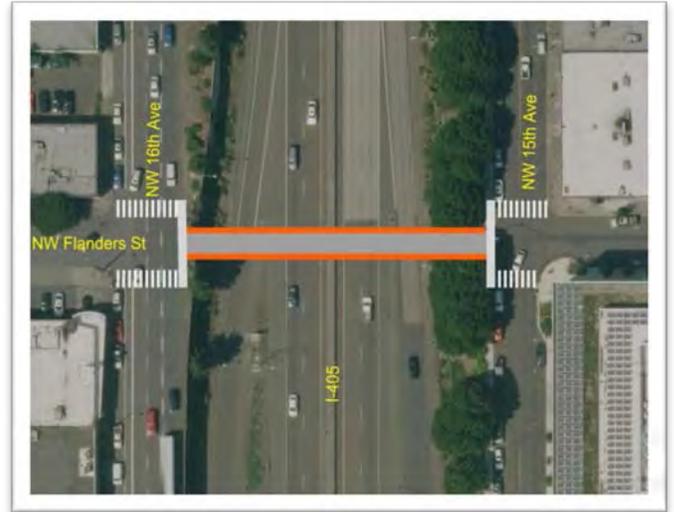


# Flanders Crossing | Alternative A

## 16 FT WIDE SINGLE SPAN STANDARD TRUSS BRIDGE

**ESTIMATED COST: \$4,046,606**

EVALUATION CRITERIA		DETAILS
PEOPLE	Bridge type embraces Vision Zero public transportation principles	Narrowest facility studied. Combines bicycles and pedestrians in both directions.
	Bridge type reduces construction impacts on community (e.g. delays, congestion, emissions, and noise)	Prefabricated single span would have least amount of impacts during construction.
	Bridge type provides positive user experience and/or valuable use of public space	Provides little room for public use. Primarily a transportation facility.
PLANET	Bridge type provides aesthetic value	Industrial look and feel. Compatible with warehouses nearby. Conventional truss design.
	Bridge type limits carbon footprint and greenhouse gas emissions during construction	Small footprint. Steel material has low carbon footprint.
	Bridge type preserves local character	Smallest footprint considered. Open truss design.
PROSPERITY	Bridge type Limits initial project construction cost and risk	\$4,046,606 Conventional construction methods result in low risk.
	Bridge type minimizes future operation and maintenance costs	Re-painting of truss elements required every 30-40 years.
	Bridge type provides capacity for sustainable growth	Narrowest bridge alternative. Provides least capacity for users.





*Example of a standard steel truss active user bridge at SE Lafayette Street in Portland, OR*



*Typical user experience on a standard steel truss bridge with protective screen both sides.*

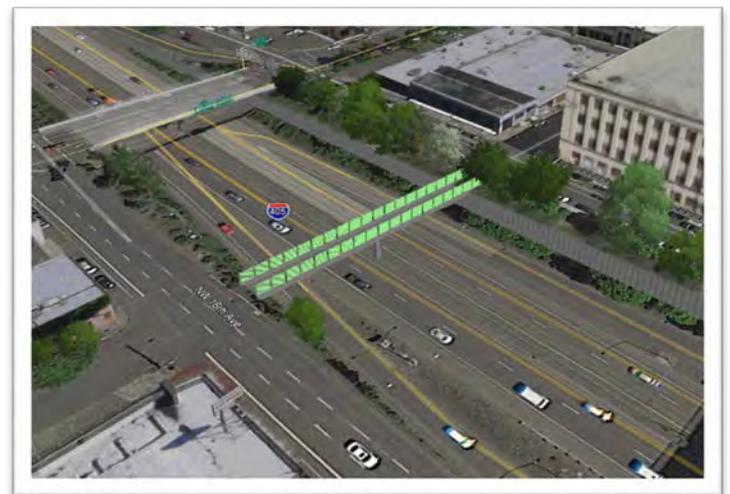
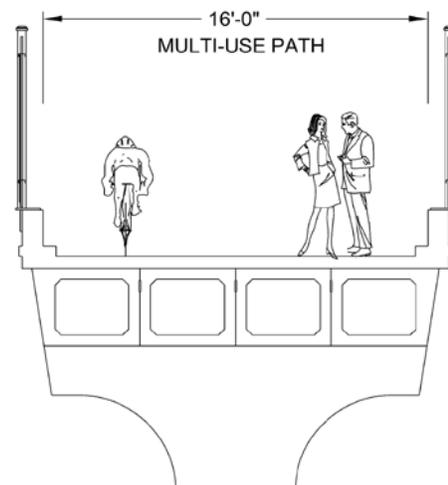
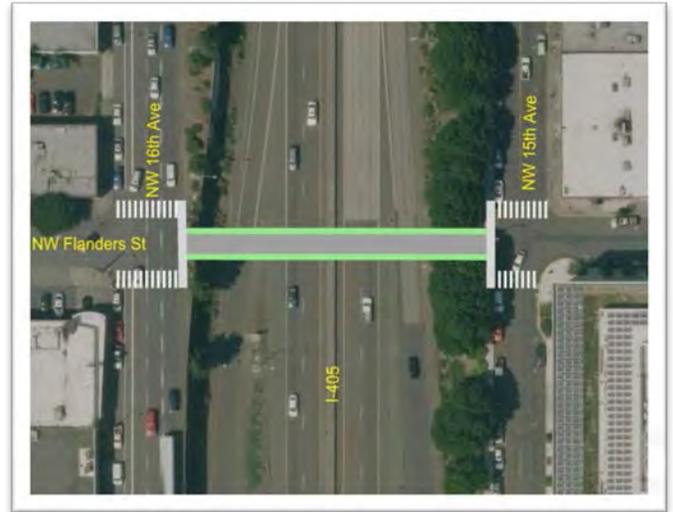
# ALTERNATIVE A

# Flanders Crossing | Alternative B

## 16 FT WIDE TWO SPAN CONCRETE BRIDGE

**ESTIMATED COST: \$4,166,155**

EVALUATION CRITERIA		DETAILS
PEOPLE	Bridge type embraces Vision Zero transportation safety principles	Narrowest facility studied. Combines bicycles and pedestrians in both directions.
	Bridge type reduces construction impacts on community (e.g. delays, congestion, emissions, and noise)	Requires pier construction in freeway median. Results in higher impact during construction.
	Bridge type provides positive user experience and/or valuable use of public space	Provides little room for public use. Primarily a transportation facility.
PLANET	Bridge type provides aesthetic value	Fits context of locale. Does not increase aesthetic value.
	Bridge type limits carbon footprint and greenhouse gas emissions during construction	Relatively small footprint limits greenhouse gas emissions.
	Bridge type preserves local character	Design similar to Everett and Glisan bridges nearby.
PROSPERITY	Bridge type Limits initial project construction cost and risk	\$4,166,155 Moderate risk due to unconventional construction methods needed to construct pier.
	Bridge type minimizes future operation and maintenance costs	Inspection would use routine methods. Maintenance costs would be low.
	Bridge type provides capacity for sustainable growth	Narrowest bridge alternative. Provides least capacity for users.





*Existing concrete bridge on Burnside Street adjacent to proposed crossing*



*Typical protective Portland Screen installed on bridges over I-405*

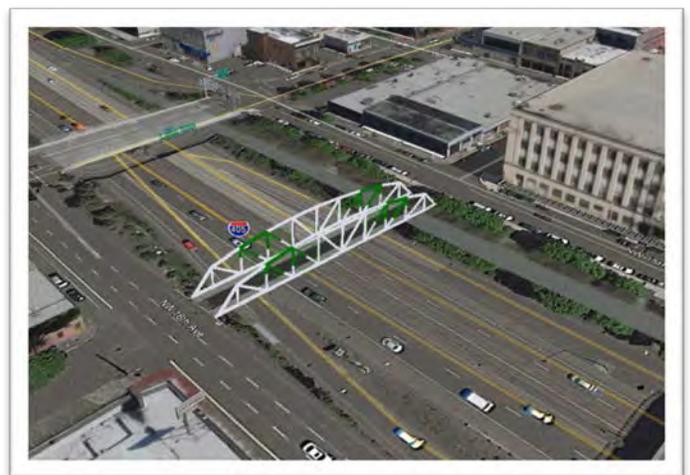
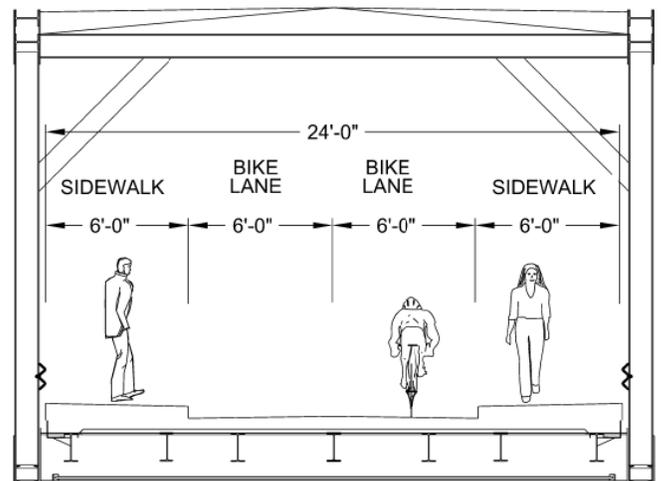
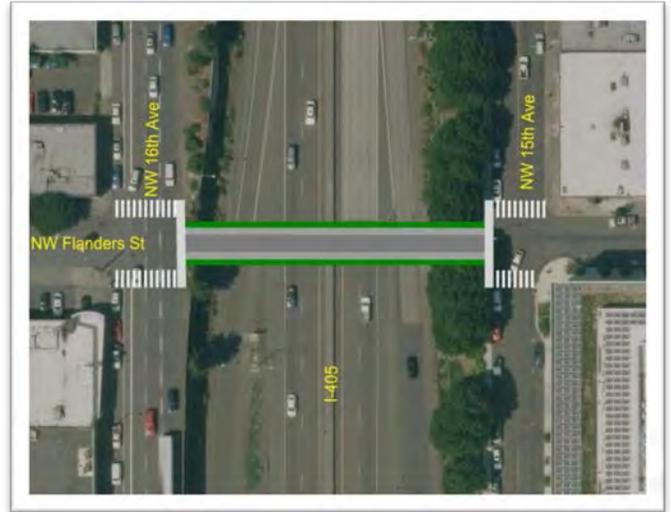
**ALTERNATIVE B**

# Flanders Crossing | Alternative C

## 24 FT WIDE SINGLE SPAN CUSTOM STEEL BRIDGE

**ESTIMATED COST: \$6,009,656**

EVALUATION CRITERIA		DETAILS
PEOPLE	Bridge type embraces Vision Zero transportation safety principles	Reduces conflicts by separating bicycles and pedestrians direction.
	Bridge type reduces construction impacts on community (e.g. delays, congestion, emissions, and noise)	Prefabricated single span would have least amount of impacts during construction.
	Bridge type provides positive user experience and/or valuable use of public space	Provides little room for public gathering space. Bracing members overhead diminish user experience.
PLANET	Bridge type provides aesthetic value	Semi-industrial look and feel. Compatible with warehouses nearby. Custom bridge options possible.
	Bridge type limits carbon footprint and greenhouse gas emissions during construction	Moderate footprint. Steel material has low carbon footprint.
	Bridge type preserves local character	Moderate footprint. Open structure design.
PROSPERITY	Bridge type Limits initial project construction cost and risk	\$6,009,656 Relatively low risk due to conventional construction methods.
	Bridge type minimizes future operation and maintenance costs	Re-painting of steel elements required every 30-40 years.
	Bridge type provides capacity for sustainable growth	Provides moderate capacity for future growth.





The custom steel bridge allows for artistic flare and improved aesthetics



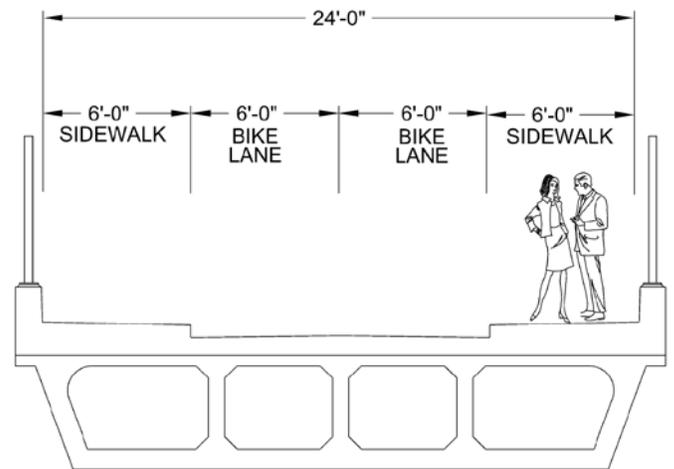
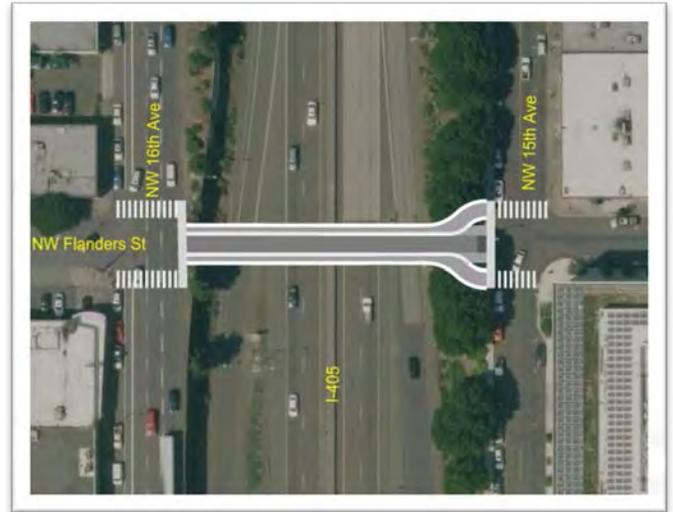
The proposed layout of Alternative C would separate bikes and pedestrians for safety

# Flanders Crossing | Alternative D

## 24 FT WIDE SINGLE SPAN CABLE-STAYED BRIDGE

**ESTIMATED COST: \$6,917,414**

EVALUATION CRITERIA		DETAILS
PEOPLE	Bridge type embraces Vision Zero transportation safety principles	Reduces conflicts by separating bicycles and pedestrians each direction.
	Bridge type reduces construction impacts on community (e.g. delays, congestion, emissions, and noise)	Construction impacts would be relatively high given complexity and scale.
	Bridge type provides positive user experience and/or valuable use of public space	Provides little room for public use, but aesthetic appeal could serve as gathering point.
PLANET	Bridge type provides aesthetic value	Iconic design would provide aesthetic focal point for location.
	Bridge type limits carbon footprint and greenhouse gas emissions during construction	Materials quantities required would result in higher greenhouse gas emissions.
	Bridge type preserves local character	Bridge may distract from local character.
PROSPERITY	Bridge type Limits initial project construction cost and risk	\$6,917,414 High risk due to complex design & specialized contractor needed.
	Bridge type minimizes future operation and maintenance costs	Inspection of cables would require special inspection equipment and methods.
	Bridge type provides capacity for sustainable growth	Attractive design and moderate width could provide capacity for future growth.





*Cable-Stayed Example | The recently built pedestrian bridge over I-5 in Eugene*



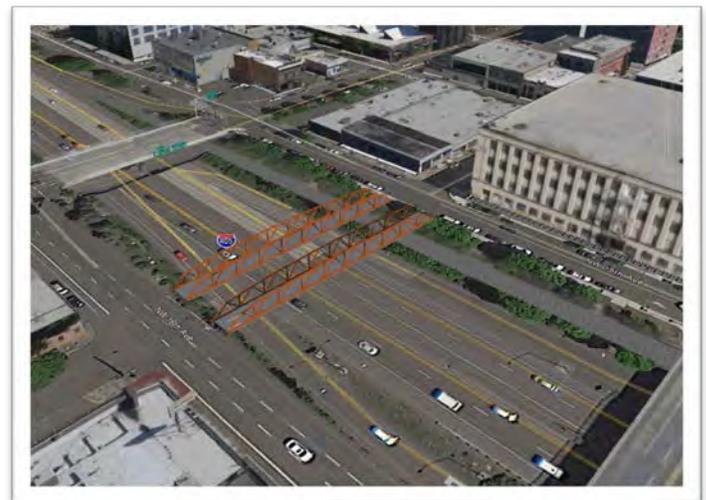
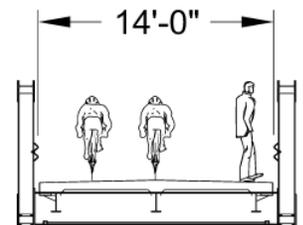
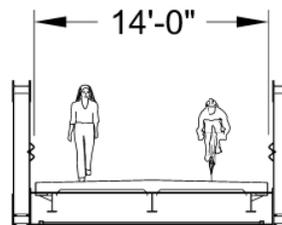
*The look, feel and operation of Alternative D would be similar to the Tillikum Crossing: Bridge of the People*

# Flanders Crossing | Alternative E

## 14 FT WIDE SINGLE SPAN PARALLEL TRUSS BRIDGES

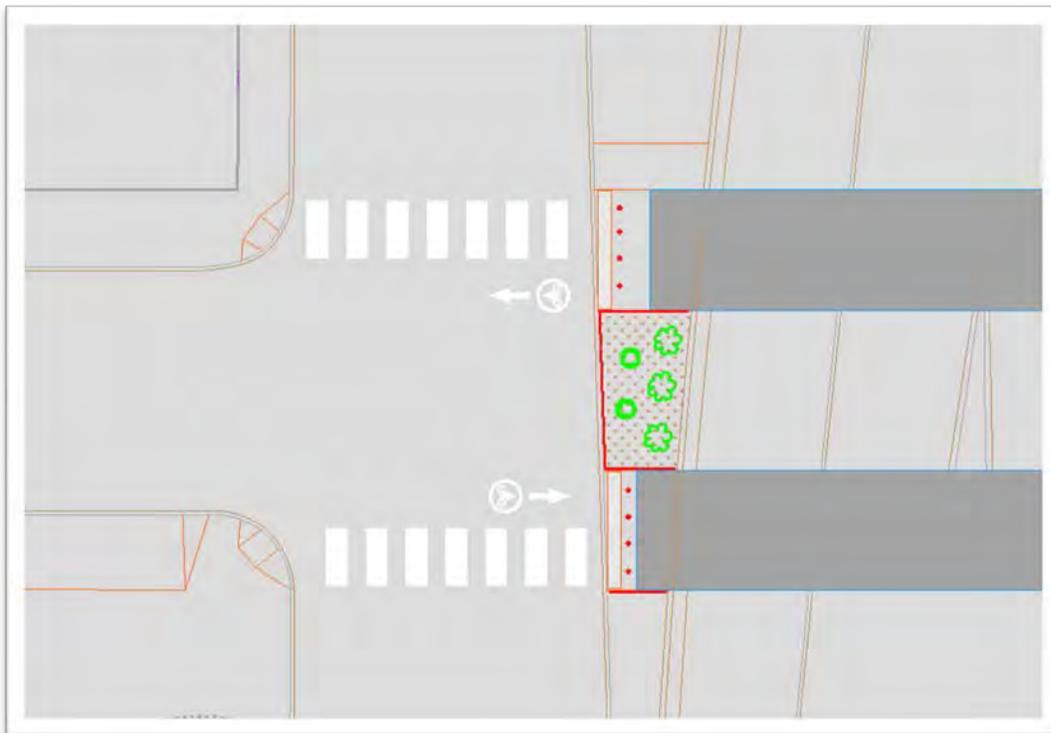
**ESTIMATED COST: \$6,493,151**

EVALUATION CRITERIA		DETAILS
PEOPLE	Bridge type embraces Vision Zero transportation safety principles	Separates bicycle traffic by direction. Safest alternative for users. Aligns with sidewalks each end.
	Bridge type reduces construction impacts on community (e.g. delays, congestion, emissions, and noise)	Construction impacts are moderate given dual bridge configuration.
	Bridge type provides positive user experience and/or valuable use of public space	Provides little room for public use. Primarily a transportation facility. No truss members overhead.
PLANET	Bridge type provides aesthetic value	Industrial look and feel. Compatible with warehouses nearby. Conventional truss design.
	Bridge type limits carbon footprint and greenhouse gas emissions during construction	Steel material has low carbon footprint. Dual bridge layout means twice the carbon footprint.
	Bridge type preserves local character	Dual bridges to be built rather than one. Open truss design.
PROSPERITY	Bridge type Limits initial project construction cost and risk	\$6,493,151 Conventional design concepts employed which reduce financial risk.
	Bridge type minimizes future operation and maintenance costs	Re-painting of truss elements required every 30-40 years.
	Bridge type provides capacity for sustainable growth	Provides enhanced capacity for future growth.





Artist rendering of the parallel bridges proposed for Alternative E, facing east



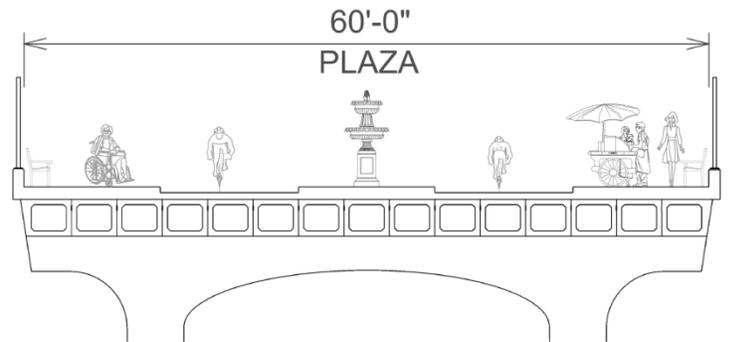
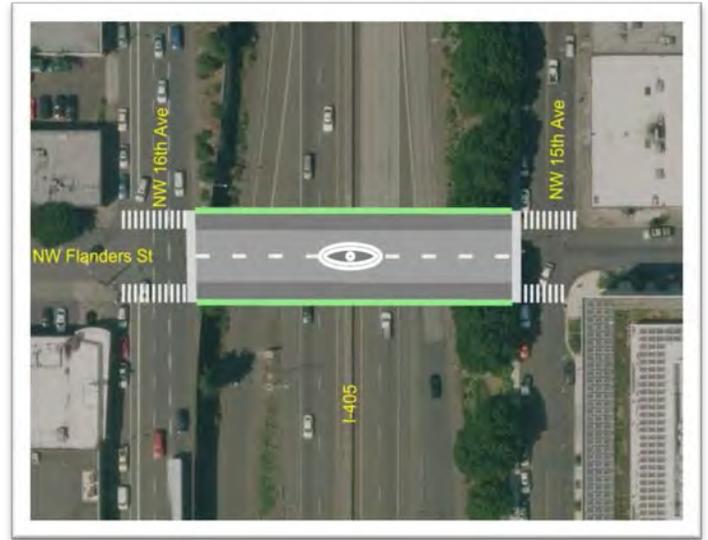
Sketch illustrating the ease of bridge alignment with connecting sidewalks for Alternative E

# Flanders Crossing | Alternative F

## 60 FT WIDE TWO SPAN CONCRETE PLAZA BRIDGE

**ESTIMATED COST: \$8,123,937**

EVALUATION CRITERIA		DETAILS
PEOPLE	Bridge type embraces Vision Zero transportation safety principles	Separates bicycles and pedestrians. Could result in conflicts of event related traffic. Aligns with sidewalks each end.
	Bridge type reduces construction impacts on community (e.g. delays, congestion, emissions, and noise)	Construction impacts are very high due to pier in freeway below and overall width.
	Bridge type provides positive user experience and/or valuable use of public space	Provides public gathering space in addition to transportation use. Possible use for civic events.
PLANET	Bridge type provides aesthetic value	Fits context of locale. Potential for public art.
	Bridge type limits carbon footprint and greenhouse gas emissions during construction	Largest bridge alternative results in largest carbon footprint and greenhouse gas emissions.
	Bridge type preserves local character	Widest bridge changes current views. Overall design is similar to adjacent bridges.
PROSPERITY	Bridge type Limits initial project construction cost and risk	\$8,123,937 Substructure requires accelerated construction technology to limit community impacts.
	Bridge type minimizes future operation and maintenance costs	Inspection would use routine methods. Maintenance costs could be moderate due to size.
	Bridge type provides capacity for sustainable growth	Provides highest capacity for growth. Plaza feel may stimulate local development.





*Artist rendering showing similar park or plaza concept over I-70 in Denver, CO*



*An example of community plaza use for Brunch on the Hawthorne Bridge in Portland*

## Cost Estimates

Estimated total project costs were generated for each of the six alternatives studied in order to inform stakeholders and project decision-makers of the anticipated bottom line. The cost estimates include the following components:

- Preliminary Engineering
- Construction
- Project Management
- Construction Engineering

The total project cost includes the cost to construct the bridge, in addition to estimated associated site development costs. Also included in the estimate are line items for a new traffic signal at NW 16<sup>th</sup> Ave. and a new rapid flash beacon at NW 14<sup>th</sup> Ave.

Preliminary Engineering figures were adjusted for the anticipated financial risk of each alternative considered. The base rate was based on the PBOT Civil Design Section’s template.

Quantities were measured from the conceptual drawings developed during alternatives analysis.

Unit prices for construction were based on similar projects recently built, ODOT historic average bid prices, and the PBOT Civil Design Section’s cost estimate template. Prefabricated truss costs were developed after consultation with a national truss manufacturer. Additionally, crane and rigging costs were created in consultation with a regional crane company.

See below for a summary of estimated total project costs for each alternative. A breakdown of each estimate is included in the Appendix.

Bridge Type	Cost Estimate
<b>ALTERNATIVE A</b> 16 FT WIDE SINGLE SPAN STANDARD TRUSS BRIDGE	<b>\$4,046,606</b>
<b>ALTERNATIVE B</b> 16 FT WIDE TWO SPAN CONCRETE BRIDGE	<b>\$4,166,155</b>
<b>ALTERNATIVE C</b> 24 FT WIDE SINGLE SPAN CUSTOM STEEL BRIDGE	<b>\$6,009,656</b>
<b>ALTERNATIVE D</b> 24 FT WIDE SINGLE SPAN CABLE-STAYED BRIDGE	<b>\$6,917,414</b>
<b>ALTERNATIVE E</b> 14 FT WIDE SINGLE SPAN PARALLEL TRUSS BRIDGES	<b>\$6,493,151</b>
<b>ALTERNATIVE F</b> 60 FT WIDE TWO SPAN CONCRETE PLAZA BRIDGE	<b>\$8,123,937</b>

## Alternatives Evaluation Summary

PBOT assembled multi-disciplinary Technical and Citizen Advisory Committees to help evaluate the six bridge alternatives. These committees included transportation engineers and planners from both PBOT and ODOT, as well as community activists and property development professionals.

The Evaluation Criteria used by evaluators were based on the study’s Guiding Principles and Objectives. An Alternatives Evaluation Worksheet (see Appendix) was developed for use in the evaluation of each alternative. The worksheet included all Evaluation Criteria and a summary of information for each bridge type. The scoring on the worksheet was based on a 5 point rating system as follows:

- 1 = Very Poor
- 2 = Poor
- 3 = Fair
- 4 = Good
- 5 = Very Good

During each evaluation meeting, participants were asked to complete the Alternatives Evaluation Worksheets, sum the rating scores for each alternative and report on the highest rated and lowest rated bridge alternative.

Bridge Type	1st Place Ranking Distribution
<b>ALTERNATIVE A</b> 16 FT WIDE SINGLE SPAN STANDARD TRUSS BRIDGE	3
<b>ALTERNATIVE B</b> 16 FT WIDE TWO SPAN CONCRETE BRIDGE	0
<b>ALTERNATIVE C</b> 24 FT WIDE SINGLE SPAN CUSTOM STEEL BRIDGE	17
<b>ALTERNATIVE D</b> 24 FT WIDE SINGLE SPAN CABLE-STAYED BRIDGE	0
<b>ALTERNATIVE E</b> 14 FT WIDE SINGLE SPAN PARALLEL TRUSS BRIDGES	4
<b>ALTERNATIVE F</b> 60 FT WIDE TWO SPAN CONCRETE PLAZA BRIDGE	2

## Preferred Alternative

Comparing the results of the multi-disciplined evaluation team of internal and external stakeholders, the preferred bridge is **ALTERNATIVE C – 24 FT WIDE SINGLE SPAN CUSTOM STEEL BRIDGE.**

The preferred alternative combines optimal active user width (24 feet clear) a single span over the freeway and tried and true construction methods. The steel materials used in the primary bridge elements exhibit a generally low carbon footprint during construction. The bridge type limits impacts to the motoring public during construction, given the ability to set the bridge in place in one overnight over a limited term freeway closure. Additionally, the bridge can be customized to suit the aspirations of the local community and provide a pleasing aesthetic feature over an urban interstate. Lastly, the configuration can be planned to allow for shared vehicular traffic for emergency responders and active users during post-earthquake recovery operations.

## SEISMIC RESILIENCY

The physical condition and status of bridges in the Portland area following an earthquake becomes a paramount factor in determining routes for emergency services, government operations, and the safe mobility of the general public. This recognition rings especially true for the Pearl District, bounded by the I-405 freeway. The bridges and overpasses that support or span I-405 were built prior to current seismic design standards. A new, seismically resilient crossing over I-405 could be achieved through the construction of the Flanders Crossing active bridge. The bridge could be designed to be used by vehicles after a seismic event. Each bridge alternative would function at different levels due to their type and geometry. See below for information on how each bridge alternative could be used after an earthquake.



Bridge Type	Earthquake Recovery Operation
<b>ALTERNATIVE A</b> 16 FT WIDE SINGLE SPAN STANDARD TRUSS BRIDGE	May accommodate only vehicles in earthquake recovery due to narrow width (16 ft). It does not appear likely that there is enough room for both emergency vehicles and active users.
<b>ALTERNATIVE B</b> 16 FT WIDE TWO SPAN CONCRETE BRIDGE	May accommodate only vehicles in earthquake recovery due to narrow width (16 ft). It does not appear likely that there is enough room for both emergency vehicles and active users. (Same as Alternative A)
<b>ALTERNATIVE C</b> 24 FT WIDE SINGLE SPAN CUSTOM STEEL BRIDGE	Will accommodate both active and vehicle use in earthquake recovery. Active users could share sidewalks, while vehicles use the 12 ft. clear space in the center of the bridge.
<b>ALTERNATIVE D</b> 24 FT WIDE SINGLE SPAN CABLE-STAYED BRIDGE	May not be able to accommodate vehicle use in earthquake recovery due to steel cable and concrete tower configuration.
<b>ALTERNATIVE E</b> 14 FT WIDE SINGLE SPAN PARALLEL TRUSS BRIDGES	Could accommodate both active users and vehicles in earthquake recovery. Modal splits between bridges are an option also.
<b>ALTERNATIVE F</b> 60 FT WIDE TWO SPAN CONCRETE PLAZA BRIDGE	Would provide the most space for active users and vehicles in earthquake recovery due to width (60 ft.). Two –way traffic for all modes could be safely accommodated.

# APPENDIX

## Additional information

### Right of Way

It is anticipated that all temporary and permanent project actions will take place within the existing public right of way. The surrounding streets appear to be City of Portland right of way. It is understood at this time that the freeway and parallel retaining walls are located in ODOT right of way. The bridge ends would be located near the interface between PBOT and ODOT's rights of way. Further investigation will be necessary during future project phases to determine which parts of the bridge are proposed in City of Portland vs. ODOT right of way.

**ODOT and FHWA:** The majority of any bridge configuration would be built in ODOT's right of way. In these locations, it is understood that, at a minimum, a Maintenance Agreement would be necessary to identify which jurisdiction is responsible for maintenance and operations activities of each bridge component.

Additionally, ODOT has had preliminary conversations the local FHWA division office to determine whether or not FHWA approval is required for a bridge to be constructed over I-405 at this location. FHWA has initially determined that ODOT has the authority to approve the proposed bridge over I-405. FHWA's expectation is that ODOT and the City enter into an agreement that allows the use of the Interstate airspace for the purpose of a bicycle and pedestrian facility. The execution of the agreement by ODOT would be subject to FHWA's review.

## Environmental Permitting

In many transportation projects, environmental concerns can be a central component of the project. Given the current planned scope for a new bridge at the project location and lack of natural environment resources, environmental impacts are anticipated to be relatively minor. However insignificant the impacts, the National Environmental Policy Act (NEPA) process may be required if a federal nexus is created. This typically happens by means of the funding source and/or required permits. Given that the federal-aid project funding is not currently being pursued and an FHWA interstate access permit approval does not appear to be required for the new crossing over the interstate, the NEPA process may not be necessary. It is generally a best practice to identify the need for NEPA as early in the project development as possible, given the duration of the process and the fact that coordination with multiple state and federal agencies is usually required. Several common federal/NEPA permits required for transportation projects are listed below. Based on the information available at this time, a description of possible permit effects on the project are described:

### Potential Environmental Permits

Permit	Expected Applicability	Expected Outcome (if required)
NEPA	Not Likely	Categorical Exclusion
Endangered Species Act	Likely	No Effect
Section 106 (NHPA)	Not Likely	No Historic Properties Adversely Affected
Migratory Bird Treaty Act	Required	No Action
CWA Section 404	Not Likely	-
Oregon Removal-Fill Permit	Not Likely	-
COP Environmental Review	Not Likely	-

**Zoning:** The existing area surrounding the project site is largely developed and is zoned **EXdCC**. The meaning of the zoning symbols is as follows:

- **EX** stands for Central Employment and “allows mixed-uses,” according to the City of Portland’s zone code, Title 33. It continues to state that “the development standards are intended to allow new development which is similar in character to existing development.”
- **d** indicates that the project falls within a Design Overlay Zone. According to Title 33, “The Design Overlay Zone is applied to areas where design and neighborhood character are of special concern.” The project may be exempt from design review because it will not require a building or sign permit (33.420.045.J).
- **CC** shows at least part of the project is located in the Central City plan district. Subsequently, the regulation maps appear to show the east end of the project within the boundaries of the River District Plan.

**Summary:** Overall it appears that the permitting process for the project would be relatively straightforward. Lack of natural resources and the existing highly developed urban environment should result in a low risk for permits to impact any project goals or objectives.

## Traffic + Mobility

The proposed Flanders Crossing would tie into the existing street network on NW 15<sup>th</sup> Ave. and NW 16<sup>th</sup> Ave. Given the new access point in the existing system, signal modification and installation would likely be necessary and may impact traffic flow and operations in the area.

### NW 16<sup>th</sup> Avenue

To provide a safe crossing for bicycles and pedestrians to and from the bridge, a new traffic signal is proposed at NW 16<sup>th</sup> Ave. and NW Flanders St. The signal may have impacts on traffic patterns in the area, including the I-405 southbound off-ramp at NW Glisan St. The PBOT Traffic Design Section is analyzing the impacts and is consulting with ODOT on the signal warrant and operation. The findings of the signal analysis will be published separately from this study.

### NW 15<sup>th</sup> Avenue

It is our understanding that traffic volumes are relatively low at the intersection of NW Flanders St. and NW 15<sup>th</sup> Ave. (east end of proposed bridge). While a crossing treatment and/or intersection control at this location may be warranted, it is unlikely that a signal of any kind is necessary. The PBOT Traffic Design Section is conducting an analysis of this intersection and will publish its findings separately from this study.

### NW 14<sup>th</sup> Avenue

Additionally, a crossing treatment may be warranted at NW Flanders St. and NW 14<sup>th</sup> Ave. The PBOT Traffic Design Section is currently working to determine if a rapid flash beacon or full traffic signal is warranted at that location.

## Roadway + Civil

The roadway portion of the project would be very limited in scope. The work would be confined to the general alignment of the bridge and local improvements required to accommodate bicycles and pedestrians.

**Bridge Alignment:** In order to best accommodate bicyclists and pedestrian expectations and safety, an alignment along the centerline of NW Flanders St. appears the most desirable for the majority of bridge alternatives. The centerline alignment would best accommodate bicycle movements which occur in the street. This would result in a small amount of out of direction travel for pedestrians in the narrower bridge concepts. However, if companion structures are built or the bridge is built out to the full width of the right of way, they can be separated and aligned closely with the sidewalks on NW Flanders St. to accommodate safer pedestrian crossings. This layout would not require any out of direction travel for pedestrians.

**Bridge Grade:** Based on preliminary data, the bridge longitudinal grade would be in the 2.0% to 2.75% range, depending on selected curb ramp style. This grade appears to meet requirements for stormwater conveyance/drainage and ADA standards.

**Stormwater and Drainage:** The stormwater would be transported across the bridge from west to east and discharged into the existing gutter on NW 15<sup>th</sup> Ave., or into a new stormwater facility if required. In preliminary discussions with BES, it was determined that a conventional swale or planter stormwater management facility would not be feasible in this location. If a facility is feasible, it would be lined due to its proximity to the existing ODOT retaining walls along I-405. If the facility is determined infeasible, an in-lieu of fee may be required by BES.

**ADA Accessibility:** All aspects of the project should comply with the Americans With Disabilities Act (ADA). The ADA most often applies to accessibility in the public right of way and affects curb ramp requirements. In terms of the bridge, the following project components are expected to be impacted by ADA:

- **Curb ramps:** Curb ramps will be required at each end of the bridge. The curb ramps need to be located and design with maximum grades which meet ADA requirements. The bridge concepts could incorporate said curb ramps into the design by means of a “driveway” section in the sidewalk. Future refinement will be required in consultation with PBOT’s ADA Coordinator to ensure that the design is in compliance with ADA standards.
- **Longitudinal grade:** The proposed longitudinal bridge grade in the 2% to 2.75% range is less than the maximum 5% grade recommended by the ADA; therefore, intermittent landings would not be required.
- **Bridge Width:** All bridge concepts provide an accessible surface wider than 60 inches and would meet any ADA clear throughway width requirements.
- **Alignment with existing sidewalks:** Further analysis is warranted to determine ideal location for curb ramps and any required mitigation to provide for appropriate crossing of NW 16<sup>th</sup> Ave. for sight impaired pedestrians. In general, the wider bridge options or companion structures appear to provide the most ideal alignments with existing sidewalks for the intents and purposes of ADA. Crosswalk alignment could be mitigated with the installation channelizing handrails that lead pedestrians to perpendicular curb ramps.

BRIDGE TYPES	EVALUATION CRITERIA									TOTAL SCORE
	PEOPLE			PLANET			PROSPERITY			
	Bridge type embraces Vision Zero transportation safety principles	Bridge type reduces construction impacts on community (e.g. delays, congestion, emissions, and noise)	Bridge type provides positive user experience and/or valuable use of public space	Bridge type provides aesthetic value	Bridge type limits carbon footprint and greenhouse gas emissions during construction	Bridge type preserves local character	Bridge type Limits initial project construction cost and risk	Bridge type minimizes future operation and maintenance costs	Bridge type provides capacity for sustainable growth	
<b>ALTERNATIVE A</b> 16 FT WIDE SINGLE SPAN STANDARD TRUSS BRIDGE	Narrowest facility studied. Combines bicycles and pedestrians in both directions.	Prefabricated single span would have least amount of impacts during construction.	Provides little room for public use. Primarily a transportation facility..	Industrial look and feel. Compatible with warehouses nearby. Conventional truss design.	Small footprint. Steel material has low carbon footprint.	Smallest footprint considered. Open truss design.	\$4,046,606 Conventional construction methods result in low risk.	Re-painting of truss elements required every 30-40 years.	Narrowest bridge alternative. Provides least capacity for users.	
<b>ALTERNATIVE B</b> 16 FT WIDE TWO SPAN CONCRETE BRIDGE	Narrowest facility studied. Combines bicycles and pedestrians in both directions.	Requires pier construction in freeway median. Results in higher impact during construction.	Provides little room for public use. Primarily a transportation facility.	Fits context of locale. Does not increase aesthetic value.	Relatively small footprint limits greenhouse gas emissions.	Design similar to Everett and Glisan bridges nearby.	\$4,166,155 Moderate risk due to unconventional construction methods needed to construct pier.	Inspection would use routine methods. Maintenance costs would be low.	Narrowest bridge alternative. Provides least capacity for users.	
<b>ALTERNATIVE C</b> 24 FT WIDE SINGLE SPAN CUSTOM STEEL BRIDGE	Reduces conflicts by separating bicycles and pedestrians direction.	Prefabricated single span would have least amount of impacts during construction.	Provides little room for public gathering space. Bracing members overhead diminish user experience.	Semi-industrial look and feel. Compatible with warehouses nearby. Custom bridge options possible.	Moderate footprint. Steel material has low carbon footprint.	Moderate footprint. Open structure design.	\$6,009,656 Relatively low risk due to conventional construction methods.	Re-painting of steel elements required every 30-40 years.	Provides moderate capacity for future growth.	
<b>ALTERNATIVE D</b> 24 FT WIDE SINGLE SPAN CABLE-STAYED BRIDGE	Reduces conflicts by separating bicycles and pedestrians each direction.	Construction impacts would be relatively high given complexity and scale.	Provides little room for public use, but aesthetic appeal could serve as gathering point.	Iconic design would provide aesthetic focal point for location.	Materials quantities required would result in higher greenhouse gas emissions.	Bridge may distract from local character.	\$6,917,414 High risk due to complex design & specialized contractor needed.	Inspection of cables would require special inspection equipment and methods.	Attractive design and moderate width could provide capacity for future growth.	
<b>ALTERNATIVE E</b> 14 FT WIDE SINGLE SPAN PARALLEL TRUSS BRIDGES	Separates bicycle traffic by direction. Safest alternative for users. Aligns with sidewalks each end.	Construction impacts are moderate given dual bridge configuration.	Provides little room for public use. Primarily a transportation facility. No truss members overhead.	Industrial look and feel. Compatible with warehouses nearby. Conventional truss design.	Steel material has low carbon footprint. Dual bridge layout means twice the carbon footprint.	Dual bridges to be built rather than one. Open truss design.	\$6,493,151 Conventional design concepts employed which reduce financial risk.	Re-painting of truss elements required every 30-40 years.	Provides enhanced capacity for future growth.	
<b>ALTERNATIVE F</b> 60 FT WIDE TWO SPAN CONCRETE PLAZA BRIDGE	Separates bicycles and pedestrians. Could result in conflicts of event related traffic. Aligns with sidewalks each end.	Construction impacts are very high due to pier in freeway below and overall width.	Provides public gathering space in addition to transportation use. Possible use for civic events.	Fits context of locale. Potential for public art.	Largest bridge alternative results in largest carbon footprint and greenhouse gas emissions.	Widest bridge changes current views. Overall design is similar to adjacent bridges.	\$8,123,937 Substructure requires accelerated construction technology to limit community impacts.	Inspection would use routine methods. Maintenance costs could be moderate due to size.	Provides highest capacity for growth. Plaza feel may stimulate local development.	

Rate each criterion for each alternative based on the following rating scale:

- 1 = VERY POOR
- 2 = POOR
- 3 = FAIR
- 4 = GOOD
- 5 = VERY GOOD

Enter your SCORE in each box:

Add scores in each row for TOTAL SCORE

Evaluator Name \_\_\_\_\_



**CITY OF PORTLAND, OREGON  
BUREAU OF TRANSPORTATION**

Date: November 17, 2015

By: C. Glasgow

FEASIBILITY STUDY ENGINEER'S ESTIMATE FOR THE IMPROVEMENT OF FLANDERS CROSSING - ALTERNATIVE "A" - 16 FT TRUSS BRIDGE

VALUES IN BLUE ARE PERCENT OF CONTRACT.

**##### BID ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	MOBILIZATION	0210	0100000A	n/a	LS	1.00	\$ 166,286.61	\$ 166,286.61
2	TEMPORARY PROTECTION & DIRECTION OF TRAFFIC	0225	0100000A	13	LS	1.00	\$ 33,257.32	\$ 33,257.32
	TEMPORARY TRAFFIC CONTROL DEVICES	225	Special		LS	1.00	\$ 100,000.00	\$ 100,000.00
22	TEMPORARY CL-6R CHAIN LINK FENCE	0270	0137000F	12	FOOT	360.00	\$ 17.60	\$ 6,336.00
23	EROSION CONTROL	0280	0100000A	11	LS	1.00	\$ 16,628.66	\$ 16,628.66
28	SEDIMENT FENCE, UNSUPPORTED	0280	0113000F	11	FOOT	280.00	\$ 2.50	\$ 700.00
29	INLET PROTECTION	0280	0114000E	11	EACH	12.00	\$ 88.00	\$ 1,056.00
30	POLLUTION CONTROL PLAN	0290	0100000A	12	LS	1.00	\$ 1,662.87	\$ 1,662.87
33	HASP/CMDP WORKPLANS	0291	1105000A	12	LS	1.00	\$ 1,000.00	\$ 1,000.00
43	REMOVAL OF STRUCTURES & OBSTRUCTIONS	0310	0106000A	1	LS	1.00	\$ 10,000.00	\$ 10,000.00
45	CLEARING AND GRUBBING	0320	0100000A	1	LS	1.00	\$ 20,000.00	\$ 20,000.00
49	GENERAL EXCAVATION	0330	0105000K	1	CUYD	50.00	\$ 35.00	\$ 1,750.00
66	TRENCH EXCAVATION, COMMON	0405	1101000K	1	CUYD	100.00	\$ 16.70	\$ 1,670.00
70	TRENCH BACKFILL, CLASS B	0405	1109000K	1	CUYD	75.00	\$ 33.00	\$ 2,475.00
82	10 INCH PIPE, HDPE ASTM F714 SDR 26 BEDDING TYPE:D, COMPLETE	0445	Special	1	FOOT	100.00	\$ 122.00	\$ 12,200.00
100	CONCRETE INLETS, TYPE G-2	0470	0315000E	1	EACH	2.00	\$ 1,770.00	\$ 3,540.00
112	CONNECTION TO EXISTING STRUCTURES	0490	0104000E	1	EACH	2.00	\$ 841.00	\$ 1,682.00
118	TRENCH RESURFACING	0495	0100000J	1	SQYD	33.33	\$ 109.00	\$ 3,632.97
	STRUCTURE DEMOLITION	0501	Special		LS	1.00	\$ 15,000.00	\$ 15,000.00
120	SHORING, CRIBBING AND COFFERDAMS	0510	0100000A	2	LS	1.00	\$ 5,000.00	\$ 5,000.00
121	STRUCTURE EXCAVATION	0510	0101000K	2	CUYD	40.00	\$ 48.30	\$ 1,932.00
123	GRANULAR STRUCTURAL BACKFILL	0510	0108000K	2	CUYD	50.00	\$ 50.00	\$ 2,500.00
	FURNISH DRILLING EQUIPMENT		Special		LS	1.00	\$ 30,000.00	\$ 30,000.00
	PERMANENT SHAFT CASINGS		Special		FOOT	200.00	\$ 500.00	\$ 100,000.00
	CSL TEST ACCESS TUBES	0512	0105000F		FOOT	600.00	\$ 8.00	\$ 4,800.00
	CSL TESTS	0512	0106000E		EACH	4.00	\$ 1,500.00	\$ 6,000.00
	DRILLED SHAFT EXC, 36 INCH DIA	0512	0110000F		FOOT	200.00	\$ 250.00	\$ 50,000.00
	DRILLED SHAFT CONCRETE	0512	Special		CUYD	55.00	\$ 500.00	\$ 27,500.00
	DRILLED SHAFT REINFORCEMENT	0530	Special		LB	20000.00	\$ 1.00	\$ 20,000.00
124	REINFORCEMENT	0530	0100000A	2	LS*	1.00	\$ 11,400.00	\$ 11,400.00
	FOUNDATION CONCRETE	0540	0111000K		CUYD	50.00	\$ 600.00	\$ 30,000.00
	DECK CONCRETE, CLASS HPC4000	0540	0207000K		CUYD	55.00	\$ 1,000.00	\$ 55,000.00
	GENERAL STRC CONCRETE, CLASS 4000	0540	0312000K		CUYD	40.00	\$ 1,500.00	\$ 60,000.00
	PREFABRICATED BRIDGE, DELIVERED	0561	Special		LS	1.00	\$ 763,198.00	\$ 763,198.00
	PREFABRICATED BRIDGE INSTALLATION	0561	Special		LS	1.00	\$ 30,000.00	\$ 30,000.00
	BRIDGE LIGHTING	0580	Special		LS	1	\$ 100,000.00	\$ 100,000.00
	2 INCH ELECTRICAL CONDUIT	0583	0105000F		FOOT	544	\$ 10.00	\$ 5,440.00
	EXPANSION JOINTS	0585	Special		FOOT	36	\$ 250.00	\$ 9,000.00
	ORNAMENTAL BRIDGE RAIL	0587	Special		FOOT	80.00	\$ 250.00	\$ 20,000.00
138	CONCRETE SLOPE PAVING	0599	0100000J	2	SQFT	350.00	\$ 11.25	\$ 3,937.08
143	AGGREGATE BASE	0640	0102000M	5	TON	10.00	\$ 36.90	\$ 369.00
154	16 INCH ASPHALT CONCRETE PAVEMENT REPAIR	0748	Special		SQYD	20.00	\$ 68.80	\$ 1,376.00
168	CONCRETE DRIVEWAYS	0759	0126000J	12	SQFT	400.00	\$ 8.40	\$ 3,360.00
170	CONCRETE WALKS	0759	0128000J	12	SQFT	100.00	\$ 7.40	\$ 740.00
179	DETECTABLE WARNING SURFACE	0759	1158000J	12	SQFT	240.00	\$ 42.30	\$ 10,152.00
193	REMOVABLE BOLLARDS	0815	0101000E	12	EACH	26.00	\$ 750.00	\$ 19,500.00
	FREEWAY SIGNS MOUNTED ON BRIDGE		Special		LS	1.00	\$ 18,000.00	\$ 18,000.00
	PERMANENT SIGNING AND STRIPING		Special		LS	1.00	\$ 7,500.00	\$ 7,500.00
238	POLE FOUNDATIONS	0970	0100000A	10	LS*	3.00	\$ 2,000.00	\$ 6,000.00
243	TRAFFIC SIGNAL INSTALLATION	0990	0101000A	10	LS*	1.00	\$ 120,000.00	\$ 120,000.00
245	FLASHING BEACON INSTALLATION	0990	Special		LS*	1.00	\$ 40,000.00	\$ 40,000.00
246	LOOP DETECTOR INSTALLATION	0990	0103000A	10	LS*	1.00	\$ 9,120.00	\$ 9,120.00
	LANDSCAPING		Special		LS	1.00	\$ 40,000.00	\$ 40,000.00

\* Unit Price Shown is on Pound, Each, or Foot Basis as Applicable

TOTAL BID ITEMS

\$ 2,010,701.51

**##### ANTICIPATED ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	REFERENCE	NUMBER	OF	UNIT	QUANTITY	UNIT PRICE	AMOUNT
1	RIGHT OF WAY MONUMENTATION				LS	0.00	\$ -	\$ -
2	RELOCATE WATER FACILITIES - FIRE HYDRANT				EACH	0.00	\$ 20,000.00	\$ -
3	RELOCATE WATER FACILITIES - METER				EACH	0.00	\$ 6,000.00	\$ -
4	STREET LIGHTING - UPGRADE LUMINAIRES				EACH	0.00	\$ 600.00	\$ -
5	STREET LIGHTING - INSTALL ARMS AND LUMINAIRES				EACH	0.00	\$ 5,000.00	\$ -
6	CONNECT CONTRACTOR INSTALLED TRAFFIC SIGNAL LOOPS TO CONTROLLER BY BOM				EACH	0.00	\$ 1,000.00	\$ -
7	STORMWATER PLANTINGS AND PLANT ESTABLISHMENT				SQFT	0.00	\$ 15.00	\$ -

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
8	STORMWATER OFFSITE MANAGEMENT FEE				SQFT	4120.00	\$ 3.70	\$ 15,244.00
9	ROCK EXCAVATION				CUYD	0.00	\$ 106.00	\$ -
10	RAILROAD PROTECTION SERVICES (ONE YEAR)				LS	0.00	\$ 100,000.00	\$ -
11	ASPHALT CEMENT ESCALATION				LS	1.00	\$ -	\$ -
12	FUEL ESCALATION				LS	1.00	\$ -	\$ -
13	TESTING CONTAMINATED MEDIA				LS	0.00	\$ 5,000.00	\$ -
14	BOLI FEE PAYMENT				LS	1.00	\$ 2,010.70	\$ 2,010.70
15	CONTRACT CONTINGENCY (REQUIREMENT TO ACCEPT BIDS UP TO 10% OVER ESTIMATE)				LS	1.00	\$ 201,070.15	\$ 201,070.15

TOTAL ANTICIPATED ITEMS

\$ 218,324.85

**SCHEDULE SUMMARY**

BID ITEMS								\$ 2,010,702
CONSTRUCTION CONTINGENCY						5%	of Bid Items *	\$ 100,535
SUBTOTAL								\$ 2,111,237
ANTICIPATED ITEMS								\$ 218,325
<b>TOTAL CONSTRUCTION</b>								<b>\$ 2,329,561</b>
PROJECT MANAGEMENT						5%	of Bid Items	\$ 100,535
DESIGN ENGINEERING						15%	of Bid Items	\$ 301,605
CONSTRUCTION MANAGEMENT						15%	of Bid Items	\$ 301,605
SUBTOTAL								\$ 703,745
PROJECT ENGINEERING & MANAGEMENT OVERHEAD						73.35%	of PM, Eng, and CM	\$ 516,197
<b>TOTAL PROJECT ENGINEERING &amp; MANAGEMENT</b>								<b>\$ 1,219,942</b>
RIGHT-OF-WAY LAND, IMPROVEMENTS, AND DAMAGES								\$ -
RIGHT-OF-WAY APPRAISAL, TITLE INSURANCE, AND NEGOTIATION								\$ -
RIGHT-OF-WAY CONTINGENCY						30%	of Land, Improve, and Damages	\$ -
<b>TOTAL PROJECT RIGHT-OF-WAY</b>								<b>\$ -</b>
INFLATION RATE ON CONTRACT				Years		Inflation		\$ 104,830
INFLATION RATE ON PERSONNEL				1		4.5%	of Construction	\$ 24,399
ESTIMATE CONTINGENCY FOR UNDEFINED OR CHANGE IN SCOPE				1		2.0%	of Eng & Mgmt	\$ 367,873
						10%	of Const, Eng & Mgmt, and Inflation	\$ -
<b>TOTAL PROJECT CONTINGENCY</b>								<b>\$ 497,102</b>
<b>TOTAL PROJECT ESTIMATE</b>								<b>\$ 4,046,606</b>

**CITY OF PORTLAND, OREGON  
BUREAU OF TRANSPORTATION**

Date: November 17, 2015

By: C. Glasgow

FEASIBILITY STUDY ENGINEER'S ESTIMATE FOR THE IMPROVEMENT OF FLANDERS CROSSING - ALTERNATIVE "B" - 16 FT CONCRETE 2-SPAN BRIDGE

VALUES IN BLUE ARE PERCENT OF CONTRACT.

**##### BID ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	MOBILIZATION	0210	0100000A	n/a	LS	1.00	\$ 137,682.01	\$ 137,682.01
2	TEMPORARY PROTECTION & DIRECTION OF TRAFFIC	0225	0100000A	13	LS	1.00	\$ 27,536.40	\$ 27,536.40
	TEMPORARY TRAFFIC CONTROL DEVICES	225	Special		LS	1.00	\$ 300,000.00	\$ 300,000.00
22	TEMPORARY CL-6R CHAIN LINK FENCE	0270	0137000F	12	FOOT	360.00	\$ 17.60	\$ 6,336.00
23	EROSION CONTROL	0280	0100000A	11	LS	1.00	\$ 13,768.20	\$ 13,768.20
28	SEDIMENT FENCE, UNSUPPORTED	0280	0113000F	11	FOOT	280.00	\$ 2.50	\$ 700.00
29	INLET PROTECTION	0280	0114000E	11	EACH	16.00	\$ 88.00	\$ 1,408.00
30	POLLUTION CONTROL PLAN	0290	0100000A	12	LS	1.00	\$ 1,376.82	\$ 1,376.82
33	HASP/CMDP WORKPLANS	0291	1105000A	12	LS	1.00	\$ 1,000.00	\$ 1,000.00
43	REMOVAL OF STRUCTURES & OBSTRUCTIONS	0310	0106000A	1	LS	1.00	\$ 10,000.00	\$ 10,000.00
45	CLEARING AND GRUBBING	0320	0100000A	1	LS	1.00	\$ 20,000.00	\$ 20,000.00
49	GENERAL EXCAVATION	0330	0105000K	1	CUYD	50.00	\$ 35.00	\$ 1,750.00
66	TRENCH EXCAVATION, COMMON	0405	1101000K	1	CUYD	100.00	\$ 16.70	\$ 1,670.00
70	TRENCH BACKFILL, CLASS B	0405	1109000K	1	CUYD	75.00	\$ 33.00	\$ 2,475.00
82	10 INCH PIPE, HDPE ASTM F714 SDR 26 BEDDING TYPE:D, COMPLETE	0445	Special	1	FOOT	100.00	\$ 122.00	\$ 12,200.00
100	CONCRETE INLETS, TYPE G-2	0470	0315000E	1	EACH	2.00	\$ 1,770.00	\$ 3,540.00
112	CONNECTION TO EXISTING STRUCTURES	0490	0104000E	1	EACH	2.00	\$ 841.00	\$ 1,682.00
118	TRENCH RESURFACING	0495	0100000J	1	SQYD	33.33	\$ 109.00	\$ 3,632.97
	STRUCTURE DEMOLITION	0501	Special		LS	1.00	\$ 15,000.00	\$ 15,000.00
120	SHORING, CRIBBING AND COFFERDAMS	0510	0100000A	2	LS	1.00	\$ 5,000.00	\$ 5,000.00
121	STRUCTURE EXCAVATION	0510	0101000K	2	CUYD	40.00	\$ 48.30	\$ 1,932.00
123	GRANULAR STRUCTURAL BACKFILL	0510	0108000K	2	CUYD	50.00	\$ 50.00	\$ 2,500.00
	FURNISH DRILLING EQUIPMENT		Special		LS	1.00	\$ 40,000.00	\$ 40,000.00
	PERMANENT SHAFT CASINGS		Special		FOOT	250.00	\$ 500.00	\$ 125,000.00
	CSL TEST ACCESS TUBES	0512	0105000F		FOOT	750.00	\$ 8.00	\$ 6,000.00
	CSL TESTS	0512	0106000E		EACH	5.00	\$ 1,500.00	\$ 7,500.00
	DRILLED SHAFT EXC, 36 INCH DIA	0512	0110000F		FOOT	250.00	\$ 250.00	\$ 62,500.00
	DRILLED SHAFT CONCRETE	0512	Special		CUYD	70.00	\$ 500.00	\$ 35,000.00
	DRILLED SHAFT REINFORCEMENT	0530	Special		LB	25000.00	\$ 1.00	\$ 25,000.00
124	REINFORCEMENT	0530	0100000A	2	LS*	1.00	\$ 20,000.00	\$ 20,000.00
	FOUNDATION CONCRETE	0540	0111000K		CUYD	50.00	\$ 600.00	\$ 30,000.00
	DECK CONCRETE, CLASS HPC4000	0540	0207000K		CUYD	90.00	\$ 1,500.00	\$ 135,000.00
	GENERAL STRC CONCRETE, CLASS 4000	0540	0312000K		CUYD	51.00	\$ 1,500.00	\$ 76,500.00
	BT 60 PRECAST PRESRESSTED GIRDERS	0550	Special		FOOT	591.00	\$ 300.00	\$ 177,300.00
	BRIDGE LIGHTING	0580	Special		LS	1	\$ 100,000.00	\$ 100,000.00
	2 INCH ELECTRICAL CONDUIT	0583	0105000F		FOOT	544	\$ 10.00	\$ 5,440.00
	EXPANSION JOINTS	0585	Special		FOOT	36	\$ 250.00	\$ 9,000.00
	ORNAMENTAL BRIDGE RAIL	0587	Special		FOOT	474.00	\$ 250.00	\$ 118,500.00
138	CONCRETE SLOPE PAVING	0599	0100000J	2	SQFT	350.00	\$ 11.25	\$ 3,937.08
143	AGGREGATE BASE	0640	0102000M	5	TON	10.00	\$ 36.90	\$ 369.00
154	16 INCH ASPHALT CONCRETE PAVEMENT REPAIR	0748	Special		SQYD	20.00	\$ 68.80	\$ 1,376.00
168	CONCRETE DRIVEWAYS	0759	0126000J	12	SQFT	400.00	\$ 8.40	\$ 3,360.00
170	CONCRETE WALKS	0759	0128000J	12	SQFT	100.00	\$ 7.40	\$ 740.00
179	DETECTABLE WARNING SURFACE	0759	1158000J	12	SQFT	240.00	\$ 42.30	\$ 10,152.00
193	REMOVABLE BOLLARDS	0815	0101000E	12	EACH	26.00	\$ 750.00	\$ 19,500.00
	FREEWAY SIGNS MOUNTED ON BRIDGE		Special		LS	1.00	\$ 18,000.00	\$ 18,000.00
	PERMANENT SIGNING AND STRIPING		Special		LS	1.00	\$ 7,500.00	\$ 7,500.00
238	POLE FOUNDATIONS	0970	0100000A	10	LS*	3.00	\$ 2,000.00	\$ 6,000.00
243	TRAFFIC SIGNAL INSTALLATION	0990	0101000A	10	LS*	1.00	\$ 120,000.00	\$ 120,000.00
245	FLASHING BEACON INSTALLATION	0990	Special		LS*	1.00	\$ 40,000.00	\$ 40,000.00
246	LOOP DETECTOR INSTALLATION	0990	0103000A	10	LS*	1.00	\$ 9,120.00	\$ 9,120.00
	LANDSCAPING		Special		LS	1.00	\$ 40,000.00	\$ 40,000.00
278	ORNAMENTAL PROTECTIVE SCREENING	1050	Special	12	FOOT	400.00	\$ 158.00	\$ 63,200.00

\* Unit Price Shown is on Pound, Each, or Foot Basis as Applicable

TOTAL BID ITEMS

\$ 1,887,183.48

**##### ANTICIPATED ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	REFERENCE	NUMBER	OF	UNIT	QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	RIGHT OF WAY MONUMENTATION				LS	0.00	\$ -	\$ -
2	RELOCATE WATER FACILITIES - FIRE HYDRANT				EACH	0.00	\$ 20,000.00	\$ -
3	RELOCATE WATER FACILITIES - METER				EACH	0.00	\$ 6,000.00	\$ -
4	STREET LIGHTING - UPGRADE LUMINAIRES				EACH	0.00	\$ 600.00	\$ -
5	STREET LIGHTING - INSTALL ARMS AND LUMINAIRES				EACH	0.00	\$ 5,000.00	\$ -
6	CONNECT CONTRACTOR INSTALLED TRAFFIC SIGNAL LOOPS TO CONTROLLER BY BOM				EACH	0.00	\$ 1,000.00	\$ -
7	STORMWATER PLANTINGS AND PLANT ESTABLISHMENT				SQFT	0.00	\$ 15.00	\$ -

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
8	STORMWATER OFFSITE MANAGEMENT FEE				SQFT	4120.00	\$ 3.70	\$ 15,244.00
9	ROCK EXCAVATION				CUYD	0.00	\$ 106.00	\$ -
10	RAILROAD PROTECTION SERVICES (ONE YEAR)				LS	0.00	\$ 100,000.00	\$ -
11	ASPHALT CEMENT ESCALATION				LS	1.00	\$ -	\$ -
12	FUEL ESCALATION				LS	1.00	\$ -	\$ -
13	TESTING CONTAMINATED MEDIA				LS	0.00	\$ 5,000.00	\$ -
14	BOLI FEE PAYMENT				LS	1.00	\$ 1,887.18	\$ 1,887.18
15	CONTRACT CONTINGENCY (REQUIREMENT TO ACCEPT BIDS UP TO 10% OVER ESTIMATE)				LS	1.00	\$ 188,718.35	\$ 188,718.35

TOTAL ANTICIPATED ITEMS

\$ 205,849.53

**SCHEDULE SUMMARY**

BID ITEMS							\$ 1,887,183
CONSTRUCTION CONTINGENCY						5% of Bid Items*	\$ 94,359
SUBTOTAL							\$ 1,981,542
ANTICIPATED ITEMS							\$ 205,850
<b>TOTAL CONSTRUCTION</b>							<b>\$ 2,187,392</b>
PROJECT MANAGEMENT						5% of Bid Items	\$ 94,359
DESIGN ENGINEERING						25% of Bid Items	\$ 471,796
CONSTRUCTION MANAGEMENT						15% of Bid Items	\$ 283,078
SUBTOTAL							\$ 849,233
PROJECT ENGINEERING & MANAGEMENT OVERHEAD						73.35% of PM, Eng, and CM	\$ 622,913
<b>TOTAL PROJECT ENGINEERING &amp; MANAGEMENT</b>							<b>\$ 1,472,146</b>
RIGHT-OF-WAY LAND, IMPROVEMENTS, AND DAMAGES							\$ -
RIGHT-OF-WAY APPRAISAL, TITLE INSURANCE, AND NEGOTIATION							\$ -
RIGHT-OF-WAY CONTINGENCY						of Land, Improve, and Damages 30%	\$ -
<b>TOTAL PROJECT RIGHT-OF-WAY</b>							<b>\$ -</b>
INFLATION RATE ON CONTRACT				Years		Inflation	\$ 98,433
INFLATION RATE ON PERSONNEL				1		4.5% of Construction	\$ 29,443
ESTIMATE CONTINGENCY FOR UNDEFINED OR CHANGE IN SCOPE				1		2.0% of Eng & Mgmt	\$ 378,741
						10% of Const, Eng & Mgmt, and Inflation	\$ -
<b>TOTAL PROJECT CONTINGENCY</b>							<b>\$ 506,617</b>
<b>TOTAL PROJECT ESTIMATE</b>							<b>\$ 4,166,155</b>

**CITY OF PORTLAND, OREGON  
BUREAU OF TRANSPORTATION**

Date: November 17, 2015

By: C. Glasgow

FEASIBILITY STUDY ENGINEER'S ESTIMATE FOR THE IMPROVEMENT OF FLANDERS CROSSING - ALTERNATIVE "C" - 24 FT ENHANCED WIDTH TRUSS

VALUES IN BLUE ARE PERCENT OF CONTRACT.

**##### BID ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	MOBILIZATION	0210	0100000A	n/a	LS	1.00	\$ 239,424.93	\$ 239,424.93
2	TEMPORARY PROTECTION & DIRECTION OF TRAFFIC	0225	0100000A	13	LS	1.00	\$ 47,884.99	\$ 47,884.99
	TEMPORARY TRAFFIC CONTROL DEVICES	225	Special		LS	1.00	\$ 100,000.00	\$ 100,000.00
22	TEMPORARY CL-6R CHAIN LINK FENCE	0270	0137000F	12	FOOT	360.00	\$ 17.60	\$ 6,336.00
23	EROSION CONTROL	0280	0100000A	11	LS	1.00	\$ 23,942.49	\$ 23,942.49
28	SEDIMENT FENCE, UNSUPPORTED	0280	0113000F	11	FOOT	280.00	\$ 2.50	\$ 700.00
29	INLET PROTECTION	0280	0114000E	11	EACH	12.00	\$ 88.00	\$ 1,056.00
30	POLLUTION CONTROL PLAN	0290	0100000A	12	LS	1.00	\$ 2,394.25	\$ 2,394.25
33	HASP/CMDP WORKPLANS	0291	1105000A	12	LS	1.00	\$ 1,000.00	\$ 1,000.00
43	REMOVAL OF STRUCTURES & OBSTRUCTIONS	0310	0106000A	1	LS	1.00	\$ 10,000.00	\$ 10,000.00
45	CLEARING AND GRUBBING	0320	0100000A	1	LS	1.00	\$ 30,000.00	\$ 30,000.00
49	GENERAL EXCAVATION	0330	0105000K	1	CUYD	50.00	\$ 35.00	\$ 1,750.00
66	TRENCH EXCAVATION, COMMON	0405	1101000K	1	CUYD	100.00	\$ 16.70	\$ 1,670.00
70	TRENCH BACKFILL, CLASS B	0405	1109000K	1	CUYD	75.00	\$ 33.00	\$ 2,475.00
82	10 INCH PIPE, HDPE ASTM F714 SDR 26 BEDDING TYPE:D, COMPLETE	0445	Special	1	FOOT	100.00	\$ 122.00	\$ 12,200.00
100	CONCRETE INLETS, TYPE G-2	0470	0315000E	1	EACH	2.00	\$ 1,770.00	\$ 3,540.00
112	CONNECTION TO EXISTING STRUCTURES	0490	0104000E	1	EACH	2.00	\$ 841.00	\$ 1,682.00
118	TRENCH RESURFACING	0495	0100000J	1	SQYD	33.33	\$ 109.00	\$ 3,632.97
	STRUCTURE DEMOLITION	0501	Special		LS	1.00	\$ 22,500.00	\$ 22,500.00
120	SHORING, CRIBBING AND COFFERDAMS	0510	0100000A	2	LS	1.00	\$ 7,500.00	\$ 7,500.00
121	STRUCTURE EXCAVATION	0510	0101000K	2	CUYD	60.00	\$ 48.30	\$ 2,898.00
123	GRANULAR STRUCTURAL BACKFILL	0510	0108000K	2	CUYD	75.00	\$ 50.00	\$ 3,750.00
	FURNISH DRILLING EQUIPMENT		Special		LS	1.00	\$ 45,000.00	\$ 45,000.00
	PERMANENT SHAFT CASINGS		Special		FOOT	300.00	\$ 500.00	\$ 150,000.00
	CSL TEST ACCESS TUBES	0512	0105000F		FOOT	900.00	\$ 8.00	\$ 7,200.00
	CSL TESTS	0512	0106000E		EACH	6.00	\$ 1,500.00	\$ 9,000.00
	DRILLED SHAFT EXC, 36 INCH DIA	0512	0110000F		FOOT	300.00	\$ 250.00	\$ 75,000.00
	DRILLED SHAFT CONCRETE	0512	Special		CUYD	55.00	\$ 500.00	\$ 27,500.00
	DRILLED SHAFT REINFORCEMENT	0530	Special		LB	30000.00	\$ 1.00	\$ 30,000.00
124	REINFORCEMENT	0530	0100000A	2	LS*	1.00	\$ 17,400.00	\$ 17,400.00
	FOUNDATION CONCRETE	0540	0111000K		CUYD	75.00	\$ 600.00	\$ 45,000.00
	DECK CONCRETE, CLASS HPC4000	0540	0207000K		CUYD	82.50	\$ 1,000.00	\$ 82,500.00
	GENERAL STRC CONCRETE, CLASS 4000	0540	0312000K		CUYD	60.00	\$ 1,500.00	\$ 90,000.00
	PREFABRICATED BRIDGE, DELIVERED	0561	Special		LS	1.00	\$ 1,259,276.70	\$ 1,259,276.70
	PREFABRICATED BRIDGE INSTALLATION	0561	Special		LS	1.00	\$ 60,000.00	\$ 60,000.00
	BRIDGE LIGHTING	0580	Special		LS	1	\$ 100,000.00	\$ 100,000.00
	2 INCH ELECTRICAL CONDUIT	0583	0105000F		FOOT	816	\$ 10.00	\$ 8,160.00
	EXPANSION JOINTS	0585	Special		FOOT	54	\$ 250.00	\$ 13,500.00
	ORNAMENTAL BRIDGE RAIL	0587	Special		FOOT	80.00	\$ 250.00	\$ 20,000.00
138	CONCRETE SLOPE PAVING	0599	0100000J	2	SQFT	525.00	\$ 11.25	\$ 5,905.62
143	AGGREGATE BASE	0640	0102000M	5	TON	10.00	\$ 36.90	\$ 369.00
154	16 INCH ASPHALT CONCRETE PAVEMENT REPAIR	0748	Special		SQYD	20.00	\$ 68.80	\$ 1,376.00
168	CONCRETE DRIVEWAYS	0759	0126000J	12	SQFT	400.00	\$ 8.40	\$ 3,360.00
170	CONCRETE WALKS	0759	0128000J	12	SQFT	100.00	\$ 7.40	\$ 740.00
179	DETECTABLE WARNING SURFACE	0759	1158000J	12	SQFT	240.00	\$ 42.30	\$ 10,152.00
193	REMOVABLE BOLLARDS	0815	0101000E	12	EACH	26.00	\$ 750.00	\$ 19,500.00
	FREEWAY SIGNS MOUNTED ON BRIDGE		Special		LS	1.00	\$ 18,000.00	\$ 18,000.00
	PERMANENT SIGNING AND STRIPING		Special		LS	1.00	\$ 7,500.00	\$ 7,500.00
238	POLE FOUNDATIONS	0970	0100000A	10	LS*	3.00	\$ 2,000.00	\$ 6,000.00
243	TRAFFIC SIGNAL INSTALLATION	0990	0101000A	10	LS*	1.00	\$ 120,000.00	\$ 120,000.00
245	FLASHING BEACON INSTALLATION	0990	Special		LS*	1.00	\$ 40,000.00	\$ 40,000.00
246	LOOP DETECTOR INSTALLATION	0990	0103000A	10	LS*	1.00	\$ 9,120.00	\$ 9,120.00
	LANDSCAPING		Special		LS	1.00	\$ 40,000.00	\$ 40,000.00

\* Unit Price Shown is on Pound, Each, or Foot Basis as Applicable

TOTAL BID ITEMS

\$ 2,847,895.95

**##### ANTICIPATED ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	REFERENCE	NUMBER	OF	UNIT	QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	RIGHT OF WAY MONUMENTATION				LS	0.00	\$ -	\$ -
2	RELOCATE WATER FACILITIES - FIRE HYDRANT				EACH	0.00	\$ 20,000.00	\$ -
3	RELOCATE WATER FACILITIES - METER				EACH	0.00	\$ 6,000.00	\$ -
4	STREET LIGHTING - UPGRADE LUMINAIRES				EACH	0.00	\$ 600.00	\$ -
5	STREET LIGHTING - INSTALL ARMS AND LUMINAIRES				EACH	0.00	\$ 5,000.00	\$ -
6	CONNECT CONTRACTOR INSTALLED TRAFFIC SIGNAL LOOPS TO CONTROLLER BY BOM				EACH	0.00	\$ 1,000.00	\$ -
7	STORMWATER PLANTINGS AND PLANT ESTABLISHMENT				SQFT	0.00	\$ 15.00	\$ -

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
8	STORMWATER OFFSITE MANAGEMENT FEE				SQFT	6120.00	\$ 3.70	\$ 22,644.00
9	ROCK EXCAVATION				CUYD	0.00	\$ 106.00	\$ -
10	RAILROAD PROTECTION SERVICES (ONE YEAR)				LS	0.00	\$ 100,000.00	\$ -
11	ASPHALT CEMENT ESCALATION				LS	1.00	\$ -	\$ -
12	FUEL ESCALATION				LS	1.00	\$ -	\$ -
13	TESTING CONTAMINATED MEDIA				LS	0.00	\$ 5,000.00	\$ -
14	BOLI FEE PAYMENT				LS	1.00	\$ 2,847.90	\$ 2,847.90
15	CONTRACT CONTINGENCY (REQUIREMENT TO ACCEPT BIDS UP TO 10% OVER ESTIMATE)				LS	1.00	\$ 284,789.60	\$ 284,789.60

TOTAL ANTICIPATED ITEMS

\$ 310,281.49

**SCHEDULE SUMMARY**

BID ITEMS							\$ 2,847,896
CONSTRUCTION CONTINGENCY						5% of Bid Items*	\$ 142,395
SUBTOTAL							\$ 2,990,291
ANTICIPATED ITEMS							\$ 310,281
<b>TOTAL CONSTRUCTION</b>							<b>\$ 3,300,572</b>
PROJECT MANAGEMENT						5% of Bid Items	\$ 142,395
DESIGN ENGINEERING						20% of Bid Items	\$ 569,579
CONSTRUCTION MANAGEMENT						15% of Bid Items	\$ 427,184
SUBTOTAL							\$ 1,139,158
PROJECT ENGINEERING & MANAGEMENT OVERHEAD						73.35% of PM, Eng, and CM	\$ 835,572
<b>TOTAL PROJECT ENGINEERING &amp; MANAGEMENT</b>							<b>\$ 1,974,730</b>
RIGHT-OF-WAY LAND, IMPROVEMENTS, AND DAMAGES							\$ -
RIGHT-OF-WAY APPRAISAL, TITLE INSURANCE, AND NEGOTIATION							\$ -
RIGHT-OF-WAY CONTINGENCY						of Land, Improve, and Damages 30%	\$ -
<b>TOTAL PROJECT RIGHT-OF-WAY</b>							<b>\$ -</b>
INFLATION RATE ON CONTRACT				Years		Inflation	\$ 148,526
INFLATION RATE ON PERSONNEL				1		4.5% of Construction	\$ 39,495
ESTIMATE CONTINGENCY FOR UNDEFINED OR CHANGE IN SCOPE				1		2.0% of Eng & Mgmt	\$ 546,332
						10% of Const, Eng & Mgmt, and Inflation	\$ -
<b>TOTAL PROJECT CONTINGENCY</b>							<b>\$ 734,353</b>
<b>TOTAL PROJECT ESTIMATE</b>							<b>\$ 6,009,656</b>

CITY OF PORTLAND, OREGON  
BUREAU OF TRANSPORTATION

Date: November 17, 2015

By: C. Glasgow

FEASIBILITY STUDY ENGINEER'S ESTIMATE FOR THE IMPROVEMENT OF FLANDERS CROSSING - ALTERNATIVE "D" - 24 FT ENHANCED WIDTH SIGNATURE BRIDGE

VALUES IN BLUE ARE PERCENT OF CONTRACT.

##### BID ITEMS #####

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
	TEMPORARY TRAFFIC CONTROL DEVICES	225	Special		LS	1.00	\$ 100,000.00	\$ 100,000.00
	BRIDGE REMOVAL	0501	Special		LS	1.00	\$22,500.00	\$22,500.00
	BRIDGE SUBSTRUCTURE AND SUPERSTRUCTURE, COMPLETE		Special		SF	5400.00	\$455.00	\$2,457,000.00
	BRIDGE LIGHTING	0580	Special		LS	1	\$100,000.00	\$100,000.00
	FREEWAY SIGNS MOUNTED ON BRIDGE		Special		LS	1.00	\$ 18,000.00	\$ 18,000.00
238	POLE FOUNDATIONS	0970	0100000A	10	LS*	3.00	\$ 2,000.00	\$ 6,000.00
243	TRAFFIC SIGNAL INSTALLATION	0990	0101000A	10	LS*	1.00	\$ 120,000.00	\$ 120,000.00
245	FLASHING BEACON INSTALLATION	0990	Special		LS*	1.00	\$ 40,000.00	\$ 40,000.00
246	LOOP DETECTOR INSTALLATION	0990	0103000A	10	LS*	1.00	\$ 9,120.00	\$ 9,120.00

\* Unit Price Shown is on Pound, Each, or Foot Basis as Applicable

TOTAL BID ITEMS

\$ 2,872,620.00

##### ANTICIPATED ITEMS #####

NO.	ITEMS OF WORK AND MATERIALS	REFERENCE	NUMBER	OF	UNIT	QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	RIGHT OF WAY MONUMENTATION				LS	0.00	\$ -	\$ -
2	RELOCATE WATER FACILITIES - FIRE HYDRANT				EACH	0.00	\$ 20,000.00	\$ -
3	RELOCATE WATER FACILITIES - METER				EACH	0.00	\$ 6,000.00	\$ -
4	STREET LIGHTING - UPGRADE LUMINAIRES				EACH	0.00	\$ 600.00	\$ -
5	STREET LIGHTING - INSTALL ARMS AND LUMINAIRES				EACH	0.00	\$ 5,000.00	\$ -
6	CONNECT CONTRACTOR INSTALLED TRAFFIC SIGNAL LOOPS TO CONTROLLER BY BOM				EACH	0.00	\$ 1,000.00	\$ -
7	STORMWATER PLANTINGS AND PLANT ESTABLISHMENT				SQFT	0.00	\$ 15.00	\$ -
8	STORMWATER OFFSITE MANAGEMENT FEE				SQFT	6120.00	\$ 3.70	\$ 22,644.00
9	ROCK EXCAVATION				CUYD	0.00	\$ 106.00	\$ -
10	RAILROAD PROTECTION SERVICES (ONE YEAR)				LS	0.00	\$ 100,000.00	\$ -
11	ASPHALT CEMENT ESCALATION				LS	1.00	\$ -	\$ -
12	FUEL ESCALATION				LS	1.00	\$ -	\$ -
13	TESTING CONTAMINATED MEDIA				LS	0.00	\$ 5,000.00	\$ -
14	BOLI FEE PAYMENT				LS	1.00	\$ 2,872.62	\$ 2,872.62
15	CONTRACT CONTINGENCY (REQUIREMENT TO ACCEPT BIDS UP TO 10% OVER ESTIMATE)				LS	1.00	\$ 287,262.00	\$ 287,262.00

TOTAL ANTICIPATED ITEMS

\$ 312,778.62

SCHEDULE SUMMARY

BID ITEMS		\$ 2,872,620
CONSTRUCTION CONTINGENCY	5% of Bid Items	\$ 143,631
SUBTOTAL		\$ 3,016,251
ANTICIPATED ITEMS		\$ 312,779
<b>TOTAL CONSTRUCTION</b>		<b>\$ 3,329,030</b>
PROJECT MANAGEMENT	5% of Bid Items	\$ 143,631
DESIGN ENGINEERING	25% of Bid Items	\$ 718,155
CONSTRUCTION MANAGEMENT	15% of Bid Items	\$ 430,893
SUBTOTAL		\$ 1,292,679
PROJECT ENGINEERING & MANAGEMENT OVERHEAD	73.35% of PM, Eng, and CM	\$ 948,180
<b>TOTAL PROJECT ENGINEERING &amp; MANAGEMENT</b>		<b>\$ 2,240,859</b>
RIGHT-OF-WAY LAND, IMPROVEMENTS, AND DAMAGES		\$ -
RIGHT-OF-WAY APPRAISAL, TITLE INSURANCE, AND NEGOTIATION		\$ -
RIGHT-OF-WAY CONTINGENCY	30% of Land, Improve, and Damages	\$ -
<b>TOTAL PROJECT RIGHT-OF-WAY</b>		<b>\$ -</b>
INFLATION RATE ON CONTRACT	1 Years	\$ 149,806
INFLATION RATE ON PERSONNEL	1 Inflation	\$ 44,817
ESTIMATE CONTINGENCY FOR UNDEFINED OR CHANGE IN SCOPE	20% of Const, Eng & Mgmt,	\$ 1,152,902

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
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and Inflation

**TOTAL PROJECT CONTINGENCY**

**\$ 1,347,525**

**TOTAL PROJECT ESTIMATE**

**\$ 6,917,414**

**CITY OF PORTLAND, OREGON  
BUREAU OF TRANSPORTATION**

Date: November 17, 2015

By: C. Glasgow

FEASIBILITY STUDY ENGINEER'S ESTIMATE FOR THE IMPROVEMENT OF FLANDERS CROSSING - ALTERNATIVE "E" - TWO COMPANION TRUSS BRIDGES - 14 FT EACH

VALUES IN BLUE ARE PERCENT OF CONTRACT.

**##### BID ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	MOBILIZATION	0210	0100000A	n/a	LS	1.00	\$ 263,628.89	\$ 263,628.89
2	TEMPORARY PROTECTION & DIRECTION OF TRAFFIC	0225	0100000A	13	LS	1.00	\$ 52,725.78	\$ 52,725.78
	TEMPORARY TRAFFIC CONTROL DEVICES	225	Special		LS	1.00	\$ 200,000.00	\$ 200,000.00
22	TEMPORARY CL-6R CHAIN LINK FENCE	0270	0137000F	12	FOOT	360.00	\$ 17.60	\$ 6,336.00
23	EROSION CONTROL	0280	0100000A	11	LS	1.00	\$ 26,362.89	\$ 26,362.89
28	SEDIMENT FENCE, UNSUPPORTED	0280	0113000F	11	FOOT	280.00	\$ 2.50	\$ 700.00
29	INLET PROTECTION	0280	0114000E	11	EACH	12.00	\$ 88.00	\$ 1,056.00
30	POLLUTION CONTROL PLAN	0290	0100000A	12	LS	1.00	\$ 2,636.29	\$ 2,636.29
33	HASP/CMDP WORKPLANS	0291	1105000A	12	LS	1.00	\$ 1,000.00	\$ 1,000.00
43	REMOVAL OF STRUCTURES & OBSTRUCTIONS	0310	0106000A	1	LS	1.00	\$ 10,000.00	\$ 10,000.00
45	CLEARING AND GRUBBING	0320	0100000A	1	LS	1.00	\$ 40,000.00	\$ 40,000.00
49	GENERAL EXCAVATION	0330	0105000K	1	CUYD	50.00	\$ 35.00	\$ 1,750.00
66	TRENCH EXCAVATION, COMMON	0405	1101000K	1	CUYD	100.00	\$ 16.70	\$ 1,670.00
70	TRENCH BACKFILL, CLASS B	0405	1109000K	1	CUYD	75.00	\$ 33.00	\$ 2,475.00
82	10 INCH PIPE, HDPE ASTM F714 SDR 26 BEDDING TYPE:D, COMPLETE	0445	Special	1	FOOT	100.00	\$ 122.00	\$ 12,200.00
100	CONCRETE INLETS, TYPE G-2	0470	0315000E	1	EACH	2.00	\$ 1,770.00	\$ 3,540.00
112	CONNECTION TO EXISTING STRUCTURES	0490	0104000E	1	EACH	2.00	\$ 841.00	\$ 1,682.00
118	TRENCH RESURFACING	0495	0100000J	1	SQYD	33.33	\$ 109.00	\$ 3,632.97
	STRUCTURE DEMOLITION	0501	Special		LS	1.00	\$ 30,000.00	\$ 30,000.00
120	SHORING, CRIBBING AND COFFERDAMS	0510	0100000A	2	LS	1.00	\$ 10,000.00	\$ 10,000.00
121	STRUCTURE EXCAVATION	0510	0101000K	2	CUYD	80.00	\$ 48.30	\$ 3,864.00
123	GRANULAR STRUCTURAL BACKFILL	0510	0108000K	2	CUYD	100.00	\$ 50.00	\$ 5,000.00
	FURNISH DRILLING EQUIPMENT		Special		LS	1.00	\$ 60,000.00	\$ 60,000.00
	PERMANENT SHAFT CASINGS		Special		FOOT	400.00	\$ 500.00	\$ 200,000.00
	CSL TEST ACCESS TUBES	0512	0105000F		FOOT	1200.00	\$ 8.00	\$ 9,600.00
	CSL TESTS	0512	0106000E		EACH	8.00	\$ 1,500.00	\$ 12,000.00
	DRILLED SHAFT EXC, 36 INCH DIA	0512	0110000F		FOOT	400.00	\$ 250.00	\$ 100,000.00
	DRILLED SHAFT CONCRETE	0512	Special		CUYD	110.00	\$ 500.00	\$ 55,000.00
	DRILLED SHAFT REINFORCEMENT	0530	Special		LB	40000.00	\$ 1.00	\$ 40,000.00
124	REINFORCEMENT	0530	0100000A	2	LS*	1.00	\$ 22,800.00	\$ 22,800.00
	FOUNDATION CONCRETE	0540	0111000K		CUYD	87.50	\$ 600.00	\$ 52,500.00
	DECK CONCRETE, CLASS HPC4000	0540	0207000K		CUYD	96.00	\$ 1,000.00	\$ 96,000.00
	GENERAL STRC CONCRETE, CLASS 4000	0540	0312000K		CUYD	40.00	\$ 1,500.00	\$ 60,000.00
	PREFABRICATED BRIDGE, DELIVERED	0561	Special		LS	1.00	\$ 1,335,596.00	\$ 1,335,596.00
	PREFABRICATED BRIDGE INSTALLATION	0561	Special		LS	1.00	\$ 60,000.00	\$ 60,000.00
	BRIDGE LIGHTING	0580	Special		LS	1	\$ 100,000.00	\$ 100,000.00
	2 INCH ELECTRICAL CONDUIT	0583	0105000F		FOOT	1088	\$ 10.00	\$ 10,880.00
	EXPANSION JOINTS	0585	Special		FOOT	56	\$ 250.00	\$ 14,000.00
	ORNAMENTAL BRIDGE RAIL	0587	Special		FOOT	160.00	\$ 250.00	\$ 40,000.00
138	CONCRETE SLOPE PAVING	0599	0100000J	2	SQFT	612.50	\$ 11.25	\$ 6,889.89
143	AGGREGATE BASE	0640	0102000M	5	TON	10.00	\$ 36.90	\$ 369.00
154	16 INCH ASPHALT CONCRETE PAVEMENT REPAIR	0748	Special		SQYD	20.00	\$ 68.80	\$ 1,376.00
168	CONCRETE DRIVEWAYS	0759	0126000J	12	SQFT	400.00	\$ 8.40	\$ 3,360.00
170	CONCRETE WALKS	0759	0128000J	12	SQFT	100.00	\$ 7.40	\$ 740.00
179	DETECTABLE WARNING SURFACE	0759	1158000J	12	SQFT	240.00	\$ 42.30	\$ 10,152.00
193	REMOVABLE BOLLARDS	0815	0101000E	12	EACH	26.00	\$ 750.00	\$ 19,500.00
	FREEWAY SIGNS MOUNTED ON BRIDGE		Special		LS	1.00	\$ 18,000.00	\$ 18,000.00
	PERMANENT SIGNING AND STRIPING		Special		LS	1.00	\$ 7,500.00	\$ 7,500.00
238	POLE FOUNDATIONS	0970	0100000A	10	LS*	3.00	\$ 2,000.00	\$ 6,000.00
243	TRAFFIC SIGNAL INSTALLATION	0990	0101000A	10	LS*	1.00	\$ 120,000.00	\$ 120,000.00
245	FLASHING BEACON INSTALLATION	0990	Special		LS*	1.00	\$ 40,000.00	\$ 40,000.00
246	LOOP DETECTOR INSTALLATION	0990	0103000A	10	LS*	1.00	\$ 9,120.00	\$ 9,120.00
	LANDSCAPING		Special		LS	1.00	\$ 40,000.00	\$ 40,000.00

\* Unit Price Shown is on Pound, Each, or Foot Basis as Applicable

TOTAL BID ITEMS

\$ 3,231,642.71

**##### ANTICIPATED ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	REFERENCE	NUMBER	OF	UNIT	QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	RIGHT OF WAY MONUMENTATION				LS	0.00	\$ -	\$ -
2	RELOCATE WATER FACILITIES - FIRE HYDRANT				EACH	0.00	\$ 20,000.00	\$ -
3	RELOCATE WATER FACILITIES - METER				EACH	0.00	\$ 6,000.00	\$ -
4	STREET LIGHTING - UPGRADE LUMINAIRES				EACH	0.00	\$ 600.00	\$ -
5	STREET LIGHTING - INSTALL ARMS AND LUMINAIRES				EACH	0.00	\$ 5,000.00	\$ -
6	CONNECT CONTRACTOR INSTALLED TRAFFIC SIGNAL LOOPS TO CONTROLLER BY BOM				EACH	0.00	\$ 1,000.00	\$ -
7	STORMWATER PLANTINGS AND PLANT ESTABLISHMENT				SQFT	0.00	\$ 15.00	\$ -

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
8	STORMWATER OFFSITE MANAGEMENT FEE				SQFT	4120.00	\$ 3.70	\$ 15,244.00
9	ROCK EXCAVATION				CUYD	0.00	\$ 106.00	\$ -
10	RAILROAD PROTECTION SERVICES (ONE YEAR)				LS	0.00	\$ 100,000.00	\$ -
11	ASPHALT CEMENT ESCALATION				LS	1.00	\$ -	\$ -
12	FUEL ESCALATION				LS	1.00	\$ -	\$ -
13	TESTING CONTAMINATED MEDIA				LS	0.00	\$ 5,000.00	\$ -
14	BOLI FEE PAYMENT				LS	1.00	\$ 3,231.64	\$ 3,231.64
15	CONTRACT CONTINGENCY (REQUIREMENT TO ACCEPT BIDS UP TO 10% OVER ESTIMATE)				LS	1.00	\$ 323,164.27	\$ 323,164.27

TOTAL ANTICIPATED ITEMS

\$ 341,639.91

**SCHEDULE SUMMARY**

BID ITEMS							\$ 3,231,643
CONSTRUCTION CONTINGENCY						5% of Bid Items*	\$ 161,582
SUBTOTAL							\$ 3,393,225
ANTICIPATED ITEMS							\$ 341,640
<b>TOTAL CONSTRUCTION</b>							<b>\$ 3,734,865</b>
PROJECT MANAGEMENT						5% of Bid Items	\$ 161,582
DESIGN ENGINEERING						15% of Bid Items	\$ 484,746
CONSTRUCTION MANAGEMENT						15% of Bid Items	\$ 484,746
SUBTOTAL							\$ 1,131,074
PROJECT ENGINEERING & MANAGEMENT OVERHEAD						73.35% of PM, Eng, and CM	\$ 829,643
<b>TOTAL PROJECT ENGINEERING &amp; MANAGEMENT</b>							<b>\$ 1,960,717</b>
RIGHT-OF-WAY LAND, IMPROVEMENTS, AND DAMAGES							\$ -
RIGHT-OF-WAY APPRAISAL, TITLE INSURANCE, AND NEGOTIATION							\$ -
RIGHT-OF-WAY CONTINGENCY						of Land, Improve, and Damages 30%	\$ -
<b>TOTAL PROJECT RIGHT-OF-WAY</b>							<b>\$ -</b>
INFLATION RATE ON CONTRACT				Years		Inflation	\$ 168,069
INFLATION RATE ON PERSONNEL				1		4.5% of Construction	\$ 39,214
ESTIMATE CONTINGENCY FOR UNDEFINED OR CHANGE IN SCOPE				1		2.0% of Eng & Mgmt	\$ 590,286
						10% of Const, Eng & Mgmt, and Inflation	\$ -
<b>TOTAL PROJECT CONTINGENCY</b>							<b>\$ 797,569</b>
<b>TOTAL PROJECT ESTIMATE</b>							<b>\$ 6,493,151</b>

**CITY OF PORTLAND, OREGON  
BUREAU OF TRANSPORTATION**

Date: November 17, 2015

By: C. Glasgow

FEASIBILITY STUDY ENGINEER'S ESTIMATE FOR THE IMPROVEMENT OF FLANDERS CROSSING - ALTERNATIVE "F" - 60 FT CONCRETE 2-SPAN PLAZA BRIDGE

VALUES IN BLUE ARE PERCENT OF CONTRACT.

**##### BID ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	MOBILIZATION	0210	0100000A	n/a	LS	1.00	\$ 266,658.94	\$ 266,658.94
2	TEMPORARY PROTECTION & DIRECTION OF TRAFFIC	0225	0100000A	13	LS	1.00	\$ 53,331.79	\$ 53,331.79
	TEMPORARY TRAFFIC CONTROL DEVICES	225	Special		LS	1.00	\$ 600,000.00	\$ 600,000.00
22	TEMPORARY CL-6R CHAIN LINK FENCE	0270	0137000F	12	FOOT	360.00	\$ 17.60	\$ 6,336.00
23	EROSION CONTROL	0280	0100000A	11	LS	1.00	\$ 26,665.89	\$ 26,665.89
28	SEDIMENT FENCE, UNSUPPORTED	0280	0113000F	11	FOOT	280.00	\$ 2.50	\$ 700.00
29	INLET PROTECTION	0280	0114000E	11	EACH	16.00	\$ 88.00	\$ 1,408.00
30	POLLUTION CONTROL PLAN	0290	0100000A	12	LS	1.00	\$ 2,666.59	\$ 2,666.59
33	HASP/CMDP WORKPLANS	0291	1105000A	12	LS	1.00	\$ 1,000.00	\$ 1,000.00
43	REMOVAL OF STRUCTURES & OBSTRUCTIONS	0310	0106000A	1	LS	1.00	\$ 15,000.00	\$ 15,000.00
45	CLEARING AND GRUBBING	0320	0100000A	1	LS	1.00	\$ 40,000.00	\$ 40,000.00
49	GENERAL EXCAVATION	0330	0105000K	1	CUYD	50.00	\$ 35.00	\$ 1,750.00
66	TRENCH EXCAVATION, COMMON	0405	1101000K	1	CUYD	100.00	\$ 16.70	\$ 1,670.00
70	TRENCH BACKFILL, CLASS B	0405	1109000K	1	CUYD	75.00	\$ 33.00	\$ 2,475.00
82	10 INCH PIPE, HDPE ASTM F714 SDR 26 BEDDING TYPE:D, COMPLETE	0445	Special	1	FOOT	100.00	\$ 122.00	\$ 12,200.00
100	CONCRETE INLETS, TYPE G-2	0470	0315000E	1	EACH	2.00	\$ 1,770.00	\$ 3,540.00
112	CONNECTION TO EXISTING STRUCTURES	0490	0104000E	1	EACH	2.00	\$ 841.00	\$ 1,682.00
118	TRENCH RESURFACING	0495	0100000J	1	SQYD	33.33	\$ 109.00	\$ 3,632.97
	STRUCTURE DEMOLITION	0501	Special		LS	1.00	\$37,500.00	\$37,500.00
120	SHORING, CRIBBING AND COFFERDAMS	0510	0100000A	2	LS	1.00	\$ 12,500.00	\$ 12,500.00
121	STRUCTURE EXCAVATION	0510	0101000K	2	CUYD	100.00	\$ 48.30	\$ 4,830.00
123	GRANULAR STRUCTURAL BACKFILL	0510	0108000K	2	CUYD	125.00	\$ 50.00	\$ 6,250.00
	FURNISH DRILLING EQUIPMENT		Special		LS	1.00	\$ 100,000.00	\$ 100,000.00
	PERMANENT SHAFT CASINGS		Special		FOOT	250.00	\$ 1,050.00	\$ 262,500.00
	CSL TEST ACCESS TUBES	0512	0105000F		FOOT	3150.00	\$8.00	\$25,200.00
	CSL TESTS	0512	0106000E		EACH	21.00	\$1,500.00	\$31,500.00
	DRILLED SHAFT EXC, 36 INCH DIA	0512	0110000F		FOOT	250.00	\$250.00	\$62,500.00
	DRILLED SHAFT CONCRETE	0512	Special		CUYD	275.00	\$500.00	\$137,500.00
	DRILLED SHAFT REINFORCEMENT	0530	Special		LB	25000.00	\$1.00	\$25,000.00
124	REINFORCEMENT	0530	0100000A	2	LS*	1.00	\$ 90,000.00	\$ 90,000.00
	FOUNDATION CONCRETE	0540	0111000K		CUYD	270.00	\$600.00	\$162,000.00
	DECK CONCRETE, CLASS HPC4000	0540	0207000K		CUYD	300.00	\$1,500.00	\$450,000.00
	GENERAL STRC CONCRETE, CLASS 4000	0540	0312000K		CUYD	51.00	\$1,500.00	\$76,500.00
	BT 60 PRECAST PRESRESSTED GIRDERS	0550	Special		FOOT	1773.00	\$ 300.00	\$531,900.00
	BRIDGE LIGHTING	0580	Special		LS	1	\$100,000.00	\$100,000.00
	2 INCH ELECTRICAL CONDUIT	0583	0105000F		FOOT	544	\$10.00	\$5,440.00
	EXPANSION JOINTS	0585	Special		FOOT	120	\$250.00	\$30,000.00
	ORNAMENTAL BRIDGE RAIL	0587	Special		FOOT	500.00	\$250.00	\$125,000.00
138	CONCRETE SLOPE PAVING	0599	0100000J	2	SQFT	1,312.00	\$ 11.25	\$ 14,758.43
143	AGGREGATE BASE	0640	0102000M	5	TON	10.00	\$ 36.90	\$ 369.00
154	16 INCH ASPHALT CONCRETE PAVEMENT REPAIR	0748	Special		SQYD	20.00	\$ 68.80	\$ 1,376.00
168	CONCRETE DRIVEWAYS	0759	0126000J	12	SQFT	400.00	\$ 8.40	\$ 3,360.00
170	CONCRETE WALKS	0759	0128000J	12	SQFT	100.00	\$ 7.40	\$ 740.00
179	DETECTABLE WARNING SURFACE	0759	1158000J	12	SQFT	240.00	\$ 42.30	\$ 10,152.00
193	REMOVABLE BOLLARDS	0815	0101000E	12	EACH	26.00	\$ 750.00	\$ 19,500.00
	FREEWAY SIGNS MOUNTED ON BRIDGE		Special		LS	1.00	\$ 18,000.00	\$ 18,000.00
	PERMANENT SIGNING AND STRIPING		Special		LS	1.00	\$ 7,500.00	\$ 7,500.00
238	POLE FOUNDATIONS	0970	0100000A	10	LS*	3.00	\$ 2,000.00	\$ 6,000.00
243	TRAFFIC SIGNAL INSTALLATION	0990	0101000A	10	LS*	1.00	\$ 120,000.00	\$ 120,000.00
245	FLASHING BEACON INSTALLATION	0990	Special		LS*	1.00	\$ 40,000.00	\$ 40,000.00
246	LOOP DETECTOR INSTALLATION	0990	0103000A	10	LS*	1.00	\$ 9,120.00	\$ 9,120.00
	LANDSCAPING		Special		LS	1.00	\$ 40,000.00	\$ 40,000.00
278	ORNAMENTAL PROTECTIVE SCREENING	1050	Special	12	FOOT	400.00	\$ 158.00	\$ 63,200.00

\* Unit Price Shown is on Pound, Each, or Foot Basis as Applicable

TOTAL BID ITEMS

\$ 3,670,912.61

**##### ANTICIPATED ITEMS #####**

NO.	ITEMS OF WORK AND MATERIALS	REFERENCE	NUMBER	OF	UNIT	QUANTITY	UNIT PRICE	TOTAL AMOUNT
1	RIGHT OF WAY MONUMENTATION				LS	0.00	-	\$ -
2	RELOCATE WATER FACILITIES - FIRE HYDRANT				EACH	0.00	\$ 20,000.00	\$ -
3	RELOCATE WATER FACILITIES - METER				EACH	0.00	\$ 6,000.00	\$ -
4	STREET LIGHTING - UPGRADE LUMINAIRES				EACH	0.00	\$ 600.00	\$ -
5	STREET LIGHTING - INSTALL ARMS AND LUMINAIRES				EACH	0.00	\$ 5,000.00	\$ -
6	CONNECT CONTRACTOR INSTALLED TRAFFIC SIGNAL LOOPS TO CONTROLLER BY BOM				EACH	0.00	\$ 1,000.00	\$ -
7	STORMWATER PLANTINGS AND PLANT ESTABLISHMENT				SQFT	0.00	\$ 15.00	\$ -

NO.	ITEMS OF WORK AND MATERIALS	SPEC REFERENCE	ITEM NUMBER	CLASS OF WORK	UNIT	TOTAL QUANTITY	UNIT PRICE	TOTAL AMOUNT
8	STORMWATER OFFSITE MANAGEMENT FEE				SQFT	12720.00	\$ 3.70	\$ 47,064.00
9	ROCK EXCAVATION				CUYD	0.00	\$ 106.00	\$ -
10	RAILROAD PROTECTION SERVICES (ONE YEAR)				LS	0.00	\$ 100,000.00	\$ -
11	ASPHALT CEMENT ESCALATION				LS	1.00	\$ -	\$ -
12	FUEL ESCALATION				LS	1.00	\$ -	\$ -
13	TESTING CONTAMINATED MEDIA				LS	0.00	\$ 5,000.00	\$ -
14	BOLI FEE PAYMENT				LS	1.00	\$ 3,670.91	\$ 3,670.91
15	CONTRACT CONTINGENCY (REQUIREMENT TO ACCEPT BIDS UP TO 10% OVER ESTIMATE)				LS	1.00	\$ 367,091.26	\$ 367,091.26

TOTAL ANTICIPATED ITEMS

\$ 417,826.17

**SCHEDULE SUMMARY**

BID ITEMS							\$ 3,670,913
CONSTRUCTION CONTINGENCY						5% of Bid Items*	\$ 183,546
SUBTOTAL							\$ 3,854,459
ANTICIPATED ITEMS							\$ 417,826
<b>TOTAL CONSTRUCTION</b>							<b>\$ 4,272,285</b>
PROJECT MANAGEMENT						5% of Bid Items	\$ 183,546
DESIGN ENGINEERING						25% of Bid Items	\$ 917,728
CONSTRUCTION MANAGEMENT						15% of Bid Items	\$ 550,637
SUBTOTAL							\$ 1,651,911
PROJECT ENGINEERING & MANAGEMENT OVERHEAD						73.35% of PM, Eng, and CM	\$ 1,211,676
<b>TOTAL PROJECT ENGINEERING &amp; MANAGEMENT</b>							<b>\$ 2,863,587</b>
RIGHT-OF-WAY LAND, IMPROVEMENTS, AND DAMAGES							\$ -
RIGHT-OF-WAY APPRAISAL, TITLE INSURANCE, AND NEGOTIATION							\$ -
RIGHT-OF-WAY CONTINGENCY						of Land, Improve, and Damages 30%	\$ -
<b>TOTAL PROJECT RIGHT-OF-WAY</b>							<b>\$ -</b>
INFLATION RATE ON CONTRACT				Years		Inflation	\$ 192,253
INFLATION RATE ON PERSONNEL				1		4.5% of Construction	\$ 57,272
ESTIMATE CONTINGENCY FOR UNDEFINED OR CHANGE IN SCOPE				1		2.0% of Eng & Mgmt 10% of Const, Eng & Mgmt, and Inflation	\$ 738,540
<b>TOTAL PROJECT CONTINGENCY</b>							<b>\$ 988,065</b>
<b>TOTAL PROJECT ESTIMATE</b>							<b>\$ 8,123,937</b>

## **Comments Received**

**Project # 1B0403**

**City of Portland**

**Naito Parkway Railroad Crossing Safety Project**



**BUILDING AMERICA®**

February 22, 2016

Mr. Matthew Garrett, Director  
Oregon Department of Transportation  
355 Capitol Street NE, MS 11  
Salem, OR 97301-3871

Dear Mr. Garrett,

On behalf of Union Pacific Railroad (UPRR), I am writing to inform you of our company's acknowledgement and support of the City of Portland's ConnectOregon VI application for the Naito Parkway railroad crossing upgrades. These proposed measures will enhance safety for all users at the railroad crossing, and is expected to allow the establishment of a Quiet Zone. Completion of these enhancements will also allow the City of Portland to complete a previously funded project to add a pedestrian/bicycle crossing of Naito Parkway to the south of the tracks and extend bike lanes on Naito Parkway.

UPRR values our relationship with the City and we look forward to working with them on this project if funding becomes available. Furthermore, we pledge to work with the parties involved to ensure all plans follow UPRR's engineering standards and safety requirements. Thank you for your consideration of investment in this important project. Please feel free to contact me if you have any questions.

Sincerely,

Wes Lujan

To: Christopher Cummings, ODOT  
From: Dan Layden, Capital Program Manager, PBOT  
Date: February 10, 2016  
RE Coordination with ODOT on Naito Railroad Crossing project

The City of Portland has coordinated extensively with ODOT Rail on the Naito Railroad Crossing project. Our communications on this project date back to 2009, prior to the submittal of the project for State grant funding.

In 2009, PBOT asked ODOT Rail to review the design concept to identify any potential issues related to the Steel Bridge rail crossing. At that time, ODOT Rail responded that the project could be permitted as designed. In 2010, upon final design engineering, PBOT requested approval of a rail crossing order to go forward to construction. PBOT was informed by ODOT Rail of a new rule about pedestrian crossing spacing that prevented the crossing order from being approved.

Between 2011 and 2014 PBOT, ODOT Rail and Union Pacific Railroad met on several occasions to discuss how the design could be modified to address the new rule and allow the project to advance to construction. Developing an acceptable design alternative was very difficult due to the numerous physical constraints the project needed to work within. In 2014 an acceptable design solution was identified, but required significant additional work to the existing rail crossing, and thus could not be funded through the existing grant. This is why we are now asking for additional funding through the *ConnectOregon* program. The City of Portland is committed to delivering this project with ODOT Rail and will continue our coordination efforts as we move forward.



*The Portland Bureau of Transportation fully complies with Title VI of the Civil Rights Act of 1964, the ADA Title II, and related statutes and regulations in all programs and activities. For accommodations, complaints and information, call (503) 823-5185, City TTY (503) 823-6868, or use Oregon Relay Service: 711.*

## **Comments Received**

**Project # 1B0405**

**Tualatin Hills Park & Recreation District**

**Waterhouse Trail Segment 4**



Denny Doyle, Mayor

April 4, 2016

Region 1 Area Commission on Transportation  
c/o Scott Turnoy  
Freight Planning Program Manager  
Oregon Department of Transportation  
555 13<sup>th</sup> Street NE, Suite 2  
Salem, OR 97301

**Re: ConnectOregon VI Project #1B0405  
THPRD Waterhouse Trail Segment 4**

Dear Commission Members,

On behalf of the City of Beaverton, I want to take this opportunity to express my support for the Tualatin Hills Park & Recreation District's (THPRD) application for ConnectOregon VI grant funds to construct Segment 4 of the Waterhouse Trail. This is an important project that will complete a gap in an existing off-street, multi-use transportation network located in northwest Beaverton. This project represents a continued partnership between the City and THPRD on transportation-related trail projects. Most recently, the City has partnered with THPRD on Segment 1 of the Waterhouse Trail and its mid-block crossing of Baseline Road, as well as the design and construction of a mid-block crossing of the Fanno Creek Trail at Hall Boulevard.

The Waterhouse Trail is a vital route in the City's off-street transportation system, linking bicyclists and pedestrians to light rail and bus service, commercial and employment centers, residential neighborhoods, civic and social services, schools, park and recreation facilities and natural areas. In April 2014, THPRD completed other segments of the Waterhouse Trail – making Segment 4 the final 950-foot left unbuilt in the 5.5-mile long trail. This unfinished segment results in trail users having to navigate a nearly half-mile, on-street, out-of-direction route at the busy and dangerous Bethany Boulevard, Cornell Road and 158th Avenue intersection area.

THPRD has also recently completed segments in its other trails that link to the Waterhouse Trail – such as the Westside Trail – making completion of this project a priority not only for THPRD, but also for the City of Beaverton. Completion of this gap is critical for the creation of an off-street transportation network, which supports the City's goal of providing a diverse system of transportation options to its residents. It is my understanding from the application that this project is shovel-ready, having previously gone through design and engineering.

Region 1 Area Commission on Transportation  
April 5, 2016  
Page Two

Thank you for your time and consideration of THPRD's request for \$400,000 in ConnectOregon VI grant funds. I urge you to recommend funding for the completion of Segment 4 of the Waterhouse Trail as a top priority project of the region.

Sincerely,

A handwritten signature in cursive script that reads "Denny Doyle". The signature is written in black ink and is positioned above the printed name and title.

Denny Doyle  
Mayor



April 1, 2016

Region 1 Area Commission on Transportation  
c/o Scott Turnoy  
Freight Planning Program Manager  
Oregon Department of Transportation  
555 13<sup>th</sup> Street NE, Suite 2  
Salem, OR 97301

**Re: *ConnectOregon VI Project #1B0405***  
***THPRD Waterhouse Trail Segment 4***

Dear Commission Members,

On behalf of Washington County, I want to take this opportunity to express my support for the Tualatin Hills Park & Recreation District's (THPRD) application for ConnectOregon VI grant funds to construct Segment 4 of the Waterhouse Trail. This is an important project that will complete a gap in an existing off-street, multi-use transportation network located in northeast Washington County. In addition to providing an off-street transportation alternative for commuters, construction of this missing segment will improve county residents' access to nearby live, work and play destinations of their community.

This project represents a continued and strengthening partnership between the County and THPRD on transportation-related trail projects. As part of the County's 2013 Bethany Boulevard overcrossing project of US-26, the County partnered with THPRD to include adequate bicycle and pedestrian facilities, as Bethany Boulevard serves as the Waterhouse Trail's crossing of the highway. More recently, the County has been involved in discussions with THPRD on design and routing of the Westside Trail as part of the County's planned improvements – beginning in 2017 – to Jenkins Road, 158<sup>th</sup> Avenue and Walker Road.

The Waterhouse Trail is a vital route in the County's off-street transportation system, linking bicyclists and pedestrians to light rail and bus service, commercial and employment centers, residential neighborhoods, civic and social services, schools, park and recreation facilities and natural areas. In April 2014, THPRD completed other segments of the Waterhouse Trail – making Segment 4 the final 950-foot left unbuilt in the 5.5-mile long trail. This unfinished segment results in trail users having to navigate a nearly half-mile, on-street, out-of-direction route at the busy and dangerous Bethany Boulevard, Cornell Road and 158th Avenue intersection area.

**Board of County Commissioners**

155 North First Avenue, Suite 300, MS 22, Hillsboro, OR 97124-3072  
phone: 503-846-8617 fax: 503-846-4545

THPRD has also recently completed segments in its other trails that link to the Waterhouse Trail – such as the Rock Creek and Westside Trails – making completion of this project a priority not only THPRD, but also for Washington County. Completion of this 950-foot long gap is critical for the creation of an off-street transportation network, which supports the County's goal of providing its residents with a diverse system of multi-modal transportation options. It is my understanding from the application that this project is shovel-ready, having previously gone through design and engineering.

The importance of this project is illustrated by the County's pledge of \$300,000 from its Major Street Transportation Improvement Program (MSTIP) Opportunity Fund to THPRD. This pledge, along with THPRD's commitment of \$300,000 in system development charge funds, makes it a well-leveraged project.

Thank you for your time and consideration of THPRD's request for \$400,000 in ConnectOregon VI grant funds. I urge you to recommend funding for the completion of Segment 4 of the Waterhouse Trail as a top priority project of the region.

Sincerely,

A handwritten signature in cursive script that reads "Andy Duyck".

Andy Duyck, Chair  
Washington County Board of Commissioners

## **Comments Received**

**Project # 5B0389**

**Eastern Oregon University**

**La Grande/EOU Grand Staircase Pedestrian Link**

Calder Loth  
Architectural Historian  
202 N. Granby Street  
Richmond, Virginia, 23220

May 16, 2016

Mr. Scott Turnoy  
Interim ConnectOregon Manager  
555 13<sup>th</sup> Street NE, Suite 2  
Salem, Oregon 97301

Dear Mr. Turnoy:

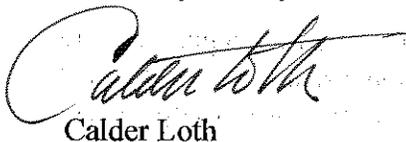
In March of last year I was contacted by an Oregon group working to save the Grand Staircase on the Eastern Oregon University campus at La Grande, and asked to provide an assessment of its architectural significance. I was known to the group through my serving on the Advisory Council of the Institute of Classical Architecture and Art in New York and as the retired Senior Architectural Historian of the Virginia Department of Historic Resources. I am also President of the Center for Palladian Studies in America.

I had no previous knowledge of the staircase and appreciated being made aware of this extraordinary work. After considerable thought and research, I believe I can state with confidence that this is a design unique in America. No other place in the country can boast a monumental exterior staircase of its scale, complexity, and beauty.

It was discouraging to learn that the staircase has suffered structural failure due to lack of maintenance and has had to be closed because of safety concerns. It is especially unfortunate that it can now no longer serve as the pedestrian connection between the university and the community. The historic photographs provide striking evidence of the great affection held by many for staircase when it was accessible and properly maintained. It was meant to be a true landmark, a place to enjoy and promote, and can be again, just as the famous Spanish Steps in Rome.

Financial support for the staircase restoration would be an important gesture of stewardship for a unique facet of Oregon's cultural heritage, one that would draw visitors to the community and be a focus of community pride as it once was and was intended to be.

Thank you for your consideration of this opportunity.



Calder Loth

RECEIVED MAY 19 2016

Arlene Young  
96 Penn Avenue  
La Grande, OR 97850  
May 12, 2016

Scott Turnoy  
Interim ConnectOregon Manager  
555 13<sup>th</sup> Street NE, Suite 2  
Salem, Oregon 97301

Grand Staircase Pedestrian Link:

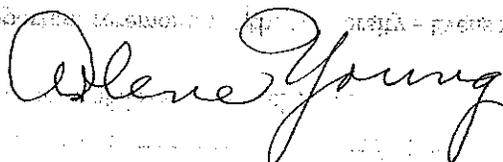
This letter is in support of ODOT funding for reconstruction of the Grand Staircase at Eastern Oregon University in La Grande. I was a student at the University in 1951 and my husband was Dean of Arts and Sciences at the time he retired after 36 years at the University. I have seen the Staircase change from being an important feature in the community as well as a beautiful entrance to the University to its present state of disrepair and closure.

At one time the Staircase was used in one of the most important ceremonies at the University -- Evensong. Every year in June as a part of the Commencement activities a public ceremony was held on these steps that was attended by townspeople, people from throughout the region as well as visiting relatives of students. The streets below the staircase were closed and folding chairs set up to accommodate all who came to witness this beautiful ceremony. At dusk all of the students of the college gathered on the staircase and sang to the community. The college orchestra was arranged at the foot of the staircase and with a lovely ceremony including a queen and court, the graduates lit candles signifying the light of knowledge and processed down the staircase into the town to share their light with the world and undergraduates would return to Inlow Hall to further their education. In the evening those flickering lights were very impressive.

Throughout the year the stairs were well used by students and townspeople as access to events at the college, as well as a gathering place for visiting, looking at the view and just enjoying this grand structure. In earlier days the students who attended Ackerman used the stairs as their main access to classrooms since they came from all over town. Parents signed up their children at birth so they could be candidates for acceptance at this laboratory school.

Not only was the structure beautiful it was an important entrance to the stately buildings and represented stability and permanence.

Having lived in La Grande for all these years I hear often of the regret voiced by all in the area, and those who return to campus that this wonderful staircase is closed. So many people are now walking to many destinations for shopping, exercise and health, as well as just enjoying the magnificent early structures in our area that it seems a shame to lose this stately landmark.



RECEIVED MAY 19 2016

**The following petition was generated by Save the Grand Staircase regarding project # 5B0389 and submitted on behalf of those listed.**

May 16, 2016

Scott Turnoy  
Interim ConnectOregon Manager  
555 13th Street NE, Suite 2  
Salem, Oregon 97301

Dear Mr. Turnoy,

We are writing to encourage ODOT to allocate a portion of the 2016 ConnectOregon funds to the La Grande/EOU Grand Staircase Pedestrian Link.

Indeed, some of our desire to see the Grand Staircase saved/reconstructed comes from cherished memories of this architectural treasure. However, we also see a much bigger picture – one that includes economic development, the safe and improved flow of pedestrian traffic, and a more livable community.

Reconstructing the Grand Staircase would reestablish the decades old pedestrian connection between the university and downtown La Grande that was lost when the steps were closed in 2004. This connection would not only provide a safe and efficient (not to mention beautiful and architecturally significant) way for pedestrians to go to and from the EOU campus, it would also stimulate the local economy by encouraging students, visitors to the campus and others to walk downtown for a meal or a movie or to spend their dollars at local shops.

Thank you for your consideration.

Sincerely,

*Joanne Graham*

*Kendal Obermeyer*

*Kathy Gray*

*Linda Bishop Hartig*

*Barb Malone*

*Keith Orr*

*Jody Craig*

*Sherie Lacrosse-Zack*

*Connie Carter*

*Nell Klumph*

*Chris Lockhart-Turner*

*Joyce Smith Martinez*

**RECEIVED MAY 18 2016**

*Raye Weatherford*

*Kelly Walker*

*Craig Ritter*

*Rosa Linkert Blain*

*Dave Van Klinken*

*Greg Blain*

*Kelly Tolman*

*Larry Schacher*

*Kathy Drummond*

*Jen Kind*

*Catherine Herrmann*

*Mary Parmer*

*Walt Blackman*

*Haze Young*

*Teresa Knapp*

*Ann Stewart Taylor*

*Lalessa Wickam-Wyatt*

*Dennis H. Coalwell*

*Debbie Talley Gorte*

*Debbie Hermann-Jederberg*

*Richard W Galloway*

*Melissa Lea*

*Debbie Davey*

*Theresa Arnson*

*Megan Banes*

*Suzanne Young*

*Jana Kilpatrick Ailes*

*JoAnna Kostoff Madsen*

*Ben Lisa Hill*

*Michelle Lowry Babb*

*Ember Sho'le Farnam*

*Rachel Ann*

*Judy Kahle*

*Lea Over*

*Karen McFarlane Holman*

*Twilla Petersohn*

*Linda Witten*

*Christian Steinmetz*

*Sunny Kumagai*

*Kim Navratil*

*Angie Dierdorff*

*Randy Shaw*

*Darcia Stone*

*Arlene Young*

*Katie Fitzgerald Curtis*

*Kristy Drury*

*Peggy Young*

*Justin Griffith*

*Linda S Roper*

*Jim Kretschmer*

*Terrence Hulse*

*Thomas Beeman*

*Dan Gassoway*

*Sharri Baker Anderson*

*Connie Williams*

*Phil Redman*

*Nancy Spradlin*

*Joyce Weimer McBride*

*Jeff Crews*

*Robin Rawlings*

*Kimberli Aldrich Denton*

*Tory Hamann Brixey*

*Cheryl Gregory Miles*

*Dennis H. Coalwell*

*JoAnne Gange*

*Debbie Crisp*

*Connie Haag*

*Dana Bevell*

*Desiree See*

*Amanda Herrmann*

*Mary Brock*

*Marianne Sipe*

*Christine Williamson-Carlson*

*Shannon Wiseman*

*Benjamin Beickel*

*Stormie Moriah Brown*

*Ginny Lambert*

*Randy Jay Harvey*

*Katie McDougall*

*Michelle Jewel McMillan*

*Nadine Faber*

*Robby Marsh*

*Mike Hink*

*Judith Towne Gollihar*

*Sheressa Dolph*

*Kathleen Hollingshead*

*Chris Wunz*

*Tyson Brooks*

*Maria Elena Liguori*

*Betty Baker*

*Lisa Swayze*

*Meegy Parker*

*Leisa Rose Baker*

*Delores Mendiguren*

*Cyndi Morris-Crowe Hirsch*

*Brian Stave*

*Vicki Holman Fryar*

*Becca Herrmann*

*Jolene Weaver-Ball*

*Becky Young Saltenberger*

*Camille Stanberry*

*Suzanne Young*

*Julie Bean*

*Charlene Counsell*

*Frances Clason*

*Genie Seymour*

*Andrea Waldrop*

*Richard Warren Scott*

*Doug Beers*

*Amanda Schaffeld*

*Denise Crader*

*Monica Bird*

*Jeanie Gallagher*

*Bob W. Gregory*

*Debbie Greer*

*Kathleen Mills*

*Kellie Sun Loveless*

*Stephanie Rogers*

*Erin Ahner*

*Gary Carman*

*Colleen Bynum*

*Georgene Boyd*

*Shawn Rothwell Power*

*Loren Davis*

*Suzen Zweifel Fors*

*Trenton Jones*

*Aaron Mai*

*Sheila Balderston-Young*

*Monica Clancy*

*Cindy Simmons Sherman*

*Janice Weaver McLaughlin*

*Sean Lough*

*Tom Craig*

*Ernie Winterton*

*Vicky Lynn Sullivan*

*Anthony Marks*

*Benamina Harmon Balmer*

*Ed Lund*

*Peggy Odegaard*

*Amy Smith*

*Lori Trullinger Presley*

*Deborah Wilson*

*Pat Pridgen*

*Kathryn Tibbs*

*Arielle Corson*

*Suzanne Goodall*

*Tiffany Crites*

*Sandi Jacobson*

*Mitch Williams*

*Michael Donnell*

*Tom Dalton*

*Summer Steele*

*Emi Kelly*

*Jacque Keeling Van Scyoc*

*Helen Moore*

*Andrea Rhoton*

*Beckie Thompson*

*Karen Goodwater*

*Cheri Dohnal*

*Cindy Bird Kastl*

*Melody Hayden*

*Kara Cheney Rudd*

*Heidi Vann*

*Amanda Kenworthy*

*Jason Griffiths*

*James Winn*

*Tina Kearns*

*Kris Walton Steeves*

*Sally A Nusser*

*Melissa Neil*

*Becky Bechtel*

*Karli Nebeker Tolman*

*Barbara Wilson Wells*

*Rebekah Nash*

*Lori Honeywell Allen*

*Kim Taylor Wilber*

<i>Gary K Wright</i>	<i>Cynthia McManus</i>
<i>Thad Miller</i>	<i>Jason Evans</i>
<i>Felipe T O Sanchez</i>	<i>Jana Miller Gruis</i>
<i>Kevin Patrick</i>	<i>Sandra Pattin</i>
<i>Lindsey Venutolo</i>	<i>Susan Krieger Gaines</i>
<i>Carrie Spencer</i>	<i>Bryan Charlton</i>
<i>Pamela Scott</i>	<i>Laura Ellis</i>
<i>Fred Walker</i>	<i>Bobi Richardson</i>
<i>Erin Perkins Winterton</i>	<i>Bonnie Dunn</i>
<i>Georgia Cook</i>	<i>Carrie Chicken</i>
<i>Casey Ann Townsend</i>	<i>Julie Ewing</i>
<i>Stacy Huffman Thompson Shown</i>	<i>Lisa Anderson</i>
<i>Tisha Butler</i>	<i>Glenis Harrison</i>
<i>Roberda McCumber</i>	<i>Kate Rose</i>
<i>Wendy Rush Knudson</i>	<i>Paul F Howell</i>
<i>Jessica Mallory</i>	<i>Misty Mellinger-Slater</i>
<i>Amy Bristol</i>	<i>Mary Phiroz</i>
<i>Evle N. Mugrage</i>	<i>Aaron Archuleta</i>
<i>Andy Altenburg</i>	<i>Brandi Richardson</i>
<i>Clint Williams</i>	<i>Jane Fox</i>
<i>Hugh McClellan</i>	<i>Melissa Lea</i>
<i>Becky Mac Donald Coles</i>	<i>Mary Rambo</i>

*Joshua Axelrod*

*Bob Myers*

*Jayne Lee Presley*

*Kim Wood Nolan*

*J Michael Frasier*

*Kevin Ludviksen*

*Shelley Page*

*Cindy Hodge Schaures*

*Karen Jean Mathson*

*Rebecca Lester*

*Greg Plass*

*Phyllis Hart*

*Kristin Miller Crowson*

*Bryan Laughlin*

*Karisa Mata*

*Linda K. Rogers*

*Amy MacKay*

*Dashia Jones*

*Dawn Garity-Gross*

*Laurie Winters Byerly*

*Rebecca Grant-Johnson*

*David Sherburn*

*Jana Pratt Warren*

*Beverly Brennan Beach*

*Lance Nielsen*

*Jan McDaniel*

*Kristal Ullman*

*Kathi Wiggins*

*Terry LaCoss*

*Amy Coalwell Roseborough*

*Loren Shaw*

*Carrie Rambo*

*Keri Miller Hagerman*

*Ava JoAnne Skillings*

*Lesla Edens Jones*

*Darlene McCall*

*Tammy Urquhart*

*Amarae Wright*

*Barb Garner*

The names above are those of individuals who have joined the Save the Grand Staircase effort through social media and asked that their names be included as signatures on this letter.



Shirley Davenport  
213 CR 469 Poplar Bluff, MO 63901  
davenport@tcmax.net  
573-686-2836

May 10, 2016

Scott Turnoy  
Interim ConnectOregon Manager  
555 13<sup>th</sup> Street NE, Suite 2  
Salem, Oregon 97301

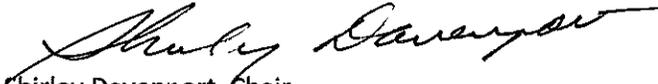
Dear Mr. Turnoy:

We have received information from Marcia Loney and Anne Olson regarding a project of "Save the Grand Staircase" efforts for Eastern Oregon University. This letter is written to encourage you to carefully consider and support the efforts of that project.

We recently completed a similar project in Poplar Bluff, Missouri through the efforts of the local citizens and also importantly grants through the Missouri Department of Transportation. Our project was one of historical significance because Poplar Bluff has been a railroad thoroughfare for many, many years. Our 1910 Grand Staircase was listed on the National Historic Register and was of significant importance for transportation and a gateway to the local depot which was on a lower level than the Main Street to which the Grand Staircase led from the depot. Tourists and local pedestrians travel up and down daily and many, many people use the area as a photography location. Dedication for the newly completed Grand Staircase is scheduled for May 13, 2016.

We, again, ask that you carefully consider the Oregon project. Economic benefits were achieved through construction and the continued use of the Grand Staircase. We are still reviewing all of the economic impact to the community.

Thank you,

  
Shirley Davenport, Chair  
Depot Grand Staircase Committee

Cc: Marcia Loney  
Anne Olson

RECEIVED MAY 16 2016

13755 SW Loney Lane,  
Hillsboro, OR 97123

May 11, 2016

Scott Turnoy  
Interim ConnectOregon Manager  
555 13th Street NE, Suite 2  
Salem, Oregon 97301

Dear Mr. Turnoy,

The purpose of this letter is to encourage ODOT to give strong consideration to the La Grande/EOU Grand Staircase Pedestrian Link application for 2016 ConnectOregon funds. I believe that reconstruction of the staircase holds great promise for ***significantly boosting the economy of La Grande, eastern Oregon and the entire state.***

This is due to the potential for and previously untapped opportunities in **cultural heritage tourism.**

When I first became involved with the Save the Grand Staircase effort I did so out of love for a treasured part of my childhood. However, I soon learned that EOU's "college steps" were much more than that.

While researching information for the Save the Grand Staircase blog I began asking myself just how "grand" the staircase is and if it were possible that it is actually "one of a kind". To answer the question we reached out to a network of Architectural Historians. The essence of their responses can best be summed up by what we heard from noted Architectural Historian Calder Loth who told us that, after searching his memory as well as various published sources he could think of/find ***no other comparable monumental exterior staircase anywhere in the United States.*** WOW.

It takes little imagination to see the potential here. With something as simple as signs on the freeway or a more involved carefully crafted cultural heritage tourism strategy, the Grand Staircase by itself or combined with other area points of interest (the Upper Perry Arch Bridge comes to mind) could easily attract visitors from elsewhere in the state, the country and potentially the world. La Grande is one of the entrance points to the Hells Canyon Scenic Byway, yet many travelers take Exit 262 from I84 bypassing La Grande. Raising awareness of the national architectural significance of the Grand Staircase could change that.

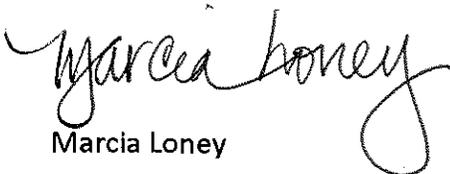
RECEIVED MAY 16 2016

Additionally, keep in mind that...

- Survey after survey has shown that cultural heritage tourists often stay longer and spend more money than other tourists. One study found that a culture and heritage tourist spends as much as *38% more per day and stays 22% longer*.
- And, a 2012 study of cultural heritage travelers organized by the Oregon Heritage Commission revealed that in Oregon these tourist spend nearly *60 percent more* than the national average.
- Presenting the Grand Staircase as place to come and see is something new. Although it was completed in 1929, it is relatively unknown outside of eastern Oregon and even La Grande. It is perhaps Oregon's best kept secret and therefore has great potential for attracting tourists who have already "been there done that" elsewhere throughout the state.

I know that there is fierce competition for Connect/Oregon funds and I do not envy those who have to choose. Thank you for your consideration.

Sincerely,



Marcia Loney

1518 N Highland St  
Portland, OR 97217

May 8, 2016

Scott Turnoy  
Interim ConnectOregon Manager  
555 13th Street NE, Suite 2  
Salem, Oregon 97301

Dear Mr. Turnoy,

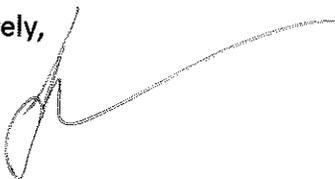
I am writing in regards to the request for 2016 ConnectOregon funds to help reconstruct the Grand Staircase at Eastern Oregon University in La Grande. As ODOT makes their decision I hope that you will consider the term "*walkability*". It is certainly one we hear a lot these days.

After discovering and falling in love with the staircase on Restore Oregon's Most Endangered Places List I traveled to La Grande so I could see it for myself. While there, I also took some time to tour the EOU campus and see a bit of the downtown area. As I did, it occurred to me that the closure of the Grand Staircase has negatively impacted the walkability of the area between the campus and La Grande's core shopping area. In a university town this is exactly the area that you would expect/hope to be one of the most walkable – encouraging a synergistic "town to gown" environment and the accompanying economic benefits.

When I heard that the project had applied for ODOT ConnectOregon funding I just had to write. Reconstructing the Grand Staircase would reopen an important (and beautifully unique) pedestrian link treasured by so many and be a big step towards a more walkable community.

Thank you for your time.

Sincerely,



Jennifer Kind

RECEIVED MAY 11 2016



RECEIVED MAY 09 2016

## EASTERN OREGON UNIVERSITY

May 3, 2016

Scott Turnoy  
Interim ConnectOregon Manager  
555 13th Street NE, Suite 2  
Salem, Oregon 97301

Re: Grand Staircase ADA Accessibility

Dear Mr. Turnoy:

I understand that ADA accessibility has come up at both the regional and modal reviews of our ConnectOregon Grand Staircase proposal and would like to provide additional information.

When funds become available for project design, we will be working closely with the State Historic Preservation Office (SHPO) to assure that reconstruction--the only preservation option available due to the irreparable state of the current structure--meets the requirements for preservation of National Register properties as required by the National Historic Preservation Act (NHPA). Due to their historic nature, NHPA requirements often preclude accessibility features which would otherwise be required by ADA.

While it would not be in keeping with the historic character of the Grand Staircase to add ramps or a mechanical elevator within the hillside, the View Terrace at the top and the base of the staircase will both be fully wheelchair-accessible via on-grade access from adjacent accessible parking. Additionally, the View Terrace is also accessible via a grade-level elevator within adjacent Inlow Hall.

These mitigation features will allow those unable to climb the staircase to enjoy the view from the top and experience this special architectural treasure up-close. Additionally, the rebuilt Grand Staircase will meet the criteria for stairway construction as defined by the Oregon Structural Specialty Code Chapter 10.

Sincerely,

A handwritten signature in black ink, appearing to read 'D. M. Lageson', written over a horizontal line.

David Lageson  
Director, Facilities Management and Planning  
Eastern Oregon University.

RECEIVED MAY 09 2016

Scott Turnoy  
Interim ConnectOregon Manager  
555 13th Street NE, Suite 2  
Salem, Oregon 97301

To Whom It May Concern:

I am writing in support of the La Grande/EOU Grand Staircase Pedestrian Link submitted by Rick Paradis, Director of Development. As a local retail business owner, this project has long been in my mind as having a great deal of potential to build the connection between EOU and the downtown community, a problem which has been in discussion in many parts of the community for several years without a lasting solution. While this effort may not be the "silver bullet" many are looking for, the project brings with it a unique value that others have not – the development of a lasting infrastructure to encourage foot traffic activity for the near term and provide an improved "pipeline" for further efforts to come.

EOU may be a draw to the area for new students, faculty and staff, but the opportunity persists to better connect with the existing community in La Grande (and beyond). I have a number of friends working at the university in various staff and faculty positions and can say with great confidence that downtown business owners would benefit a great deal not just from student financial support, but from the university culture. Further, the downtown business community offers employment, internships and a multitude of other learning opportunities. Saying that we have much to learn from each other is a gross understatement.

It shouldn't be surprising that the La Grande economy is still coming back from the damage done in 2008. While that includes filling empty storefronts, it also means investing in the unique history of the area. Projects have grown in scale from the refresh of the public square to a full restoration of a long buried 400 seat theater from the early 20<sup>th</sup> century. The staircase is a very similar extension of these efforts – in order to realize our potential, we have to build the most unique components of our DNA.

While the Northeastern Oregon region covers a broad geographic area, the culture and economy of the communities within are tied closely together. With the scale of its economy and the university, La Grande plays an integral role in that framework in both driving employment and education at the same time. While this may seem a comparatively small effort relative to the continued holistic vitality of the region, I cannot think of a project more representative of the strategy so many of us are driving towards.

Many thanks,  
Jim Whitbeck  
Blue Mountain Outfitters  
310 880 2991  
1124 Adams Ave  
La Grande, OR 97850

May 6, 2016

Scott Turnoy  
Interim ConnectOregon Manager  
555 13th Street NE, Suite 2  
Salem, Oregon 97301

Dear Mr. Turnoy:

In the early 1970s, I worked as a reporter and photograph on *The Observer* daily newspaper of La Grande, Ore.

It was during my time in La Grande and for *The Observer* that I first laid eyes on the Grand Staircase of what was then Eastern Oregon College and is now Eastern Oregon University.

For *The Observer*, I took photos during one of the wonderful Evensong events held on the Staircase. While Evensong was a production of the University it was an appreciated and well-attended community event.

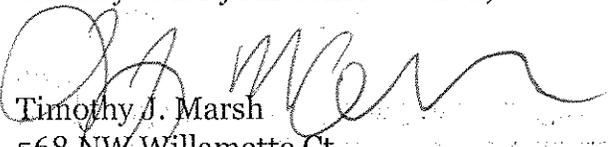
It has been some 45 years since La Grande has been my home, but I've never forgotten the Grand Staircase. When I'm in La Grande, the Staircase is always a "must" place to visit.

The Staircase is not a bridge, but it certainly is a bridge between the university and the city. What a beautiful and graceful "bridge" it is, connecting La Grande and, up the Staircase, the university on its plateau.

I encourage the Oregon Department of Transportation to allocate ConnectOregon funds for the Grand Staircase reconstruction. Doing so will create improved pedestrian "traffic" flow between the campus and city.

A reconstructed Grand Staircase should be noted on signage on Interstate 84 so those traveling through Union County will be sure to visit it. Eastern Oregon University and La Grande would reap the economic benefits through visitors who patronize the University and La Grande's business community.

Thank you for your consideration,

  
Timothy J. Marsh  
568 NW Willamette Ct  
McMinnville, Oregon 97128

**Comments Received**

**Project # 2M0361**

**Port of Newport**

**International Terminal Shipping Facility**



Comments to the Region 2 Review Committee on Port of Newport Application  
May 25, 2016

Thank you for the opportunity to provide comment on the applications under review today. My name is Doug Cooper. I am Vice President of Resources for Hampton Affiliates, a lumber company headquartered in Portland, OR. Hampton is a family owned business operating three sawmills in Oregon located in Willamina, Tillamook, and Warrenton. These mills, and the over 600 workers they employ, rely on a stable local timber supply.

One of the applications under your review, the Port of Newport venture, will directly threaten the ability of these mills to continue operation. The Port of Newport's project would utilize funds to develop an area the Port intends to use as a log yard to facilitate the export of logs to destinations including China. The Port of Newport's communication and promotional materials have been altered through the application process to highlight or hide the plan to export logs. The last presentation made before the Cascades West Area Commission on Transportation was scrubbed clean of any reference to the export of logs and log handling capability.

I want to emphasize that there is a log shortage in Oregon. Local buyers exist for Oregon's timber and it will be harvested and sold whether the Port of Newport exports logs or not. The Mary's River Lumber sawmill in Philomath, Oregon recently announced the closure of that sawmill as well as their mills in Washington due to a lack of log availability. Numerous other mills have closed in the last 6 months in both Oregon and Washington. The Port of Newport project will negatively impact the sawmill and related non-sawmill employment in neighboring communities. For this reason, I want to urge you to consider the available facts surrounding the project and determine the project is not deserving of ConnectOR VI funds intended to benefit the State of Oregon.

The Port of Newport Analysis Benefit claims are primarily based in savings resulting from the substitution of transportation to other shipping destinations. There are not an additional 10-15 days per month of incremental jobs to the state of Oregon, but simply a redistribution of work, by taking jobs from one delivery point and giving them to another.

Many people are not aware approximately that 70% of a log is converted into lumber and the remaining 30% of a log is produced as bark, wood chips, sawdust, and wood shavings. These products are sold to nearby pulp and paper mills including the mills in Toledo and Halsey, OR, particleboard mills including the mill in Albany, OR, wood pellet mills(including the plant in Brownsville, OR, as well as agricultural uses and landscape product manufacturers. The export of raw logs weakens Oregon's wood manufacturing sector and sectors utilizing wood byproducts, and costs rural communities jobs and revenue derived from the wide variety of value-added products.



EXECUTIVE OFFICE

## HAMPTON LUMBER MILLS, INC.

9600 SW Barnes Road  
Suite 200  
Portland, Oregon 97225-6666  
Telephone 503.297-7691  
Fax 503.203-6618  
[www.HamptonAffiliates.com](http://www.HamptonAffiliates.com)

The Port of Newport has also cited environmental, financial, and social benefits resulting from the project due to a claimed reduction of CO2 emissions by delivering to Newport as compared to other domestic locations. Delivery of logs to the Port of Newport for export is equivalent to the delivery to a sawmill, however the complete log export delivery cycle does not end until the log is loaded onto a vessel, reaches a distant port, and is offloaded before delivery to a manufacturing plant. The evaluation of environmental benefits cited in the application must accurately assess the complete delivery cycle of all products shipped, and weigh the environmental costs of the cycle that may more than offset the claimed benefits.

Hampton supports economic development that comes from expanding trade. However, few including our own state and federal officials, are supportive of exporting raw materials where the maximum economic benefit to our communities, rural and urban, is clearly by sustaining the value-added manufacturing of that raw material here in Oregon. It is for this reason that harvested logs from state and federal land are restricted from export.

The analysis and rating work completed to date by the Oregon Department of Transportation, the Marine Modal Committee, the Oregon Freight Advisory Committee, and the Cascades West ACT, has resulted in a ranking of projects across Region 2. It is your charge to ensure the ranking of each application is appropriate based on the true benefits of the project to the entirety of Region 2 and the State of Oregon.

Thank you for your time and consideration.

**Peter M. Bregman**  
**Eagle Roost Tree Farm**  
**PO box 1808**  
**1679 N. Bay View Dr.**  
**Waldport, OR 97394**  
**Ph # 541-563-6428 Cell #209-604-7306**

RECEIVED APR 05 2016

April 3, 2016

Freight Planning Program Manager  
Scott Turnoy  
555 13<sup>th</sup> Street NE, Suite2  
Salem, Or 97301

Dear Mr. Scott Turnoy:

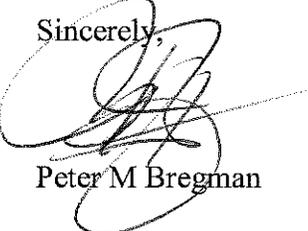
My family and I are small timberland owners in Lincoln County. We would like to give our strong support to the Port of Newport's Terminal Shipping Facility (TSF) EDA and Connect Oregon VI grant applications. Upon completion, the TSF would give us a much needed market for our sustainably managed forest products. The availability of another market opportunity would have many benefits to our business.

Those benefits would be:

- a. Increased competition for our product which would allow us to realize a better return on our investment in our product. Recent forestry harvest regulation changes will make it all the more important that we get the best price possible for our products.
- b. We would see a reduction in our overall trucking costs. Once completed, the TSF would be the shortest haul for us to get our products to an export market. Our current options are more costly and less efficient. Our use of the TSF would utilize the soon to be completed US 20 realignment, and lessen the amount of trucks on 1-5 travelling North and South.
- c. The income generated from our business benefits local businesses and families. We try and use local companies and businesses whenever possible, which create jobs and keep people working in Lincoln County. It is very important to us that we not only reinvest in our land but also our community.
- d. Our business is family owned and the investment in our forest is 30-50 years. The opportunity that the TSF will provide by creating a better market for our products will allow us to realize the best possible return on our long term investment and will allow us to reinvest in our future. The success of our business will not only benefit us but also the State of Oregon.

Thank you for considering the benefits to Oregon's local small timberland owners that the TSF will provide.

Sincerely,

  
Peter M Bregman

## TURNOY Scott

---

**From:** Kristin Rasmussen <KristinRasmussen@HamptonAffiliates.com>  
**Sent:** Thursday, March 24, 2016 4:38 PM  
**To:** TURNOY Scott  
**Subject:** Port of Newport International Shipping Terminal  
**Attachments:** Raw Log Exports to China trends.pptx

Hi Scott,

I understand ODOT requested further information from the Port of Newport in regards to their ConnectOregon VI application and that the Port has responded and provided attachments (A-F), including private sector letters of commitment. Would it be possible to get copies of those attachments?

We appreciated ODOT's inquiry into the impacts of a declining log export market. Along those lines, I wanted to share with you a chart demonstrating the two-year trend for vessels arriving in China with raw log exports from the U.S.

Thank you,  
Kristin



**Kristin Rasmussen**

*Public Affairs & Communications Manager*

Hampton Affiliates

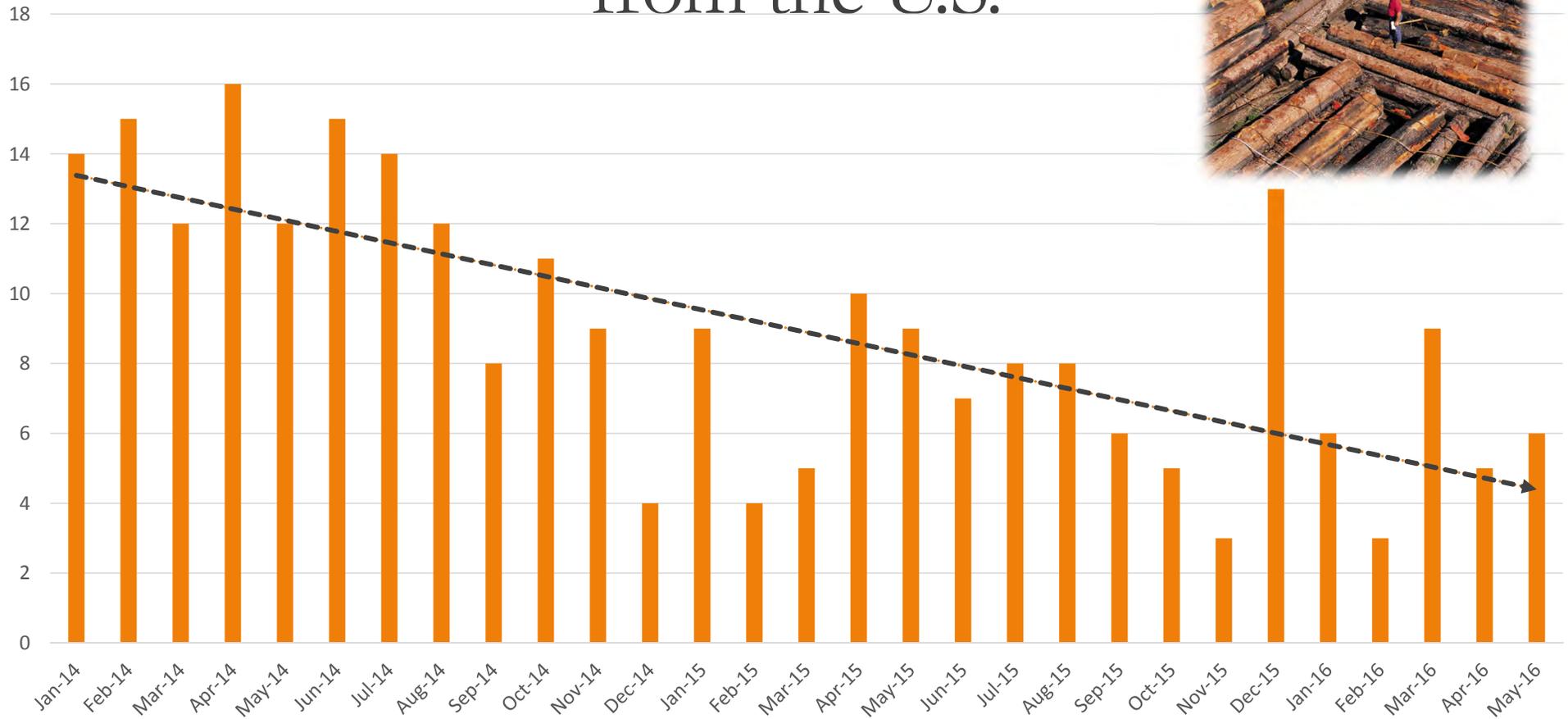
9600 SW Barnes Road, Suite 200

Portland, OR 97225

Tel. 503-203-6563

Fax 503-203-6604

# Vessels arriving in China with Raw Log Exports from the U.S.



RECEIVED MAR 17 2016

**Scott Turnoy**  
*Freight Planning Program Manager*  
Oregon Department of Transportation  
555 13<sup>th</sup> Street NE, Suite 2  
Salem, OR 97301

Dear Commission Chair and Members,

My family and I are small timberland owners in Lincoln County. We would like to give our strong support to the Port of Newport's Terminal Shipping Facility (TSF) Connect Oregon VI grant application. Upon completion, the TSF would give us a much needed market for our sustainably managed forest products. The availability of another market opportunity would have many benefits to our business.

Those benefits would be:

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- We would see a reduction in our overall trucking costs. Once completed, the TSF would be the shortest haul for us to get our products to an export market. Our current options are more costly and less efficient. Our use of the TSF would utilize the soon to be completed US 20 realignment, and lessen the amount of trucks on I-5 travelling North and South.
- The income generated from our business benefits local businesses and families. We try and use local companies and businesses whenever possible, which creates jobs and keeps people working in Lincoln County. It is very important to us that we not only reinvest in our land but also our community.
- Our business is family owned and the investment in our forest is 30-50 years. The opportunity that the TSF will provide by creating a better market for our products will allow us to realize the best possible return on our long term investment and will allow us to reinvest in our future. The success of our business will not only benefit us but also the State of Oregon.

Thank you for considering the benefits to Oregon's local small timberland owners that the TSF will provide.

Sincerely,

A handwritten signature in black ink, appearing to read "Peter Bregman", enclosed within a large, loopy circular scribble.

*Peter Bregman*  
*P O Box 1808*  
*Waldport, OR 97394*

Thomas and April Newton

908 Reserve St. Silverton, OR 97381

March 16, 2016

Dear Commission Chair and Members,

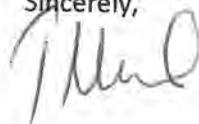
My family and I are small timberland owners in Lincoln County, owners of 100 acres, and co-owners of 300 more acres. We would like to give our strong support to the Port of Newport's Terminal Shipping Facility (TSF) Connect Oregon VI grant application. Upon completion, the TSF would give us a much needed market for our sustainably managed forest products. The availability of another market opportunity would have many benefits to our business.

Those benefits would be:

- Increased competition for our product which would allow us to realize a better return on our investment in our product. Recent forestry harvest regulation changes will make it all the more important that we get the best price possible for our products.
- We would see a reduction in our overall trucking costs. Once completed, the TSF would be the shortest haul for us to get our products to an export market. Our current options are more costly and less efficient. Our use of the TSF would utilize the soon to be completed US 20 realignment, and lessen the amount of trucks on I-5 travelling North and South.
- The income generated from our business benefits local businesses and families. We try and use local companies and businesses whenever possible, which creates jobs and keeps people working in Lincoln County. It is very important to us that we not only reinvest in our land but also our community.
- Our business is family owned and the investment in our forest is 30-50 years. The opportunity that the TSF will provide by creating a better market for our products will allow us to realize the best possible return on our long term investment and will allow us to reinvest in our future. The success of our business will not only benefit us but also the State of Oregon.

Thank you for considering the benefits to Oregon's local small timberland owners that the TSF will provide.

Sincerely,



## **TURNOY Scott**

---

**From:** Russ and Linda Glascock <rgranch@pioneer.net>  
**Sent:** Tuesday, March 15, 2016 4:19 PM  
**To:** TURNOY Scott  
**Subject:** Port of Newport's Terminal Shipping Facility (TSF) Connect Oregon VI grant application

Dear Commission Chair and Members,

My family and I are small timberland owners in Lincoln County. We would like to give our strong support to the Port of Newport's Terminal Shipping Facility (TSF) Connect Oregon VI grant application. Upon completion, the TSF would give us a much needed market for our sustainable managed forest products. The availability of another market opportunity would have many benefits to our business.

Those benefits would be:

- Increased competition for our product which would allow us to realize a better return on our investment in our product. Recent forestry harvest regulation changes will make it all the more important that we get the best price possible for our products.
- We would see a reduction in our overall trucking costs. Once completed, the TSF would be the shortest haul for us to get our products to an export market. Our current options are more costly and less efficient. Our use of the TSF would utilize the soon to be completed US 20 realignment, and lessen the amount of trucks on traveling North and South on highway 99W and I5.
- The income generated from our business benefits local businesses and families. We try and use local companies and businesses whenever possible, which creates jobs and keeps people working in Lincoln County. It is very important to us that we not only reinvest in our land but also our community.
- Our business is family owned, and we grow our forest for 40-50 years. The opportunity that the TSF will provide by creating a better market for our products will allow us to realize the best possible return on our long term investment and will allow us to reinvest in our future. The success of our business will not only benefit us but also the State of Oregon.

Thank you for considering the benefits to Oregon's local small timberland owners that the TSF will provide.

Sincerely,

Russ Glascock, Eddyville,OR



**KNAPPA  
OFFICE**

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OLD HWY. 30  
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OREGON  
97103

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97048

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(503)  
556-0410

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(503)  
556-2805

**WWW.  
Teevin Bros.  
COM**

March 4, 2016

Re; Port of Newport-Connect Oregon 6 application

Dear Connect Oregon VI Review Committee Members;

My name is Eric Oien and I serve as the General Manager of Teevin Brothers Land & Timber. Our company was started over 35 years ago by Shawn Teevin as a small sole proprietorship but today employs over 100 people at multiple locations throughout Oregon. Our business lines have become highly diversified over the past 30 years and today include multi-commodity material handling, terminal operations, trucking, rock quarries, and timberland management. Last year Teevin Brothers assisted our customers in the export of 200+ million board feet of logs to Asian markets, 180 million board feet of lumber to California and Hawaii, and 140 thousand tons of cargo to Hawaii.

The reason for my letter is to share some clarifying information regarding the Port of Newport's Connect Oregon application to build support infrastructure for the recently completed international terminal. Having personally reviewed all of the information that has been submitted in response to the application, as well as attended nearly all of the public meetings held thus far, it appears to me that there is some confusion regarding several key issues. My hope is that I can provide each of you with a few brief facts to help clear up any of the concerns you may have regarding these issues.

**"Overseas markets are disappearing"**: While log export activity has undoubtedly fallen from the peak in 2013 the demand for Pacific Northwest logs from China and Japan is far from dead. The Ports of Coos Bay, Astoria, Aberdeen, and Port Angeles have exported combined volume of 40 million board feet (7 vessels) to China alone as of the middle of February and the outlook remains table for the remainder of the year. China is currently working through a housing and credit crisis similar to what the US experienced in 2007/08 however there is little doubt amongst experts that China will continue to grow once the crisis has been passed and in turn will demand an even greater quantity of commodities, including forest products (logs & lumber), from the Pacific Northwest. The point is that even during the well publicized downturn in China there are still substantial quantities of logs being exported from the Pacific Northwest ports.

**"There is not enough timber on Oregon coast to supply mills and exporters"**: Because of advanced forest genetics and state of the art operating procedures Pacific Northwest timberlands are currently capable of producing more timber per acre than at any time in history. Within a 20 mile radius of Newport exists over 300 thousand acres of privately owned and sustainably managed timberlands that are capable of producing 134 million board feet of timber annually for centuries to come. This volume is enough to sustainably supply both the Newport terminal, with an estimated maximum annual consumption of 50 million board feet, and still generate a substantial quantity of timber for local sawmills. It is also worth noting that Federal, State, and Municipal timber is currently reserved exclusively for consumption by domestic sawmills and not eligible to be exported.



LAND & TIMBER CO.

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(503)  
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FAX  
(503)  
556-2805

•

**WWW.  
TEEVIN BROS.  
COM**

**“Export facilities are responsible for sawmills being closed and jobs lost”:** Two points on this; First, every major export dock in the Pacific Northwest has high tech thriving sawmills operating within 20 miles of export docks (i.e. Port Angeles WA, Olympia WA, Aberdeen WA, Longview WA, Astoria OR, and Coos Bay OR). In fact both Longview and Astoria had thriving export docks in existence prior to Weyerhaeuser and Hampton (Warrenton) building sawmill facilities within 5 miles of the port docks. Considering the empirical evidence it seems counterintuitive that Newport would be the only port in the Pacific Northwest where the local community and timber basket could not support both an export and sawmill industry. Second, given that only 30% of an average timber stand goes toward export it is logical to assume that higher prices for export logs at Newport will encourage additional harvest by local landowners and in turn generate additional domestic quality timber for domestic manufacturers. This exact phenomenon occurs within every highly competitive wood basket in the Pacific Northwest with the net result being more logs for both export and domestic consumption and more money returned to local landowners, workers, and communities.

**“This infrastructure is not a necessary transportation improvement”:** The proposed shipping terminal creates the critical transportation link between a \$28M deep draft dock and a \$400M state highway project. Without an area to consolidate and process cargo both the dock and the highway will never be used to their fullest potential. The transportation savings created by this facility for timberland owners will be so significant in fact that it will allow many of them to harvest timber that is otherwise uneconomical to harvest due to the high costs of transportation to far off markets. This terminal will also allow domestic mills to import timber from Alaska and Canada and improve their ability to compete with other mills that have lower resource costs. I see no project that represents a more perfect opportunity to connect the final link in a critical transportation corridor in Oregon and revitalize a community that badly needs an economic boost.

I thank you all for your time in reading this letter and I hope the information presented has proven useful as you consider the many worthwhile projects before you. . If you have further questions about this project I welcome you to call or email me any time.

Sincerely,

Eric Oien  
General Manager  
Teevin Brothers Land & Timber  
Cell: 360-880-1003  
Email: eoien@teevinbros.com

## **TURNROY Scott**

---

**From:** John W Smith <john.smith@jwses.com>  
**Sent:** Wednesday, March 09, 2016 4:10 PM  
**To:** TURNROY Scott  
**Subject:** Newport's Terminal Shipping Facility

Dear Commission Chair and Members,

My family and I are small timberland owners in Lincoln County. We would like to give our strong support to the Port of Newport's Terminal Shipping Facility (TSF) Connect Oregon VI grant application. Upon completion, the TSF would give us a much needed market for our sustainably managed forest products. The availability of another market opportunity would have many benefits to our business.

Those benefits would be:

- Increased competition for our product which would allow us to realize a better return on our investment in our product. Recent forestry harvest regulation changes will make it all the more important that we get the best price possible for our products.
- We would see a reduction in our overall trucking costs. Once completed, the TSF would be the shortest haul for us to get our products to an export market. Our current options are more costly and less efficient. Our use of the TSF would utilize the soon to be completed US 20 realignment, and lessen the amount of trucks on I-5 travelling North and South.
- The income generated from our business benefits local businesses and families. We try and use local companies and businesses whenever possible, which creates jobs and keeps people working in Lincoln County. It is very important to us that we not only reinvest in our land but also our community.
- Our business is family owned and the investment in our forest is 30-50 years. The opportunity that the TSF will provide by creating a better market for our products will allow us to realize the best possible return on our long term investment and will allow us to reinvest in our future. The success of our business will not only benefit us but also the State of Oregon.

Thank you for considering the benefits to Oregon's local small timberland owners that the TSF will provide.

Regards

John W Smith  
Legion Rd.  
Waldport, OR





February 23, 2016

Subject: Port of Newport Project “International Terminal Shipping Facility”: Concerns and Questions

*ConnectOregon* VI Review Committee Members,

I serve as CEO for Hampton Affiliates, a family-owned, vertically integrated lumber company headquartered in Portland, OR. Hampton operates eight sawmills in the Pacific Northwest, including three in Oregon located in the communities of Warrenton, Tillamook, and Willamina.

As such, I have significant concerns with the Port of Newport’s “International Terminal Shipping Facility” project application, specifically regarding the breadth of claimed benefits and unacknowledged negative economic impacts to Oregon’s sawmill communities.

As the state begins review of the *ConnectOregon* VI applications, I would like to share with you some specific questions I have related to the Port of Newport’s project and the recently completed Economic Benefit Review associated with the application.

The guidelines for the *ConnectOregon* VI review process state that Oregon Department of Transportation (ODOT) economists and Business Oregon development officers will evaluate the economic benefit of each project application. The Economic Benefit Review, which is included in the score for tiering should include a review of: 1) the application’s analytical methodology for estimating project benefits; 2) the project’s likelihood to retain or generate new distinct jobs in Oregon (not just move jobs from one part of the state to another); and 3) the project’s level of certainty to produce benefits.

After considerable recent study, we began to question whether the evaluation that currently takes place is consistent with the stated review process and sufficient for making judgments about submitted projects.

After submitting the questions outlined below to ODOT, we were informed that the agency does not conduct a level of economic review sufficient to answer our questions about the likelihood of economic benefits (or costs) or job creation potential. If these rather basic economic questions are not addressed at the ODOT economic review level, I am deeply concerned that this important analysis will be bypassed, for as the applications move to higher and broader levels of review, so too does this general level of analysis. If *ConnectOregon* reviews are indeed intended to be high level in nature, there are some basic questions that still need to be addressed, if not by ODOT in the Economic Benefit Analysis, then by the review committees to ensure accountability in the allocation of public funds.

I respectfully request all *ConnectOregon* VI reviewers carefully consider the following questions pertaining to the Port of Newport’s “International Terminal Shipping Facility”:

**Level certainty to produce benefits**

- In the applicant’s supporting materials (TIGER VII Grant Benefit-Cost Analysis), the Port of Newport estimated \$900,000 annually in Port Operating Income (new net tariff revenues). What specific port activities and products are behind this estimate?



- Are there customer commitments behind the Port's revenue estimates?
- How did the reviewers consider the geographic, infrastructure, market, and economic benefits and limitations of transporting agricultural goods from the Port of Newport to other West Coast ports?
- How much of the activity and revenue generation potential relates to log exports versus other activities such as barging agricultural products and other goods?

**Likelihood to retain or generate new distinct jobs in Oregon**

- In the last 30 years, hundreds of sawmills have closed in Oregon due to lack of logs. Since there is currently a shortage of logs available for local sawmills, how did the applicant determine that the Port's export log yard would create new jobs rather than just transfer existing jobs from local sawmills and other ports?
- How will the reviewers evaluate the negative economic impact a new log export facility will have on sawmills in neighboring Tillamook, Linn, and Polk Counties by exporting additional logs to China through Newport?

Attached for your review are two exhibits that demonstrate; 1) existing sawmill plywood and pulp mills negatively affected by additional log exports in the region; and 2) jobs created by utilizing logs for domestic manufacturing compared to export of raw logs to China.

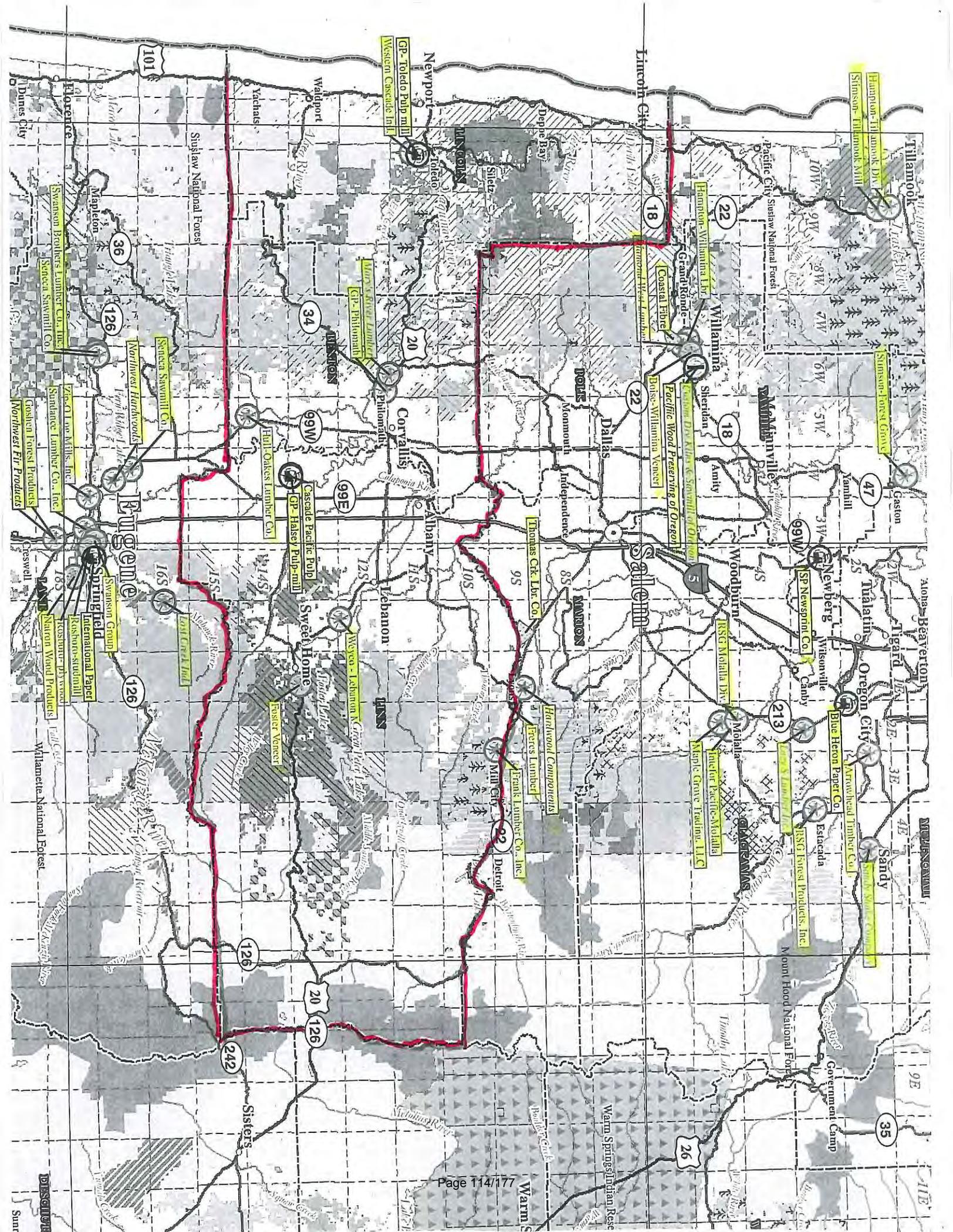
It is my hope that by acknowledging and addressing these questions, the *ConnectOregon* review committees will improve due diligence and expectations related to proposals for use of precious public funds. Thank you for your attention to this matter and for your work to improve Oregon's economy and infrastructure.

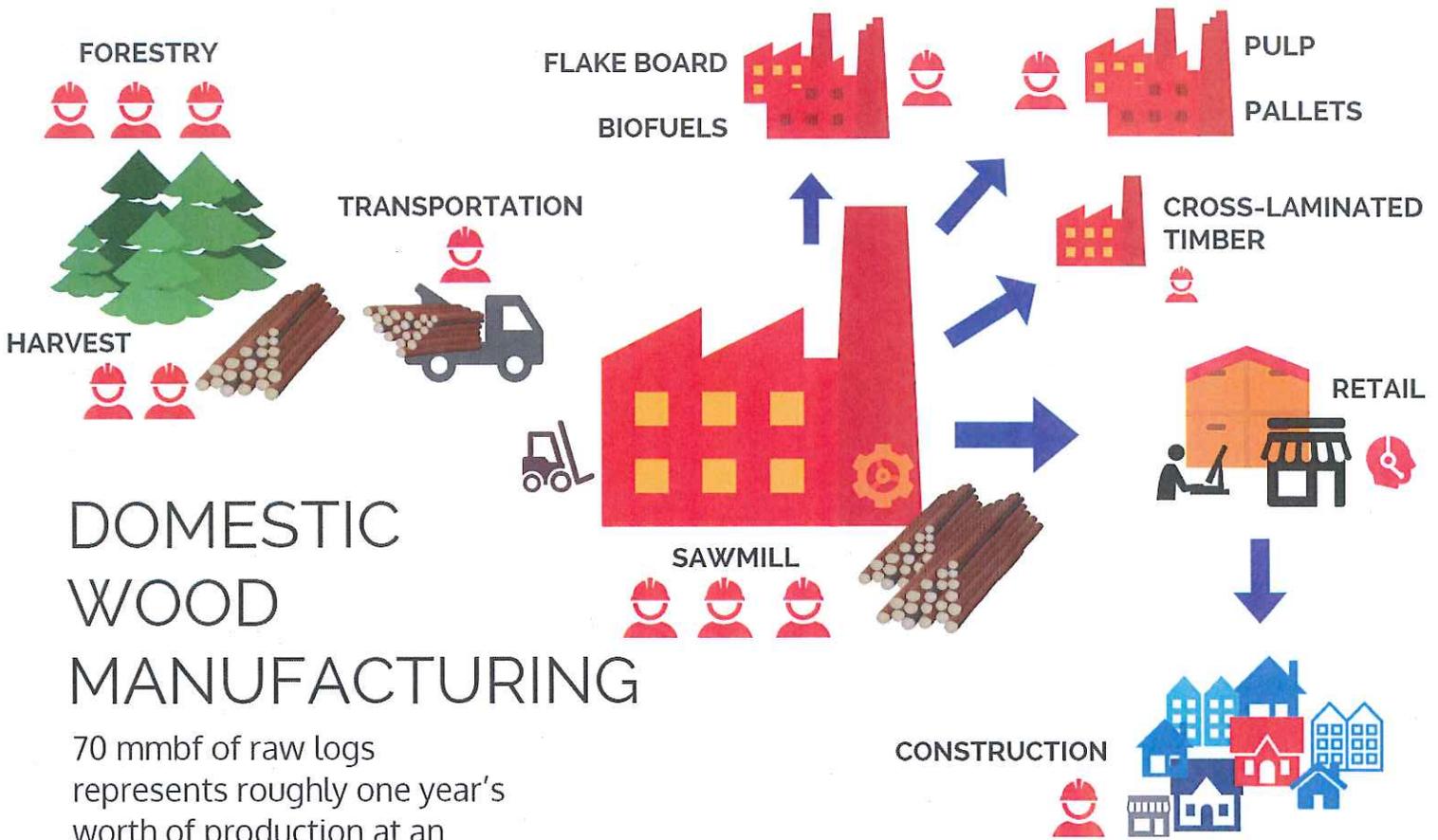
Sincerely,  
HAMPTON LUMBER MILLS, INC.

STEVE ZIKA  
Chief Executive Officer

Enclosures - 2

c: Chris Cummings, ODOT  
Tammy Baney, Oregon Transportation Commission





## LOG EXPORTS

Shipment of raw logs to Asia limits the ability of local mills to access these resources and create jobs and value-added goods here at home.



**Comments to Marine Modal Committee on Port of Newport Application  
February 18, 2016**

Thank you for the opportunity to provide additional comment on the applications under review today. My name is Doug Cooper. I am Vice President for Resources for Hampton Affiliates, a lumber company headquartered in Portland, OR. As mentioned earlier, Hampton operates three sawmills in Oregon located in Tillamook, Willamina, and Warrenton. These mills, and the 600 workers they employ, rely on a stable local timber supply. I believe one of the projects under your review will directly threaten the ability of these mills to continue operation.

For this reason, I want to urge the committee to take a careful look at the Port of Newport's International Terminal Shipping Facility application, specifically concerning the likelihood to produce economic benefit and wide-ranging opportunities. Hampton has submitted specific questions and a request for further information to the Oregon Department of Transportation and look forward to a response but I would like to take the opportunity to reiterate some of our questions and concerns here today.

In the ConnectOregon VI Economic Benefit Review, which is included in the score for tiering, reviewers were asked to examine the application's methodology for estimating project benefits, the project's likelihood to retain or generate new distinct jobs in Oregon (not just move jobs from one part of the state to another), and to examine the level of certainty to produce benefits. I do not believe the Port of Newport's application adequately meets these objectives.

The Port of Newport project would utilize funds to develop a "laydown area" which the applicant describes in presentations, documentation and internal discussions, as "the log yard" to serve the intent of exporting logs to destinations including China. The Port of Newport has not provided any substantive data or information supporting actual product metrics to generate the claimed \$900,000 of annual revenue. The TIGER Grant IMPLAN analysis included as an appendix in the ConnectOregon application claims anticipated measurement of 10-15 days per month of vessel calls. The Analysis Benefit claims are primarily based in savings resulting from substitution of transportation to other shipping destinations, thus these are not an additional 10-15 days per month of incremental jobs to the state of Oregon but simply a redistribution of work by taking jobs from one delivery point and giving them to another.

The benefit cost analysis for the project makes it clear that products, namely logs, will be diverted to the Port of Newport from existing customers. The Port of Newport's application states that the project will create 37 long-term (non-construction) jobs all of which are linked to the one log export facility. Those jobs simply substitute for the same functional job at the present customer. Those customers include the Hampton's sawmills mentioned earlier as well as



other log purchasing manufacturing plants in the region. I want to emphasize that there is a log shortage in Oregon. Oregon's timber will be harvested whether the Port of Newport exports logs or not. But the impact of this project is far from neutral. The same rationale extends to forest-related jobs. As my colleague stated in earlier remarks, this project will in fact negatively impact sawmill and non-sawmill employment in neighboring communities.

Many people are not aware approximately 70% of a log is converted into lumber and the remaining 30% of a log is produced as bark, wood chips, sawdust, and wood shavings. These products are sold to nearby pulp and paper mills (including the mill in Toledo, Oregon), particleboard mills, wood pellet mills, bioenergy plants, agricultural crop and livestock producers, and landscape product manufacturers. The export of raw logs weakens Oregon's wood manufacturing sector and sectors utilizing wood byproducts, and costs rural communities jobs and revenue derived from the wide variety of value-added products.

In addition, there is no actual data of what agricultural products will be shipped by the Port of Newport and how they will be transported. The Port of Newport's application also lacks clear and direct support from the agricultural community. Rather, when you look at the letters of support that accompany the Port's application, the private sector interests are overwhelmingly timber related. I believe these concerns were echoed in the recent Economic Benefit Review of the project, wherein one reviewer commented that "customer commitments are a concern" and "shipping of agricultural products appears aspirational at present."

The applicant has also cited environmental, financial, and social benefits resulting from the project due to a claimed reduction of CO2 emissions by delivering to Newport as compared to other domestic locations. Delivery of logs to a sawmill for manufacturing into lumber ends at the sawmill. Delivery of logs to the Port of Newport for export is equivalent to the delivery to a sawmill, however the complete log export delivery cycle does not end until the log is loaded onto a vessel, reaches a distant port, is offloaded, and then delivered to a manufacturing plant. The evaluation of environmental benefits cited in the application must accurately assess the complete delivery cycle of all products shipped, and weigh the environmental costs of the cycle that may more than offset the claimed benefits.

Hampton supports economic development that comes from expanding trade, however, few, including our own state and federal officials, are supportive of exporting raw materials where the maximum economic benefit to our communities, rural and urban, is clearly by value-added manufacturing of that raw material here in Oregon. It is for this reason that harvested logs from state and federal land are restricted from export.

I understand it is not the business of this committee or the ConnectOregon program to weigh in on economic policy for the state. It is, however, your charge to ensure projects under your review are feasible and likely to produce benefits rather than costs to Oregon's economy. I urge you to take a careful look at the Port of Newport's application and the assumptions and implications therein.

Thank you for your time and consideration.

**The following petition was generated by Hampton Lumber Mills, Inc. regarding project # 2B0361 and submitted on behalf of those listed.**

To: The Oregon Transportation Commission and  
Governor Kate Brown:

We the undersigned urge you to deny the Port of Newport's request for tax-payer dollars to develop capacity to export raw logs through its international shipping terminal. For the past several years the Port of Newport has viewed log exports as the "bread and butter" of their export terminal business plan. It is clear from the Port's application for ConnectOregon VI funding that log export interests are the only clear and committed beneficiaries of the Port's project.

Shipment of raw logs to Asia limits the ability of Oregon mills to access these resources and create jobs and value-added goods here at home and the government should not subsidize this activity. Development of the log export yard at the Port of Newport would negatively affect our three sawmills in Oregon and the more than 600 direct employees who live and work in these communities. A recent report by the Oregon Department of Forestry noted that wood products manufacturing creates three jobs per million board feet of timber, whereas logs that go overseas create fewer than one port job per million board feet.

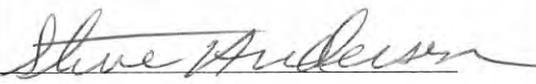
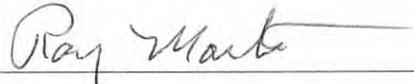
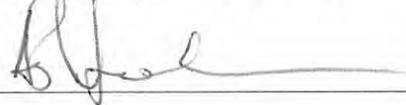
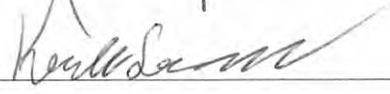
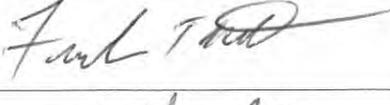
We live in a global economy and private land owners should be able to seek markets overseas but we would hope that our state managers would do no further harm to the local sawmill industry by subsidizing the export of our raw materials and the jobs and value added products that go along with them.

**Please say no to Connect VI funding for the Port of Newport's International Shipping Terminal. Don't subsidize a project that ships our jobs overseas.**

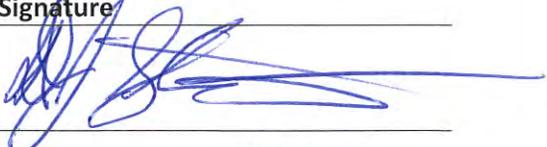
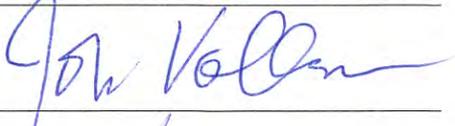
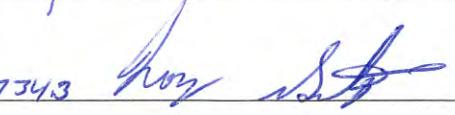
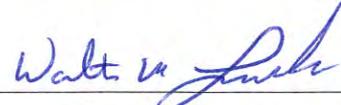
There are numerous transportation needs throughout the state. We urge you to lend your support to projects that don't have such a direct negative impact on other parts of Oregon's economy.

	Name	Address	Signature
1	Meg Commerford	PO BOX 683 Willamina OR 97396	Meg Commerford
2	Steven Hickman	601 Olive way E Monmouth, OR 97361 Sheridan	Steven Hickman
3	Arnold Rich	P.O. Box 31	Arnold Rich
4	Wesley De	660 E Ridgeway Rd Sheridan OR	Wesley De
5	Brian Little	7019 WE Coburn Dr McMinnville, OR	Brian Little
6	Garth Henshaw	3256 SW Third Ave Sheridan OR 97378	Garth Henshaw
7	Ralph Foster	838 SW 50th St McMinnville	Ralph Foster

**No Connect VI Funds for Log Exports at Port of Newport**

	Name	Address	Signature
8	Colten J. Bolton-Dakes	481 E Ellendale Ave Apt 37 Dallas, OR 97338	
9	Nick Frieder	213 NW Lalack Ct Dallas, OR 97338	
10	Margret Reed	446 SW 2nd St Sher	
11	JASON KAY	Box 516 SHERIDAN OR	
12	Steve Anderson	P.O. Box 231 Williamina	
13	RAY MARTIN	401 SE CLARK SHERIDAN OR,	
14	Yvonne Wilkerson	745 SW BATES AV Williamina OR	
15	Ashton Wilson	705 NE E ST Williamina OR	
16	Juan Ahumada	100 S 16th St Indep. OR 97351	
17	Aaron Eakins	232 SW Monroe St Sheridan, OR	
18	Mordell Salonen	232 SW Monroe St Sheridan,	
19	Michael Blake	904 S.E. Sheridan Rd. Sheridan, OR	
20	Nathan Washet	1337 SW Blaine M.M.ville OR 97145	
21	Frank Whitten	6014 Paddock Ln Sweet Home OR 97386	
22	Michael Godsey	1695 Davidson St. SE Salmon, OR 97307	
23	Kyle Anderson	1355 NW 2nd St Apt 22 M.M.ville	

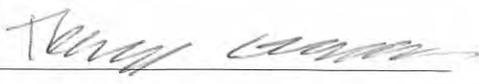
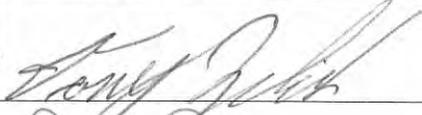
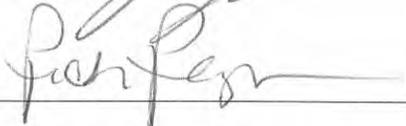
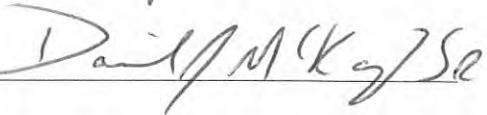
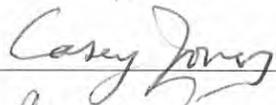
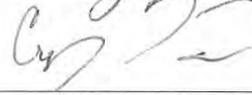
No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
88	OS Strouse	320 Sw. Hill Dr Willamina OR	
89	Wade Abbott	935 NW 2nd St. #23 McMinnville, OR	
90	John Vollman	236 SW Water Sheridan OR	
91	Troy Gretson	57300 Hebo rd Grade Ronde OR 97343	
92	Lyle Alexander	331 NE Yamhill St Sheridan, OR 97378	
93	Tim Wofford	772 SE meadows LP Sheridan, OR 97378	
94	Wes Mullins	7044 SE Amity RD	
95	Joe Augus	9600 Sw BARND Portland, OR 97221	
96	Joey Hill	132 Clay St. Monmouth OR 97361	
97	Tyler Zook	720 Sw pioneer ct Willamina, OR 97396	
98	Kyle Doyle	1240 Sw oregon Ave Dallas, OR	
99	Walter Lucks	420 South Bridge Sheridan OR 97378	
100	WALTER NUTT	669 SAGEHIVE CR INDIANAPOLIS, OR 97351	
101	Mike Patterson	1629 SE Jonathan Ave Dallas OR 97332	
102	William Nelson	38903 CARLINE Willamina OR 97396	
103	Dore Bowser	978 Ventura St Heizer OR 97303	

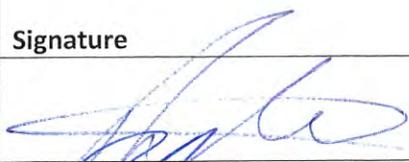
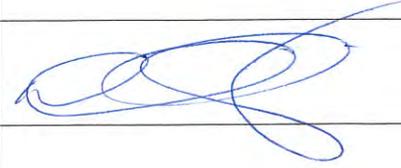
No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
104	ADAM COLEMAN	439 SW PEMBERLY LOOP McMINNVILLE OR 97128	Adam C
105	Zachary Zook	14045 SE Burns Rd. McMinnville, OR 97128	Zachary Zook
106	Gordon Reed	118 Enos ST Amity, OR 97101	Gordon Reed
107	Coyler	4813 Fehrer Ct NE Salem OR 97305	Coyler
108	DAVID HART	P.O. BOX 287 WILLAMINA, OR	David Hart
109	Sharon Trieth	8370 Grand Ronde Rd Grand Ronde Ore	Sharon Trieth
110	KJ Konink	46695 Hwy 22 HEBO OR 9722	KJ Konink
111	Rick Williams	P.O. Box 476 Willamina, OR 97396	Rick Williams
112	Ken Beard	460 NE Clark Ct. McMinnville, OR 97128	KEN BEARD
113	STEVE ZIKA	7145 SW SHALOW LN PORTLAND, OR 97225	Steve Zika
114	Bradley Rowell	325 SE LaCreole dr. DALLAS, OR 97338 #103	Bradley Rowell
115	JUAN GARCIA	4637 DUCHESS CT SALEM - OR 97301	Juan Garcia
116	Tim Buckner	709 Ash St Dayton OR, 97114 PO Box 805	Tim Buckner
117	KATHERINE CLABB	281 SE BARBER AVE PO # 1190 WILLAMINA OR 97396	Katherine Clabb
118	Brandon Terrill	5801 Salmon River Hwy OTIS, OR 97568	Brandon Terrill
119	Kristin Rasmussen	8805 N Hartman Portland, OR 97203	Kristin Rasmussen

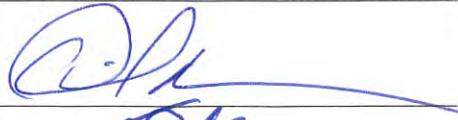
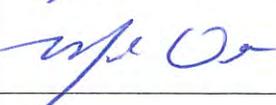
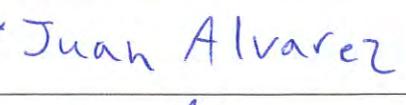
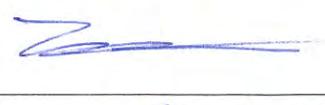
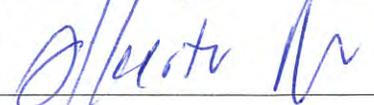
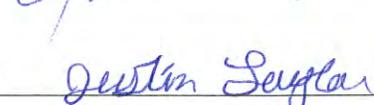
No Connect VI Funds for Log Exports at Port of Newport

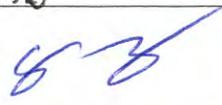
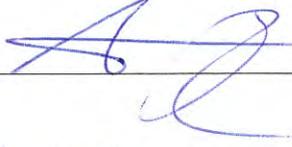
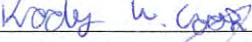
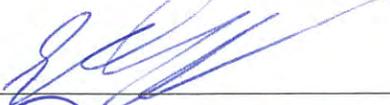
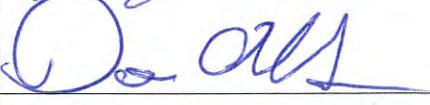
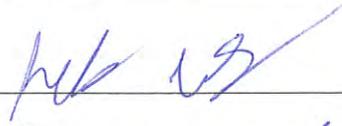
	Name	Address	Signature
40	Joshua Jones	1960 S. Church St. Dallas, Oregon 97338	
41	Kellen Channing	2600 Tallard Rd. Willamina, OR 97356 414 NW Evans St	
42	Trenton Klopfenstein	Sheridan, OR 97328	
43	Elwood Ohlson	705 NE E St Willamina, OR 97396	
44	Tony Zelich	143 SW Harrison St Sheridan, OR, 97378	
45	Rick Phelps	Pillomath OR 97370	
46	DAN MCKAIG	WILLAMINA, OR. 97396	
47	Casey Jones	McMinnville OR 97128	
48	Greg Foot	154 SE Baker St Willamina OR 97396	
49	Brian Perrone	246 NE Center Sheridan	
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No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
24	Ricardo Arceaga		
25	Rafael Orozco		
26	Armando Alvarez		
27	Jim Comunford	90 Box 483 Willamina	
28	DENNIS OWENS	378 SW ORCA willamina 97396	
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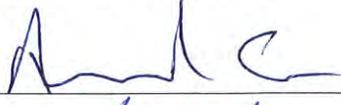
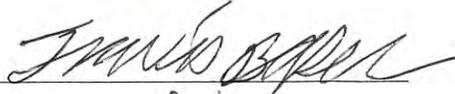
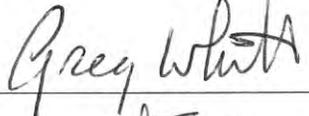
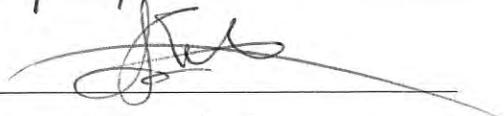
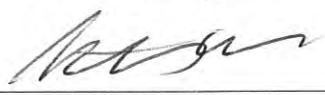
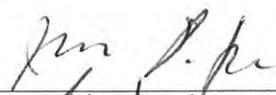
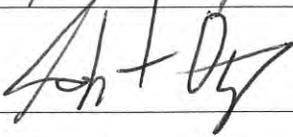
No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
120	DAVID H. SALMON	PORTLAND 97229 770 NW 112 <sup>TH</sup> AVE	
121	BRETT GRIFFIN	1661 NW MEDINA H MCMINNVILLE 97128	
122	Kevin Osborn	PO Box 793 Amity OR 97101	
123	Tyler Maples	1450 SW Bridalwood Dr. Unit 19 Dallas OR 97338	
124	Mark Olsen	5987 Compton Lane SE Salem OR 97306	
125	STEVEN FISHER	3702 SE SKYHAWK LN	
126	Ashlyn Strouse	445 NW Gutbrod Kol Sheridan, OR 97378	
127	Tristan Thomas	936 ne hembree st mcminnville OR	
128	Tyler Juhnke	804 maddox Ave Amity OR	
129	Juan Alvarez	236 Independence way Independence, OR	
130	GREG STAW	1211 SW Hayter Dallas OR	
131	Jacob Sutton	410 Wabell Ave Amity OR 97101	
132	Eric Guardiola	320 NE 2 <sup>ND</sup> Street William, Or 97396	
133	Alberto Rincon	905 Fuller Ln SE Salem OR 97306	
134	Justin Taylor	438 NE Balm St Sheridan OR 97378	
135			

	Name	Address	Signature
136	Derek Gerwig	975 SW Pioneer Dr. Willamina OR 97396	971-237-3555
137	Joe Sabey	1203 SW Pioneer Dr. Willamina OR	
138	KRAIG KIRCHER	291 NW CARD AVE DALLAS OR 97338	
139	BJ Wilson	5430 mill creek rd sheridan, OR	
140	Salvador Nojar	280 I Street Independence OR	
141	Adam Tickets	20001 KOSM <sup>50th</sup> sheridan, OR 97378	
142	Tom Cost	P.O. Box 271 Grant Ronde OR 97347	
143	Levi Fendall	203 NE balm St sheridan OR 97378	
144	Daniel Oldham	Po Box 215 Carlton, OR 97111	
145	Jeff Wallis	1748 SE Godsey	
146	Joe Crawford	2800 Salmon River Hwy	
147	David Keyser	39280 SW Oak Lane Willamina, OR. 97396	
148	Adam Mendenhall	13110 Beck RD DALLAS, OR 97338	
149	Tom Collins	336 SW Cherry St DALLAS, OR 97338	
150			
151			

No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
72	Ed Jack	Monmouth 157 Gentle Av	Ed Jack
73	Dennis Osborn	322 SW Sheridan Cornwall OR	Dennis Osborn
74	Jeff Moore	1316 NW Cedar ST McMinnville OR 97128	J Moore
75	JACK HAGGITT Jack Haggitt	1765 SW TAMARACK ST Apt. 9 McMinnville, OR 97128	Jack Haggitt
76	James Martin	310 SE JEFFERSON ST. Sheridan OR 97378	James Martin
77	David Reader	1246 36 <sup>th</sup> Ave Forest Grove OR	David Reader
78	Mike Skjei	142 SW HARRISON'S Sheridan OR	Mike Skjei
79	Rick Shaser	P.O. Box 44 Grove Road OR	Rick S
80	Karen L. Hill	6820 Ridgeway Rd Sheridan, OR 97378	Karen L. Hill
81	JEFF BLACKWELL	435 SW Hill Dr Willamina OR 97146	Jeff Blackwell
82	Josh K. Holland	1448 Pinner Dr Willamina OR 97146	Josh Holland
83	Bustavo Garcia	Salem OR 97317	Bustavo Garcia
84	Jamison Seaman	413 SW Corn Wall St Sheridan, OR, 97378	Jamison Seaman
85	Amanda Horton	Amity, OR	Amanda Horton
86	JAMES KNIGHT	Macmill	JAMES KNIGHT
87			

	Name	Address	Signature
56	<del>Chad Chitwood</del> Chad Chitwood	Sheridan OR 9220 Lincoln St 97378	
57		PO Box 662 Grand Ronde	
58	Armando Garcia		
59	Jim Stafford	4660 SW 202 <sup>nd</sup> Ave Astoria OR 97103	
60	Travis Baker	246 SW Sheridan OR	
61	Greg Whitt	453 SW IDA St McMinnville	
62	<del>Allego Jiminez</del>	4606 37TH Av. 97305 Salem	
63	Ken Hortsen	2321 SW DABDAM ST MAC	
64	Dick Skellenburger	337 NE Blair St. Sheridan OR 97378	
65	Steven Hale	1112 SE Monmouth Cutoff Rd Dallas OR	
66	Jesse R. Zook	847 S. Bridge Sheridan, OR	
67	Robert Owings	3291 DE Falconer St Sheridan OR 97378	
68	David Sinder	116 Gettell Ct Astoria OR 97101	
69	Gerald Saben		
70	Lucas Headings	PO BOX 341 Amity OR, 97101	
71			

No Connect VI Funds for Log Exports at Port of Newport

To: The Oregon Transportation Commission and  
Governor Kate Brown:

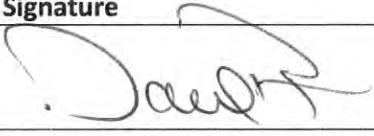
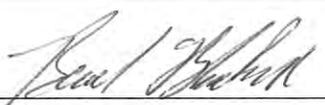
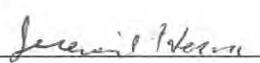
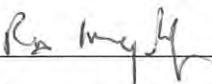
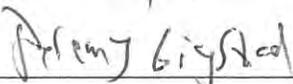
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	Name	Address	Signature
1	Dave Butler	Amity OR	
2	Brad Blackwell	463 N.W Evans St Sheridan OR	
3	Jeremiah Howard	778 NE Phoebe St Sheridan OR	
4	Ray Meyerhofer	P.O. Box 252	
5	Jeremy Gysted	151 First St Fall City	
6	Brad Reid	PO Box 714 Willamina OR 97306	
7	Michael Perry	13940 S.W. EAST CR Rd Willamina OR 97306	

**No Connect VI Funds for Log Exports at Port of Newport**

	Name	Address	Signature
24	Steven M. Deabe	P.O. Box 728 Amity OR 97101	Steven Deabe
25	Blas Grimaldo	210 SW Westval St Memphisville	Blas Grimaldo
26	Luis PARRIS	GRANDROVE P.O. Box 52	Luis PARRIS
27	Marvin Dale Lux Mum Dal Zj	P.O. Box 596 Williamina 97396	Marvin Dale Lux
28	Sharon Sabey Sh Sabey	Box 253 Williamina OR 97396	Sh Sabey
29	Jose Camacho	680 Greencrest St NE Salem OR 97301	Jose Camacho
30	Matt VanWinkle	820 Ramsey Ct NW Salem OR 97304	Matt VanWinkle
31	Cameron Sabey	691 NE 5th St. Williamina, OR 97396	Cameron Sabey
32	Alex Melgaa	611 W 13 Memphisville OR	Alex Melgaa
33	Ron Dentbo	1617 SW FIELDS ST APT H MEMPHISVILLE, OR	Ron Dentbo
34	Rich <del>W. Dentbo</del>	395 SW Fir St Williamina OR 97396	Rich <del>W. Dentbo</del>
35	Nick Johnson	94767 Lovelace Ave Junction City OR	Nick Johnson
36	David Hampton	21255 Eda Dr. NW Salem, Or. 97304	David Hampton
37			
38			
39			

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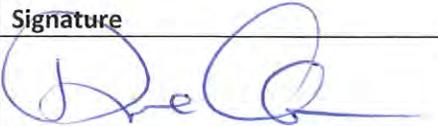
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	Name	Address	Signature
1	DAVID ALBRECHT	4231 NE 63RD AVE PORTLAND, OR 97218	
2	MIKE MANNEX	4985 SW JOSHUA CT. TUALATIN, OR 97062	
3	Daniel Weaver	3980 NW 192 <sup>nd</sup> Ave Portland, OR 97229	
4	Kaitlin Bartel	Portland, OR 97229	
5	Joanie Spencer	Lake Oswego, OR	
6	DAVE STINSON	Gresham, OR 97030	
7	CARLIN IRISH	BEAVERTON, OR	

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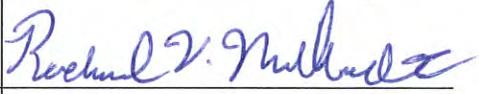
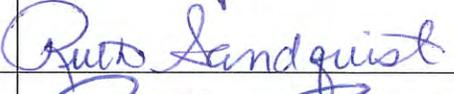
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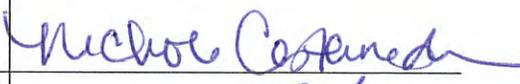
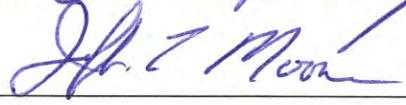
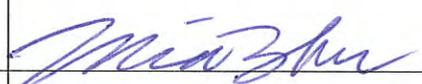
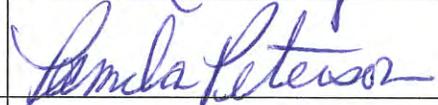
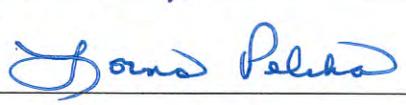
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	Name	Address	Signature
1	VICTORIA SHAYLOR	97224	Vj Shaylor
2	Tricia Niewie	97213	<del>Tricia Niewie</del>
3	JACOB DOMINGUEZ	97239	Jacob Dominguez
4	Nikki Commerford	97223	Nikki Commerford
5	Lynne Irwin	97236	Lynne Irwin
6	Karleigh Prestianni	97003	Karleigh Prestianni
7	RABEL ROBERTS	97239	Rabel Roberts

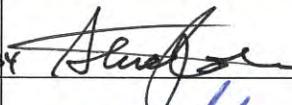
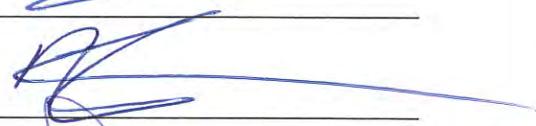
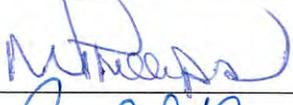
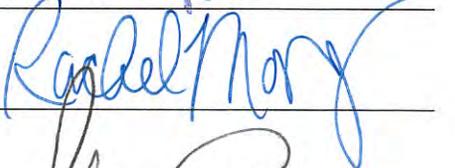
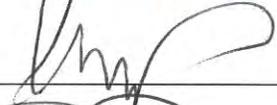
No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
8	Lee Eastman	97124	
9	Jason Isaac	97132	
10	JASON LOWE	97068	
11	Richard Milbradt	97023	
12	Debbie Schwartzkopf	97008	
13	George Bonner	97071	
14	Emily Andrews	97110	
15	Ruth Sandquist	97124	
16	Steve Samples	97378	
17	Yvonne Peinstow	97035	
18	Nahemi Neri	97124	
19	Brian Vetrone	97202	
20	James McDade	97229	
21	John Detkiefs	97080	
22	Steve Semmla	97202	
23	MICHELLE STUMPF	97206	

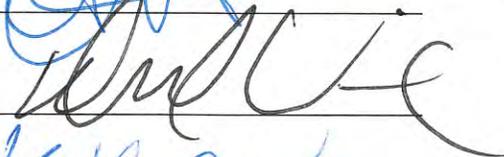
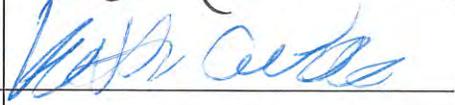
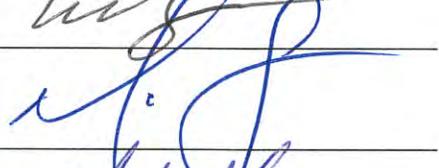
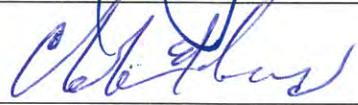
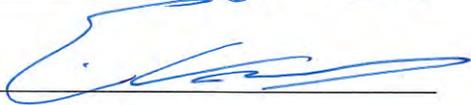
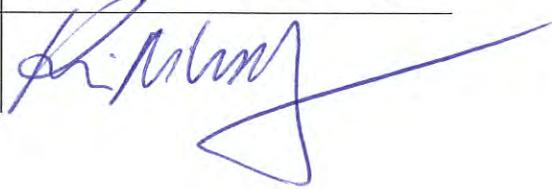
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	Name	Address	Signature
24	Kit Labelle	97229	
25	Nichole Costanzo	97225	
26	Scott Gascho	97013	
27	Thim Holt	97007	
28	Ryan Sweeney	97119	
29	Blake Phillip	Ukmerica 97005	
30	JEFF Moore	97007	
31	Min Zhu	97123	
32	MIKE NUNENKAMP	97070	
33	Chris Watson	97007	
34	Meredith Whitney	97008	
35	Pamela Peterson	97056	
36	Lorna Petcka	97229	
37	Katie LaValley	97035	
38	Bob Bluhm	97062	
39			

No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
8	James Nare	Lake Oswego 97035	
9	Carter Stinton	20280 SW Seiffert Rd Sherwood OR 97140	
10	Mike Pannin	3009 Vinter Dr Fruit Grove, OR 97116	
11	Steve Cole	1701 High Land Dr. LAKE OSWEGO, OR 97034	
12	Jay Hart	PDX 97225	
13	MARCUS MUELLER	11858 NW VALLEY VISTAS HILLSBORO, OR 97124	
14	SEFF MONTGOMERY	8967 SW WOODS DR BEAVERTON, OR 97007	
15	COLLIN THOMASON	PORTLAND 97201	
16	David Zaccaron	Oregon City 97041	
17	MARK PURTEN	1530 SW PDX MITCHELL LANE	
18	Mike Phillips	Newberg, OR	
19	Rachel Moshofsky	4721 NW Spahr Terr. Portland, OR 97210	
20	Genon Heskamp	20741 Morgan 97006	
21	Jason Jacobson	16625 SW Sidney Sherwood 97140	
22	Dallas Reid	97005	
23	Lory Calvin	97220	

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	Name	Address	Signature
24	Angela Ackerman	11432 SW Capitol PHLD, 97202 <sup>HWY</sup>	
25	DAVID R. VISSÉ	10045 SW ALSEA CT TUALATON OR 97062	
26	Kathlin Crider	97702	
27	JL Cartman	97062	
28	Dillon Van Dusen	97217	
29	Neat Beymer	3624 SE Rex 97202	
30	MICHAEL SWIGER	13888 SW ESSEX DRIVE TIGARD OR 97223	
31	CHARLES LAMPING	7255 CROSS CREEK RD DUNDEE, OR	
32	Patrice Somerville	97225	Patrice Somerville
33	Patrick Griess	97003	Patrick J Griess
34	Erik Vorwerks	97223	
35	Brad Calahan	97225	
36	Stephen Murphy	97225	
37	Brandon Gibbs	97225	
38	Dawn Drummond	97225	Dawn D Drummond
39	Kim Grey	97223	

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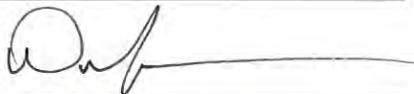
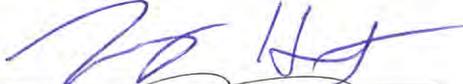
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1	DEREK SAMPSON	97280	
2	Jerome Houston	97006	
3	Michael Zumwalt	97123	
4	LETTA BURNOVAS	97229	
5			
6			
7			

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40	Nancy Ketter	97124	Nancy Ketter
41	Adam Payne	97217	Adam Payne
42	Judy Sunkle	97070	Judy Sunkle
43	Luke Watson	97123	Luke Watson
44	Darrel Jones	97303	Darrel Jones
45	Laura Opel	97003	Laura Opel
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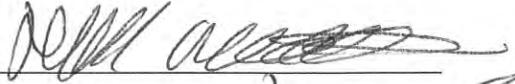
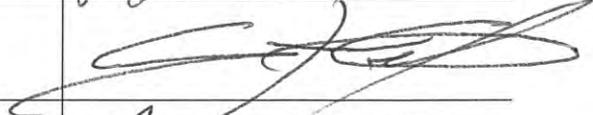
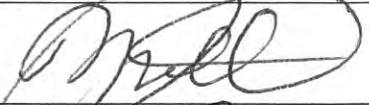
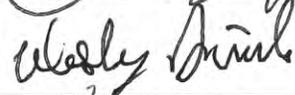
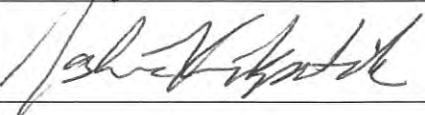
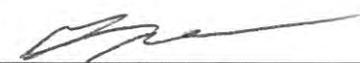
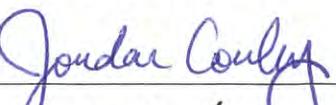
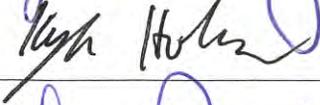
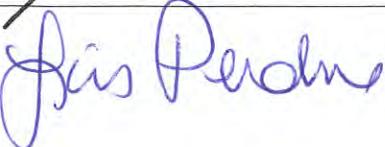
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1	Amber Hakvill	25 SW Alder Ct	Amber Hakvill
2	Nic Andersen	<del>7426</del> warrenton	Nic Andersen
3	Erick Gres	6 Halsey Rd Apt #2	Erick Gres
4	Matt Harrington	168 SE 5 <sup>th</sup> Apt 8	Matt Harrington
5	Hank Ausler	3992 SUNSET BLVD	Hank Ausler
6	Manny Nelson	2634 Grand St	Manny Nelson
7	Jim Bennett	33931 Hwy 26 Seaside OR 97138	Jim Bennett

No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
56	Derek Moorcroft	Warrenton Hampton	
57	ERICKSON P. Goff	Warrenton Hampton	
58	Justin Gauthier	Warrenton Hampton	
59	Justin Fickett	Warrenton Hampton	
60	Steven Betz	War. Hampton	
61	Wesly Jiroch	Warrenton Hampton	
62	JOSHUA KIRKPATRICK	WARRENTON	
63	Juan Melendez	Warrenton	
64	GRACIE HATHAWAY	WARRENTON	
65	Paul Gill	Elsie, Oregon	
66	Matt Carter	Warrenton	
67	Kevin Moore	Warrenton	
68	Adam Hardester	Warrenton	
69	Jordan Couly	astoria, oregon	
70	Kyle Holmes	warrenton, OR	
71	Lois Perdrie	Seaside OR	

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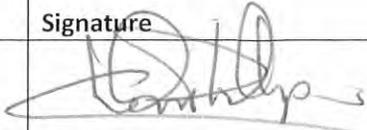
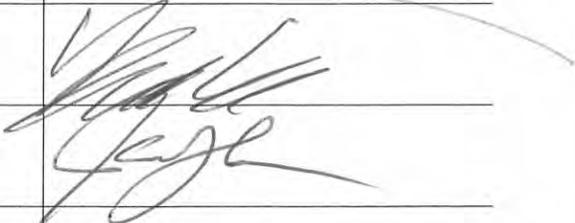
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	Name	Address	Signature
1	DAVID KUWERT	TILLAMOOK	
2	MARK ELSTON	TILLAMOOK	
3			
4			
5			
6			
7			

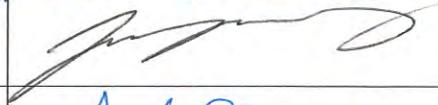
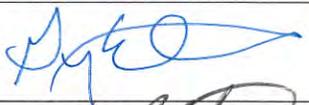
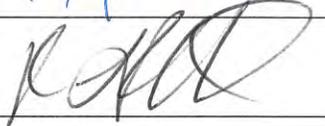
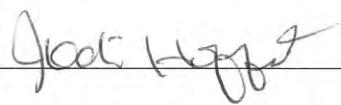
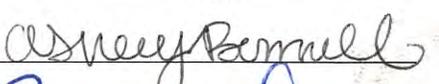
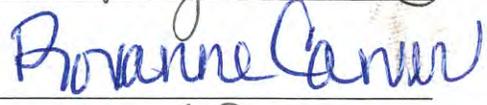
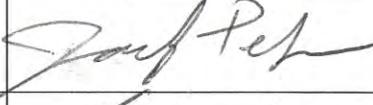
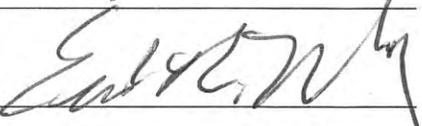
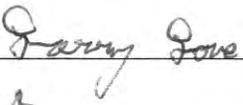
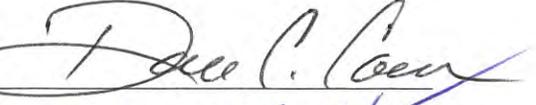
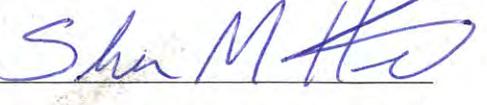
**No Connect VI Funds for Log Exports at Port of Newport**

	Name	Address	Signature
40	Charles Phillips	1402 OSTER RD GEARHART OR 97138	
41	Brad Culver	Warrenton	
42	Tom Mendoza	Warrenton	
43	Antonio Alcantar	Astoria	AAhnter
44	Jason Edwards	35 nw Birch Ct warrenton, OR 97141	
45	Peter Alexander	1328 S MAIN AVE Warrenton OR	
46	Kien Ross	Astoria	
47	David Stull	<del>XXXXXXXXXX</del> Astoria	
48	Eric Oxford	Warrenton	Eric Oxford
49	Marc A Pedraza	Warrenton	Marc A Pedraza
50	Jacob Gauthier	Astoria	
51	Mark Brewer	Astoria	
52	Tommy Myers	Warrenton	Tommy Myers
53	MIKE CAMPBELL	679 CLATSOP AVE ASTORIA	Michael Campbell
54	Bill Nichols P.E.	285 W. LEXINGTON ASTORIA, OR 97103	
55			

No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
24	Paul H Walworth	Pacific City	Paul H Walworth
25	<del>Darren Pappas</del>	Pleasant Hill	Darren Pappas
26	Julio Bernal	4390 Hwy 101 S. 1920 ABBY LANE TILLAMOOK OR 97141	Julio A Bernal
27	MIKE MURPHY SR		Mike Murphy
28	CON TROTTER	P.O. Box 5 NORSTAR	Con Trotter
29	DAVID CROWNOVER	2735 Seaside Hedge	David Crowner
30	Evonne Pacinda	3315 SE 115th Ave. Portland, OR 97266	Evonne Pacinda
31	Rhyon Stresing	Tillamook	Rhyon Stresing
32	Doug Steinbach	Bay City	Doug Steinbach
33	Victor Lopez	Tillamook	Victor Lopez
34	Michael S Murray	5519 3rd St. #3 Tillamook, OR. 1113 6th St	Michael S Murray
35	Mathew Taylor	Tillamook OR 6480 UNION ST	Mathew Taylor
36	Mark Dunham	Bay City OR	Mark Dunham
37	LARRY RIVERS	2210 nielsen Rd Tillamook OR.	Larry Rivers
38	Mark Valdez	813 4th St Tillamook OR	Mark Valdez
39	Kris Wiedmaier	2505 7th St Tillamook OR 97141	Kris Wiedmaier

No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
8	Timothy R Dunn	8790 Mill Creek Rd Tillamook OR	
9	Robert Green	3606 Hawthorne Ln. Tillamook OR 97141	
10	Don Smith	Tillamook, OR 97141 309 Williams Ave	
11	Jeremy McCarty	610 Evergreen Dr. Tillamook, OR 97141	
12	Gary Ellison	1514 4th ST TILLAMOOK, OR 97141	
13	Ryan Pettit	244 10th St Tillamook OR 97141	
14	Joel Hoffer	8355 Magnolia Dr Tillamook, OR 97141	
15	Asneey Bonnen	2070 BERRY RD Tillamook OR	
16	Roxanne Cannon	Tillamook	
17	JACOB DELZER	4990 ENTERPRISE RD. DALLAS OR	
18	ERIC R. NORBERG	Lincoln, CITY ORE 1775 NE 10th St.	
19	Larry Love	16590 Farmer Cr., Rd Clarendale, Ore	
20	Buck Shoach	7995 Vaughn RD Tillamook OR	
21	Dale Cooper	1615 BOLT CIRCLE TILLAMOOK OREGON 97141	
22	Shaun Hilton	716 Madrona Ave Tillamook OR 97141	
23	Cene Collier	524 cedar Tillamook ore 97141	

No Connect VI Funds for Log Exports at Port of Newport

To: The Oregon Transportation Commission and  
Governor Kate Brown:

We the undersigned urge you to deny the Port of Newport's request for tax-payer dollars to develop capacity to export raw logs through its international shipping terminal. For the past several years the Port of Newport has viewed log exports as the "bread and butter" of their export terminal business plan. It is clear from the Port's application for ConnectOregon VI funding that log export interests are the only clear and committed beneficiaries of the Port's project.

Shipment of raw logs to Asia limits the ability of Oregon mills to access these resources and create jobs and value-added goods here at home and the government should not subsidize this activity. Development of the log export yard at the Port of Newport would negatively affect our three sawmills in Oregon and the more than 600 direct employees who live and work in these communities. A recent report by the Oregon Department of Forestry noted that wood products manufacturing creates three jobs per million board feet of timber, whereas logs that go overseas create fewer than one port job per million board feet.

We live in a global economy and private land owners should be able to seek markets overseas but we would hope that our state managers would do no further harm to the local sawmill industry by subsidizing the export of our raw materials and the jobs and value added products that go along with them.

**Please say no to Connect VI funding for the Port of Newport's International Shipping Terminal. Don't subsidize a project that ships our jobs overseas.**

There are numerous transportation needs throughout the state. We urge you to lend your support to projects that don't have such a direct negative impact on other parts of Oregon's economy.

	Name	Address	Signature
1	PAULA HUSTON	3280 Fitzpatrick Rd Tillamook, OR 97141	Paula Huston
2	Marty Brint	PO Box 407 HEBO OR	Marty Brint
3	Rick Balaban	7855 Docs Ln NW Yreka, CA 96095	Rick Balaban
4	John M. Farrier	6400 Cedar Springs Place Tillamook OR	John M. Farrier
5	SCOTT FACKREHL	9685 SOUTH PRAIRIE RD TILLAMOOK, OR 97141	Scott Fackrehl
6	Joshua Huffman	6440 Colonial Way Tillamook, OR 97141	John M. Farrier
7	John Satter	24775 Wilson River Hwy Tillamook OR 97141	John Satter

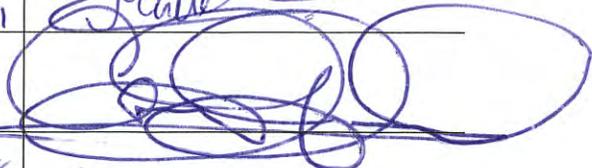
No Connect VI Funds for Log Exports at Port of Newport

	Name	Address	Signature
40	Jonathan Cordova	3815 3rd St #12 97141 Tillamook OR	Jonathan Cordova
41	Tracy Oviatt	16305 HWY 101 S. Tillamook, Or. 97141	Tracy Oviatt
42	Chris Walling	1506 4th St Tillamook OR 97141	Chris Walling
43	RAYMONA STEGMAN	3412 DOMINGO CT. #4 TILLAMOOK OR 97141	Raymond Stegman
44	Jacob Fowler	1205 Miller Ave Tillamook, OR, 97141	Jacob Fowler
45	BRIAN ROSE	906 NE SULLY AVE UNIT B Tillamook, OR 97141	Brian Rose
46	Samuel Walker	17105 Trask River Rd. Tillamook, OR 97141	Samuel Walker
47	Luther Rhoads	3803 Maple Ln. Tillamook OR 97141	Luther Rhoads
48	RODOLFO IZQUIERDO	4416 GLENVIEW AVE. TILLAMOOK, OR. 97141	Rodolfo Izquierdo G.
49	Sean Richards	3425 Meccy St. Bay City OR, 97107	Sean Richards
50	Kevin Szabados	10525 Hwy 101 South Tillamook OR 97141	Kevin Szabados
51	Rodney Eames	504 EVERGREEN AVE CARIBELLE OR 97118	Rodney Eames
52	YOLANDA EAMES	504 EVERGREEN 1103 5th St APT 5	Yolanda Eames
53	Andrew O'Connor	Tillamook Oregon	Andrew O'Connor
54	Scottie J. Blount	1116 Meadow St. Tillamook Oregon 97141	Scottie J. Blount
55	Travis Stoeffler	210 Nelsen Rd Tillamook OR 97141	Travis Stoeffler

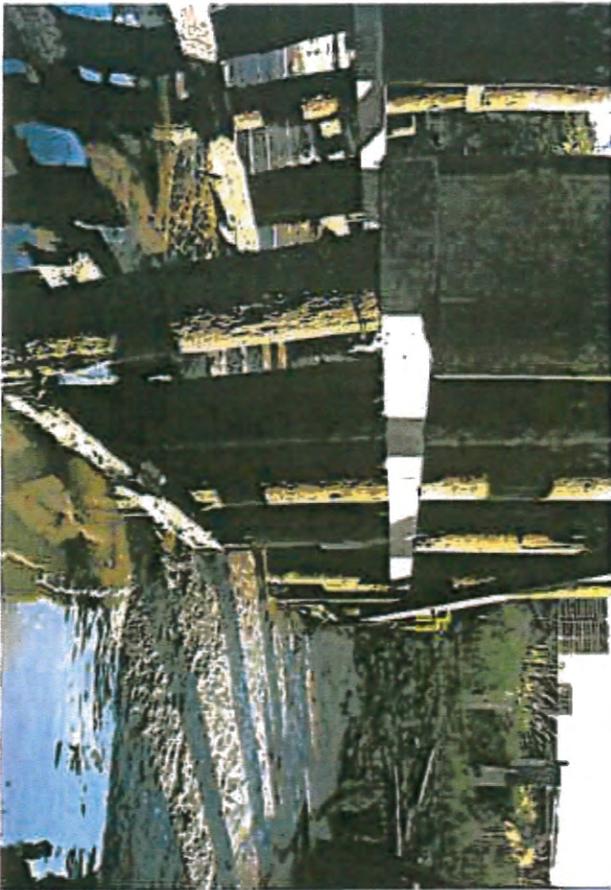
No Connect VI Funds for Log Exports at Port of Newport

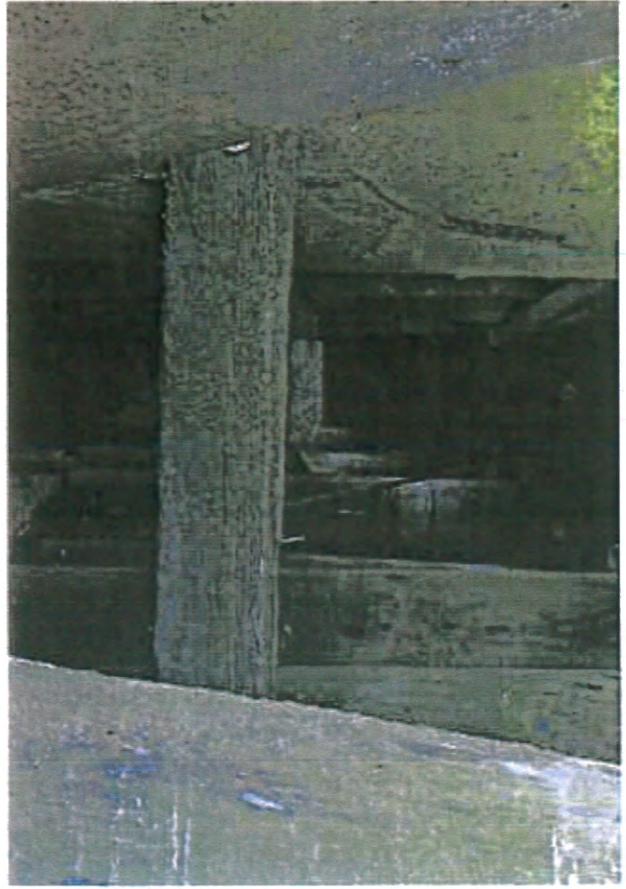
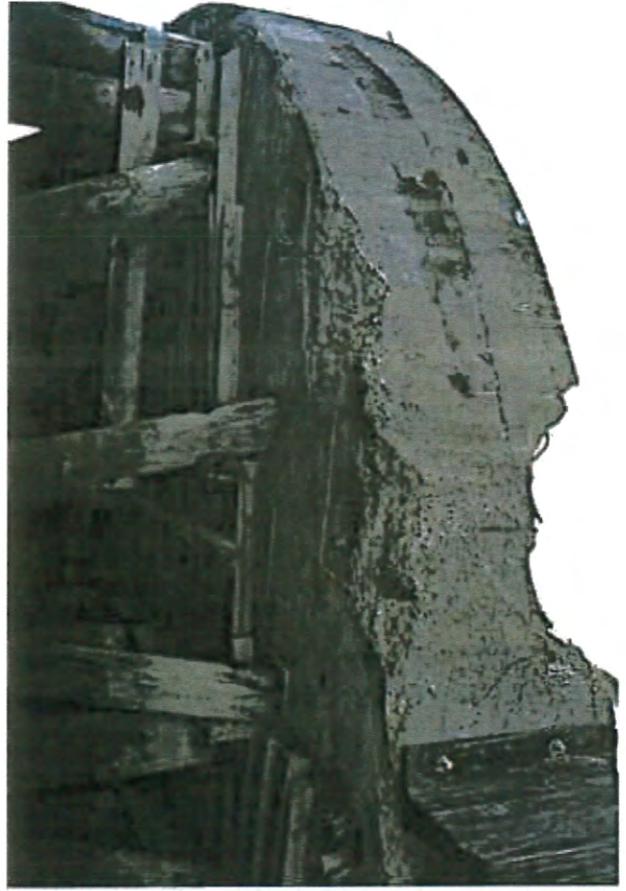
	Name	Address	Signature
56	Brad Rudd	2935 Old Latime Rd #7 Tillamook Oregon	Brad Rudd
57	Jose Vega	9550 5th St. Bay City OR 97107	Jose Vega
58	Jeff Mendenhall	1024 NE 13th Ave Rockaway Beach, OR	J Mendenhall
59	<del>Harold</del>	1015 Tule Rd Tillamook OR 97141	Harold
60	Todd L. Weaver	11880 Hwy 101 South Tillamook OR 97141	Todd Weaver
61	Dan Stephenson	7390 2nd Street Bay City	Dan Stephenson
62	Jose Drocco	2290 NIELSEN RD. TILLAMOOK OR 97141	Jose Drocco
63	Kurt Eckerson	5205 Kenway DR Tillamook, OR 97141	Kurt Eckerson
64	DeWayne Harkless	1104 Stillwell Ave Tillamook, OR, 97141	DeWayne Harkless
65	Caly woods	113 Grove St Tillamook OR 97141	Caly Woods
66	Derek Stone	800 Evergreen DR Tillamook, OR 97141	Derek Stone
67	Agostin Garcia	7455 Trask River RD	Agustín García
68	Pedro mendez garcia	903 Pacific Ave Tillamook, OR 97141	Pedro Mendez
69	HENRY M Owen	1508 12th St Tillamook, OR	Henry M Owen
70	Tanner, Barichio	15760 Wilson River Hwy Tillamook OR	Tanner Barichio
71	Luis Benito	2209 9th Tillamook, OR	Luis Benito

No Connect VI Funds for Log Exports at Port of Newport

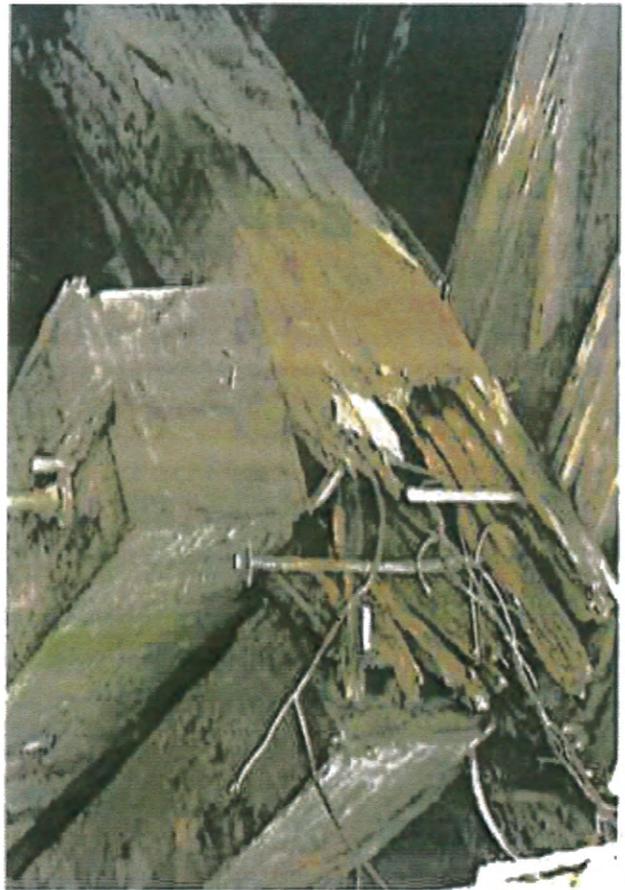
	Name	Address	Signature
72	Jeff Meyers	2310 6th Street Tillamook OR 97141	
73	Mauricio Magana	5311 3rd St Tillamook OR 97141	
74	Craig Jackson	5509 3rd St Tillamook OR	
75	Nadin Walker	33258 97108 EAST OAK RD BENTON	
76	Samantha Houston	3280 Fitzpatrick Rd Tillamook OR 97141	
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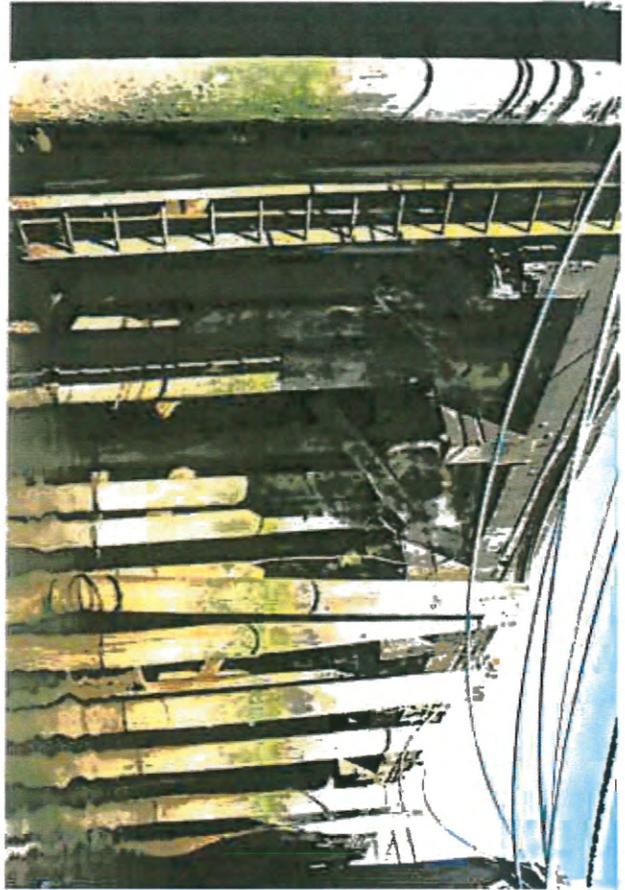
No Connect VI Funds for Log Exports at Port of Newport













## HAMPTON LUMBER MILLS, INC.

9600 SW Barnes Road  
Suite 200  
Portland, Oregon 97225-6666  
Telephone 503.297-7691  
Fax 503.203-6618  
[www.HamptonAffiliates.com](http://www.HamptonAffiliates.com)

February 18, 2016

### COMMENT ON CONNECTOREGON VI APPLIATION: INTERNATIONAL TERMINAL SHIPPING FACILITY - PORT OF NEWPORT

I want to thank the committee for the opportunity to comment today. My name is Kristin Rasmussen and I am the Public Affairs Manager for Hampton Affiliates, a family-owned, vertically integrated lumber company headquartered in Portland, OR. Hampton operates eight sawmills in the Pacific Northwest, including three in Oregon located in the communities of Warrenton, Tillamook, and Willamina. Joining me is my colleague, Doug Cooper, Vice President of Resources for Hampton.

We are here today to express concern with the Port of Newport's request for ConnectOregon VI funds to develop capacity to export raw logs through its international shipping terminal. For the past several years the Port of Newport has viewed log exports as the "bread and butter" of their export terminal business plan. It is clear from the Port's application for ConnectOregon funding that log export interests are the only clear and committed beneficiaries of the Port's project.

Development of the log export yard at the Port of Newport would negatively affect our three sawmills in Oregon and the more than 600 direct employees who live and work in these communities. I would like to submit for your consideration, a petition signed by more than 350 of our employees in Oregon, opposing use of public ConnectOregon funds to develop log export capacity at the Port of Newport.

Shipment of raw logs to Asia, subsidized by Asian interests, limits the ability of Oregon mills to competitively access these resources and create jobs and value-added goods here at home. A recent report by the Oregon Department of Forestry noted that wood products manufacturing creates three jobs per million board feet of timber, whereas logs that go overseas create fewer than one port job per million board feet. Rather than creating new jobs, by subsidizing development of log exports out of Newport, the state would be reducing employment opportunities in domestic wood manufacturing in neighboring communities.

We live in a global economy and private land owners should be able to seek markets overseas but we would hope that the State of Oregon would do no further harm to the local sawmill industry by subsidizing the export of our raw materials and the jobs and value-added products that go along with them.

As you review the projects presented here today, we urge you to consider the broader implications of what is proposed and carefully weigh potential for negative impacts to other parts of Oregon's economy.



EXECUTIVE OFFICE

## HAMPTON LUMBER MILLS, INC.

9600 SW Barnes Road  
Suite 200  
Portland, Oregon 97225-6666  
Telephone 503.297-7691  
Fax 503.203-6618  
[www.HamptonAffiliates.com](http://www.HamptonAffiliates.com)

Thank you again for your attention to this matter and your continued commitment to enhancing Oregon's economy and infrastructure.

Respectfully submitted,

A handwritten signature in black ink, reading "Kristin Rasmussen". The signature is written in a cursive, flowing style.

Kristin Rasmussen  
Hampton Affiliates  
9600 SW Barnes Road, Suite 200  
Portland, OR 97225

503-203-6563  
[kristinrasmussen@hampmtonaffiliates.com](mailto:kristinrasmussen@hampmtonaffiliates.com)

## **Comments Received**

**Project # 2M0366**

**Teevin Bros. Land & Timber Co.**

**Mooring Points RM 66.5**

-----Original Message-----

From: Glenn Roger Dorband [<mailto:info@ravenstudiosart.com>]

Sent: Wednesday, June 29, 2016 8:30 AM

To: WRIGHT Marie A

Subject: Teevin Bros. Land and Timber Company grant application

Dear Ms. Wright,

Yesterday the North Coast Forest Coalition put out a bulletin with the news that Teevin Brothers Land and Timber Company has applied for one of the ConnectOregon grants. This is of particular concern to me in light of the ongoing struggle in Clatsop County over the future of our environment. In the past year there has been an ever increasing call for more timber harvest by local timber companies. Local taxing districts have joined the call in hopes of a way out of their budget woes. This is contrary to the will of the people as expressed in the Clatsop Vision 2030 Together plan that was initiated by the county commissioners and approved in its final form in January of 2015.

The vision plan clearly expressed the majority opinion in the county to "protect and enhance our scenic beauty and natural resources... and preserve our forests". Nowhere in the plans 16 point Economic Goals is timber harvest mentioned, let alone increased timber harvest which would without doubt encroach on areas of the county forests already designated for conservation.

A grant to the Teevin Brothers company to improve their log export facility would not be in keeping with the will of the people of the county. The Teevin Brothers request is a call for increased logging. It would perpetuate the fate of our county as a timber "colony" for Asia. In the long run it will hurt our growing recreational and tourism industry by further degrading the environment and it will result in more local milling jobs being exported to China.

Your prospectus for grant qualification states that the environment will benefit from granted funds. A Teevin Brothers grant will degrade the environment with further industrialization of the Columbia River and its Clatsop watershed. Increased export translates to increased clearcutting leading to further erosion, reduced water quality, and increased release of CO2 at this crucial time when global warming is rapidly advancing.

Please deny this grant which is not in the spirit of ConnectOregon.  
Thank you.

Roger Dorband

Roger Dorband / Raven Studios  
462 6th Street  
Astoria, Oregon 97103  
503 936-2715

**From:** Darvel T Lloyd [<mailto:darvlloyd@gmail.com>]  
**Sent:** Wednesday, June 29, 2016 8:41 AM  
**To:** KAUTZ Sharon L  
**Subject:** Grant request to expand private log-exporting terminal

Good morning,

I heard that Teevin Bros. Land and Timber Company is asking *ConnectOregon* to contribute \$750,000 to expanding its log export facility in Knappa, Clatsop County.

I agree completely with the North Coast State Forest Coalition: "Exporting jobs to Asia and facilitating the rapid destruction of our forest landscape should *not* qualify for a *ConnectOregon* grant." Oregon taxpayers should NOT be subsidizing private timber companies in their quest for higher profits at the expense of our precious ecosystems, which have turned into vast acreages of sterile, highly erodible monoculture.

Thank you for your consideration.

Sincerely,

Darvel Lloyd  
54 SE 74th Ave.  
Portland, OR 97215  
503-593-2996

## **Comments Received**

**Project # 2R0420**

**Knife River Corporation Northwest**

**Knife River Rock Train System Improvements**

March 3, 2016

Scott Turnoy  
Oregon Department of Transportation  
555 13<sup>th</sup> Street NE  
Salem, OR 97301

SUBJECT: ConnectOregon VI Application – 2R0420 Knife River Rock Train System Improvements

Mr. Turnoy,

Regarding the above referenced application, we respectfully submit this letter for the public record to further supplement our company's application materials. Within this letter we will address a few items within our application that we believe could have been scored higher by Oregon Department of Transportation and Oregon Office of Business Development staff. As a first time applicant, we adversely affected our application scoring due to our own ignorance of the States scoring methodology. Had we answered questions with a greater understanding of the scoring we believe that our application could have been scored at 60 points rather than 53 points.

Regarding scoring consideration b, whether our project results in an economic benefit to the state, we received a score of 10 out of 20 from the review committee, but believe the proper score for our application is 16. In our answer to question 38 we failed to select any of the statewide business clusters, but should have selected a majority of them, if not all of them. The following statement was provided in our application: *"The project reduces costs to multiple private industries and public clients who purchase ready mix concrete and asphalt concrete pavement materials."* In recent years, construction aggregates transported by the Knife River rock train were used on projects for Nike (Athletic & Outdoor Gear and Apparel Cluster), Intel (Semiconductor and Electronics Components Cluster), nursery and hay storage customers (Agriculture Cluster), and multiple airport projects (Aviation Cluster). These are just a few examples of how any improvement in this operation will benefit many of Oregon's Statewide Business Clusters in the future as we continue to serve a broad range of customers within our geographic market. With this additional information, we ask that the review committees be aware that our project should have received 6 additional points based on our answer to question 38 had we answered the question accurately.

Regarding scoring consideration f, whether our project has a useful life expectancy that offers maximum benefit to the state, we received a score of 5 out of 10, but believe that we could have received a 6 out of 10. This score is a result of our answer to question 31 in which we stated the useful life of our project as 20 years and provided the following explanation: *"The useful life of the project is 20 years pursuant to the minimum requirements. Our current infrastructure has been in place for nearly 16 years and is in good working order. We maintain all of our assets in good working order and expect the new infrastructure to last at least 20 years. Knife River has a proven record of maintaining our assets in Oregon."* Our intent for this project, though not expressly stated in our application, is to continue to operate for more than 20 years. The limiting factor for this operation is the amount of mining remaining at Reed pit in Salem. Knife

River Corporation has an estimated reserve of 25 million tons, or over 25 years of mining before the reserves are depleted. The operation will continue until these reserves are depleted. We request that this information be considered with our application.

To further demonstrate the need for this project, we have included as an attachment (see Knife River Rock Train Demand Forecast) to this letter a projection of the Portland Metropolitan Area market demand for construction aggregates for the years 2016 through 2025. The forecast demand for the Portland Metro Market in this case has been derived from estimated population growth and new housing starts as predicted by the Oregon Office of Economic Analysis. At our current market share, these projections show that the demand for construction aggregates by our facilities at Coffee Lake and Hillsboro will exceed the rock train system capacity under the current operating environment. The graph shows that the capacity of the rock train has changed over the years due to decreases in track speed and increased traffic on Portland and Western's line. In order to meet the projected demand Knife River will have to resort to supplementing the rock train operation with trucks if the project is not completed. Let us be clear, this project will take trucks off of the highway.

Also included as attachments to this letter are additional letters of support from a few of our largest customers who can attest to the need for this project, as well as a letter of support from Portland and Western Railroad. These items were not included in our original application, and we request that they be taken into consideration by each of the ConnectOregon VI review committees.

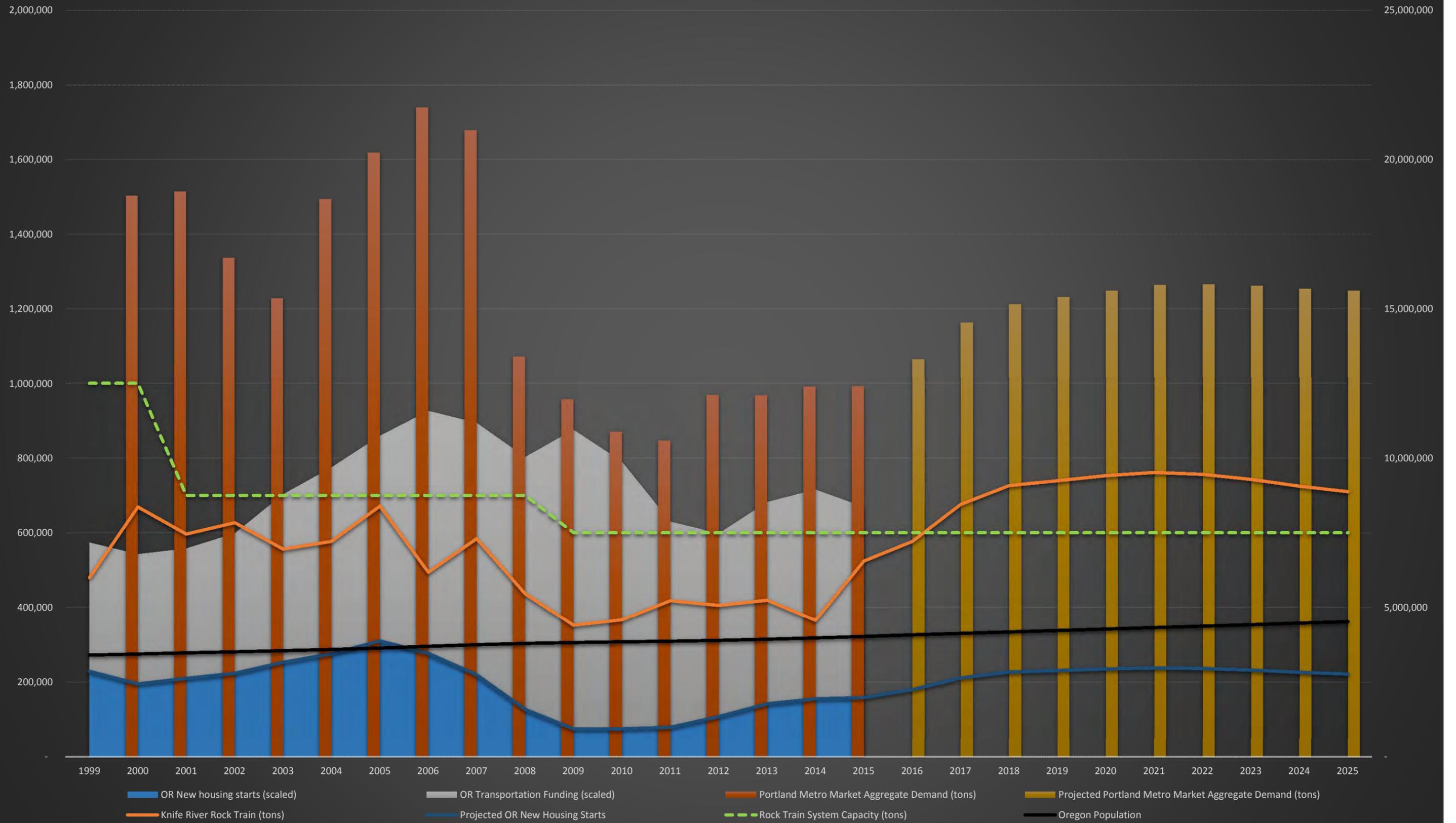
The transportation costs associated with getting construction aggregates to market make up the largest component of cost within the value chain. Knife River Corporation – Northwest has taken a multi-modal approach to getting our aggregates to the end user in the most efficient manner possible whether it be by truck, rail, or barge. In the case of the rock train, the average 500,000 tons/year moved from the Willamette Valley up to the Portland Metropolitan market eliminate approximately 16,700 truck trips from our roads and highways. With the completion of this proposed project we will increase the capacity of the operation by improving the efficiency & capacity of Knife River's facilities, decreasing train turn times for Portland & Western, and improving reliability for all of our customers. With this project we have the opportunity to eliminate thousands of additional truck trips from state highways each year which will not only reduce the transportation costs for all users of our value added construction aggregate products, but reduce wear and tear on the State highway system. These investments will deliver tremendous value to the state's economy for many years to come, and are a great reason to select this project for funding under the ConnectOregon VI program.

Respectfully,



Brodie Harvey  
Knife River Corporation – Northwest

# Knife River Rock Train Demand Forecast





*Kevin R. Haugh  
General Manager*

November 19<sup>th</sup>, 2015

Mr. Brodie Harvey  
Railroad Manager  
Knife River Corporation  
Portland, Oregon

Dear Mr. Harvey,

The Portland & Western Railroad, Inc., supports Knife River's ConnectOregon VI application to construct additional aggregate storage & conveyance equipment at both the Reed and Coffee Lake Facilities. We are confident that these improvements will improve the efficiency of the entire operation, and provide opportunities for both our companies to expand the business of getting aggregates to market.

The Portland & Western has benefited from our association with Knife River in Oregon for many years. Since we began providing service for Knife River aggregates in the late 90's the revenue stream generated from rock train traffic has allowed us to continuously invest in the maintenance of our own track infrastructure for which all customers along the line benefit. It is without a doubt that millions of dollars of private funding has been invested into Knife River's infrastructure to make the rock train operation possible for which the Oregon Economy has also greatly benefited. By our own estimates, the rock train can be attributed with the removal of over ½ million trucks off Oregon's roads over its history. This project is another great example of how a public private partnership can leverage public financing to generate tremendous returns for our local economy.

We look forward to continuing to work with Knife River Corporation and are hopeful for the opportunity to expand the amount of customers that we can reach with Knife River's aggregates carried by the rock train.

Sincerely,

Kevin R. Haugh  
General Manager



# ROGER LANGELIERS CONSTRUCTION CO.

## Concrete Construction:

- ◆ Paving
- ◆ Curb & Gutter
- ◆ Saw & Seal
- ◆ Flat Work
- ◆ Catch Basins
- ◆ Pavement Maintenance

Oregon CCB # 106364  
Washington CCB # ROGERLC027KQ

February 29, 2016

Billy Stimpson  
General Manager  
Knife River Corporation - Northwest  
Linnton, OR

Mr. Stimpson,

Roger Langeliers Construction Co. is a privately owned Oregon construction company which employs over 100 team members between our operations in western & central Oregon. We are pleased to write this letter in support of Knife River Corporation's application for ConnectOregon funds to make improvements to its rock train operations. We understand that this operation is critical to supplying the aggregate necessary for the production of ready mix concrete at both the Coffee Lake Ready Mix facility, and the Hillsboro Ready Mix facility. As a consumer of ready mix concrete, we purchase large volumes for the projects that we build every year from these two facilities. We have seen continued growth in the demand for our concrete construction services in the years since the 2007 recession, and believe that these facilities will play increasingly important roles in the Portland metro market going forward. Knife River's ability to supply these facilities with quality sand & gravel out of the Willamette valley has resulted in a consistent high quality ready mix product at a competitive price.

We feel strongly that any investment made to improve the efficiency of the rock train operation will be a benefit to our company, the industry as a whole, and the state of Oregon. Not only does the investment help keep trucks hauling concrete aggregates off of state highways, but it ensures the availability of high quality alluvial sand & gravels out of the Willamette valley. We recommend that the review committees select the Knife River rock train improvement project for ConnectOregon funding.

Sincerely,

Webster Langeliers

President

7145 NW Progress Court  
Hillsboro, OR 97124  
(503) 533-5709  
FAX (503) 533-7891

62880 Mercury Place  
Bend, OR 97701  
(541) 318-6200  
FAX (541) 318-9050  
Page 164/177

32929 Roberts Court  
Coburg, OR 97408  
(541)302-6648  
FAX (541) 302-6647

February 29, 2016

Brandon Bond  
Asphalt Operations Manager  
Knife River Corporation - Northwest  
Sherwood, OR



Mr. Bond,

BRIX Paving Company would like to extend our support of your application for Connect Oregon funds. Our company has worked in the paving industry in the Northwest since 1998. In 2015, we purchased more than 50,000 tons of hot mix asphalt from your Coffee Lake asphalt plant in order to complete a wide variety of projects. The hot mix asphalt that we purchase is used on projects both big and small ranging from State highways to residential driveways, and everything in between. We will continue to bring our business to Knife River's Coffee Lake asphalt plant due to its strategic location in a growing market, the competitive prices, and the consistency & quality of its hot mix asphalt products. Without the availability of hot mix asphalt from your facility, our company would be forced to truck material longer distances at added costs to our customers. It is critical that the facility continue to be able to meet the demand of the market.

Any investment made into the Knife River's Coffee Lake facility and rock train operation should be viewed as an investment in the future of our state. We believe it will help to keep trucks hauling rock off of our highways and streets thereby decreasing the cost of transportation investment for the taxpayer. With more and more people moving to Oregon and building houses or starting business, we expect the demand for our services and Knife River's products to continue to grow. It is without a doubt that the Knife River Coffee Lake facility will be increasingly important for the region. For these reasons, it is our recommendation that the Connect Oregon selection committee choose the Knife River project for funding.

Thank You,

BRIX Paving Company

A handwritten signature in black ink, appearing to read "Brick R. Obye".

Brick R. Obye

President

February 29, 2016

Lakeside Industries  
P.O. Box 7016  
Issaquah, WA 98027

Stratos Flanders  
General Manager  
Knife River Corporation - Northwest  
32260 Old Hwy 34  
Tangent, OR

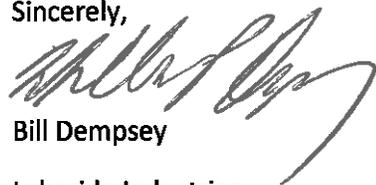
Dear Mr. Flanders,

Lakeside Industries is a family-owned and locally managed company that has been in operation since the 1950's. We rely on the Knife River/Portland & Western rock train for the delivery of a majority of our crushed aggregates for the production of hot mix asphalt at our Hillsboro plant. We have benefited from our relationship with Knife River in this manner since the late 1990's when the rock train began running to our Hillsboro facility. 2015 was a good year for our company. We received shipments of approximately 80,000 tons of aggregate at this facility via the rock train which comprised a large portion of our production for the year. We look forward to an equally busy year in 2016.

We support Knife River's application for ConnectOregon VI grant funds. We believe that this project will benefit our organization for years to come, and will allow us to continue to be competitive in this market for quality asphalt products. We also feel strongly that this project will benefit the state of Oregon's freight system by keeping trucks off of local streets and highways. It is a good use of the ConnectOregon funding, and a project that should be strongly considered by the ConnectOregon review committees.

Please accept our support for this project.

Sincerely,



Bill Dempsey

Lakeside Industries

**Comments Received**

**Project # 4R0417**

**Red Rock Biofuels LLC**

**Rail Spur & Lake County RR Bridge Improvements**



**BUILDING AMERICA®**

April 8, 2016

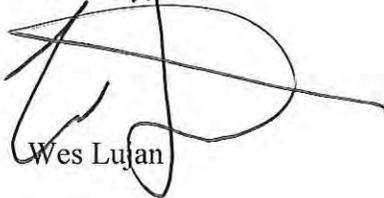
Mr. Matthew Garrett  
Director  
Oregon Department of Transportation  
355 Capitol Street NE, MS 11  
Salem, OR 97301-3871

Dear Mr. Garrett,

On behalf of Union Pacific Railroad (UPRR), I am writing to inform you of our company's acknowledgement and support of Red Rock Biofuels LLC (RRB) ConnectOregon VI application to help construct a rail spur at its biofuels facility and improvements to two Lake County Railroad bridges. These improvements will help them provide safe and reliable service to modern railcars.

UPRR values our relationship with the RRB and we look forward to working with them on this project if funding becomes available. Furthermore, we pledge to work with the parties involved to ensure all plans follow UPRR's engineering standards and safety requirements. Thank you for your consideration of investment in this important project. Please feel free to contact me if you have any questions.

Sincerely,



Wes Lujan

## **Comments Received**

**Project # 2T0431**

**Lane Transit District**

**Santa Clara Community Transit Center and  
Park & Ride**

June 13, 2016

Aurora Jackson, General Manager  
Lane Transit District  
P.O. Box 7070  
Springfield, OR 97475

Dear Ms. Jackson,

I am writing this letter of support for the Santa Clara Community Transit Center project on behalf of Cornerstone Community Housing. This project will increase access to employment, healthcare, and recreational opportunities for the more than 200 residents of Cornerstone's Santa Clara housing communities.

Cornerstone Community Housing operates four affordable residential communities in Santa Clara. Furthermore, we've been awarded HOME funds to construct an additional 28 units of affordable housing in this area. We're excited about the benefits that residents will receive from LTD's new community resource. Not only will our latest project have access to the new transit center, but three of our four Santa Clara communities are within a half mile of the proposed site. Those communities include Apple Orchard, Oak Leaf, and Green Leaf Villages.

This project aligns with our mission to provide quality, affordable housing and offer services that promote opportunities for personal and economic independence. We are proud to support this proposal and appreciate the excellent work LTD has put into developing it. We strongly encourage the commitment of financial resources required to fund this project.

Sincerely,



Darcy Phillips  
Executive Director

## TURNOY Scott

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**From:** Peter Bolander <pwbolander@hotmail.com>  
**Sent:** Saturday, June 11, 2016 10:39 AM  
**To:** Connect Oregon  
**Cc:** ltd@ltd.org  
**Subject:** Comment on Proposed Lane County ConnectOregon Grant Proposal

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hello:

I am writing to offer a review comment on one of the proposed Lane County ConnectOregon grant applications. I trust you are the folks to connect with concerning my comment and I trust my comments do not come too late in the evaluation process. I learned just last week about this particular project and it's submittal for consideration.

The project I have a comment on is the Lane Transit District proposal to help fund the Santa Clara park-and-ride facility. My comment is to ensure that in the evaluation process that the team looks at the impact of this facility to the traffic on River Road.

My understanding is that no agency has performed a traffic study around the River Road/Beltline interchange which this project is very close to. Many homes have been built just north of this interchange recently but due to either their size and/or location (more than a mile from the interchange) I have been told that a traffic study is not required. Unfortunately every car from these recently built (or soon to be built) homes travel south to this interchange to get to the Eugene and/or Springfield area, there is no other way to go south and I would guess that 99% of all people along River Road in this area travel to the Eugene/Springfield area. Adding buses and especially having them turn left from the proposed facility onto River Road is only going to add to the current traffic congestion that occurs every weekday morning and evening and other times during the week. Therefore I urge that a traffic study be performed to evaluate the impact of the proposed facility to traffic on River Road and the River Road/Beltline interchange prior to approving this proposed project.

In additional I find it ironic that just a few years ago the current Lane Transit District park-and-ride, just south of the River Road/Beltline interchange, sold part of that facility's parking area to a store (Aaron's). If they really need additional space now why was part of their current park-and-ride area sold a few years ago to create a smaller park-and-ride? It doesn't seem to make sense to me, especially when I think the location is much better suited for a park-and-ride location compared to the proposed location.

Thank you for your time and consideration of my request prior to your final decision on the proposed ConnectOregon grant application.

If have any further questions or need any clarification of my request please let me know.

Peter Bolander  
229 Rosemary Ave.  
Eugene, Oregon 97404  
541-344-0849

**Comments Received**

**Project # 1R0413**

**Union Pacific Railroad**

**Portland Passenger-Freight Rail Speed Improvement  
Project**

## TURNOY Scott

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**From:** Julie Chapman <bugthewonderdog@gmail.com>  
**Sent:** Sunday, June 26, 2016 10:54 PM  
**To:** TURNOY Scott  
**Subject:** Fwd: ConnectOregon project funding

Dear Scott Turnoy,

If you are not the correct person to receive this letter would you please forward it to the correct recipient.

Regarding the application for Union Pacific junction improvements in Portland:

“Union Pacific and subsidiaries, track, signal and elevation improvements at a Portland junction of UP and BNSF railroads, \$8.3 million request, \$4.7 million match. The project will eliminate a 10 mph restriction and reduce wait times for 35 daily Amtrak (passenger) and freight trains by up to 21 minutes.”

<http://pamplinmedia.com/pt/9-news/311818-190296-eight-metro-area-projects-in-line-for-state-bonds>

Wait! Wasn't there just a unanimous City Council decision in November, 2015, for Portland to build no new fossil fuel infrastructure (Resolution 1157)?

And, what makes Union Pacific a worthy recipient of our state bond monies? They have been less than respectful of their responsibilities to guarantee safe transit of these trains, for the provision of track safety for the excess weight of the tanker cars, and in allowing clouds of coal dust from uncovered coal cars to pollute waterways and railside communities.

I would appreciate an explanation of how this serves the interests of the people of Oregon, and not just the narrow interests of Union Pacific. These expanded wait times are the result of large shipments of fossil fuels for transport, at a critical time when there are daily demonstrations of the lethal climate impacts of temperature rise/carbon dioxide pollution.

Thank you for your attention,

Julie Chapman  
1720 SW 4th Avenue, #1616  
Portland, Oregon 97201

*“Every night on the TV news now is like a nature hike through the Book of Revelations,” Al Gore, TED talks.*

# **Comments Received**

**Project # 4A0365**

**City of Redmond, Oregon**

**Redmond Municipal Airport**

# CENTRAL OREGON AREA COMMISSION ON TRANSPORTATION COACT

July 14, 2016

Tammy Baney, Chair  
Oregon Transportation Commission  
Oregon Department of Transportation  
Communications Division  
355 Capitol Street NE, MS 11  
Salem, OR 97301-3871

Dear Chair Baney and Commissioners:

On behalf of the Central Oregon Area Commission on Transportation (COACT), I am writing to express our support of the list of ConnectOregon VI projects as prioritized by the *Final Review Committee*. The process went very well and it effectively recognized the rankings of both the Modal and Region Review Committees. We urge you to approve the list as presented.

We would also like to testify to the state wide significance of project 4A0365, the Redmond Airport Taxiway B Rehabilitation, ranked 37<sup>th</sup> by the Final Review Committee. This project is right at the *cut line* for funding, and deserves to be funded. Taxiway B connects the primary runway with the US Forest Service area of the airport which includes smoke jumpers, hot shots, the regional training base, a national cache and fire retardant operations. The taxiway pavement index is rated '0', which means C-130 aircraft cannot use it, when it is needed for fire retardant *fill-ups* during fire-fighting operations across the northwest. Another important statewide consideration is the FEMA designation of Redmond Airport as the operations hub for Oregon during the Cascadia event.

We urge you to support the list as prioritized and include Redmond Airport. Thank you for the consideration, and for the opportunity to provide input to ConnectOregon and other transportation investment decisions affecting Central Oregon.

Sincerely,



Alan Unger, COACT Chair  
Deschutes County Commissioner

Cc: Commissioner Wayne Fording, Jefferson County  
Judge Mike McCabe, Crook County  
Lonny Macy, Confederated Tribes of Warm Springs  
Bob Bryant, ODOT Region 4  
Gary Farnsworth, ODOT Region 4  
Scott Turnoy, ODOT ConnectOregon VI Program

**Comments Received**

**Project # 1M0407**

**Port of Portland**

**Terminal 6 Auto Staging Facility**



Melanie Wiegner  
Regional Director  
Government Relations  
(916) 442-2929

Ford Motor Company  
925 L Street  
Suite 390  
Sacramento, CA 9581

July 11, 2016

Tammy Baney, Chair  
Oregon Transportation Commission  
355 Capitol Street NE, MS #11  
Salem, OR 97301-3871

Dear Commissioner Baney:

Ford Motor Company is pleased to support the Port of Portland's (Port) Connect Oregon VI funding application for the Terminal 6 Auto Staging Facility. As you know, we work with Auto Warehousing Company (AWC) which processes our exports at the Berth 601 facility at the Port.

We are considering expanding our export volume through the Port of Portland if expansion plans for auto staging for the Berth 601 facility come to fruition. Our projections for volume could increase from approximately 50,000 units for the 2015 calendar year to close to 150,000 units in the coming years. If expansion is not completed, we will need to secure space at other port locations.

Ford Motor Company values its partnership with both AWC and the Port of Portland. We appreciate the Port's ongoing commitment to updating and improving infrastructure at the Terminal 6 Auto Staging Facility. We fully support this project for consideration for funding under the Connect Oregon VI program.

Thank you for considering our comments regarding this important matter and please don't hesitate to contact us should you have questions.

A handwritten signature in black ink, appearing to read "Melanie Wiegner", with a long horizontal flourish extending to the right.

Melanie Wiegner  
Regional Director, Government Relations

cc: Commissioner David Lohman  
Commissioner Susan Morgan  
Commissioner Alando Simpson  
Commissioner Sean O'Hollaren  
Tim Van Wormer, Port of Portland  
Dennis Matteo, AWC