2022 ODA Pavement Evaluation Program
Sunriver Airport

Sunriver, Oregon

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Prepared for

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TABLE OF CONTENTS

TABLES

FIGURES

APPENDICES

APPENDICES (continued)

Appendix F: Work History Report

1 OVERVIEW

GRI assisted with updating the Oregon Department of Aviation (ODA) airport pavement management system and developing a five-year plan for global maintenance and rehabilitation (M&R) and preservation work for the Sunriver Airport in Sunriver, Oregon. This project was implemented as a part of the ODA and Federal Aviation Administration (FAA) *Oregon Continuous Aviation System Plan*. The information provided in this report ensures compliance with FAA Grant Assurance Number 11, which outlines that an airport shall have an effective airport pavement maintenance-management program in place to receive federal financial assistance for the construction, reconstruction, or repair of airport pavements.

GRI conducted surveys of the airside pavement at Sunriver Airport in 2022 in accordance with the procedures of Advisory Circular 150/5380-7B and ASTM International (ASTM) D5340. We uploaded the survey data into the PAVER database and used the software to provide a rapid calculation of the pavement condition index (PCI) rating. The PCI is a numerical indicator that defines the functional condition of the pavement based on visual inspection. The scale ranges from zero to 100, where zero represents a pavement in the worst possible condition with no remaining functional life and 100 represents a pavement in the best possible condition with no defects.

2 PAVEMENT INVENTORY

Sunriver Airport is located in Sunriver, Oregon, and is owned and operated by Sunriver Resort. The airport consists of one runway that serves a variety of general aviation and air taxi aircraft. The general location of the airport is shown on the Sunriver Airport Location Map, Figure 2.1.

Figure 2.1 – SUNRIVER AIRPORT LOCATION MAP

Sunriver Airport contains one runway, one primary parallel taxiway, and multiple connector taxiways and aprons. The airside pavements at Sunriver Airport are comprised of asphalt concrete (AC) and portland cement concrete (PCC) pavements. The airport pavements, delineated by surface type and branch use, are shown on the Sunriver Airport Pavement Area by Surface Type, Figure 2.2 and the Sunriver Airport Pavement Area by Branch Use, Figure 2.3. The pavement inventory, including work history for each pavement section, is displayed spatially on the Sunriver Airport Pavement Inventory, Figure 2.4. The pavement facilities summarized by branch and section are listed in Tables 1A and 2A, respectively, in Appendix A. The sample unit layout for each section is shown on Figure 1A in Appendix A. We used the sampling rates outlined in Table 3A of Appendix A in our survey. The pavement inventory, including work history for individual airport pavement sections, is provided in the Work History Report, Appendix F.

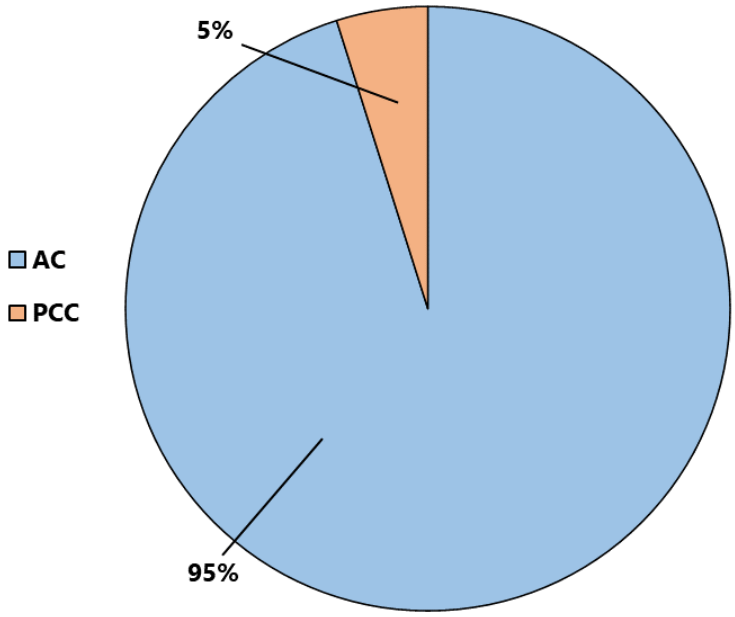


Figure 2.2 – SUNRIVER AIRPORT PERCENT OF PAVEMENT AREA BY SURFACE TYPE

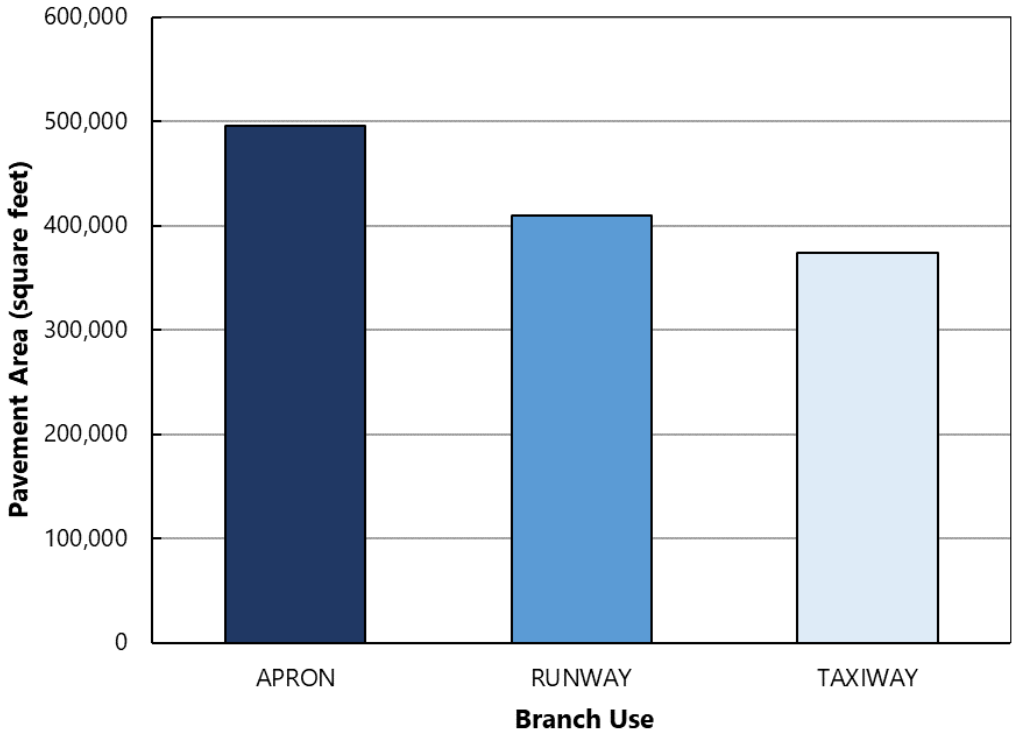
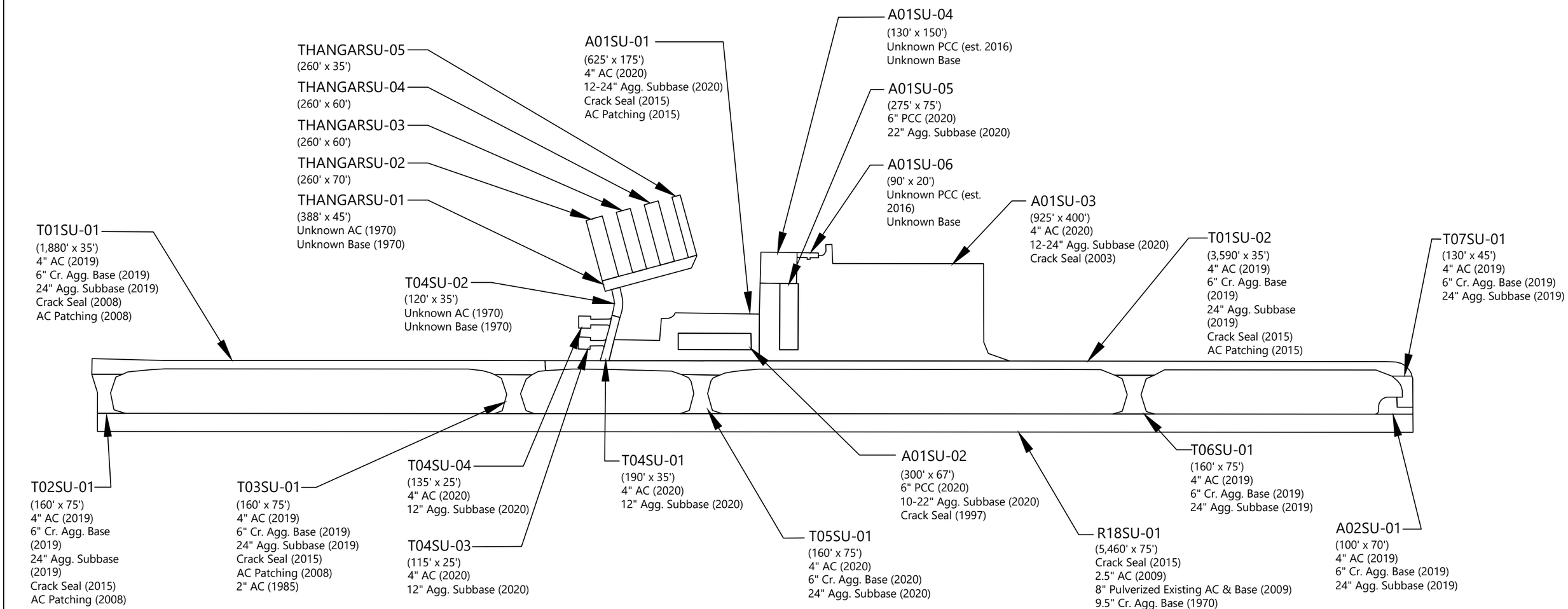
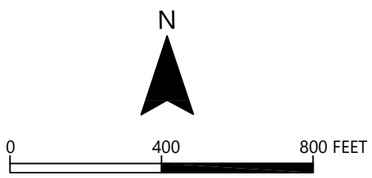


Figure 2.3 – SUNRIVER AIRPORT PAVEMENT AREA BY BRANCH USE



Abbreviations: AC = Asphalt Concrete; PCC = Portland Cement Concrete; Cr. = Crushed; Agg. = Aggregate; est. = Estimated



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STATEWIDE PAVEMENT EVALUATION PROGRAM

SUNRIVER AIRPORT
PAVEMENT INVENTORY

DEC. 2022 JOB NO. 6593-B FIG. 2.4

3 PAVEMENT CONDITION INSPECTION RESULTS

3.1 Introduction

GRI conducted a visual PCI survey of the airside pavements at Sunriver Airport in March 2022. The 2022 survey work was performed on sections last inspected in 2017 in order to update the Sunriver Airport inspection data. GRI performed the 2022 PCI survey in accordance with the methods described in FAA Advisory Circular 150/5380-6C and ASTM D5340, and further discussed in Appendix B of this report.

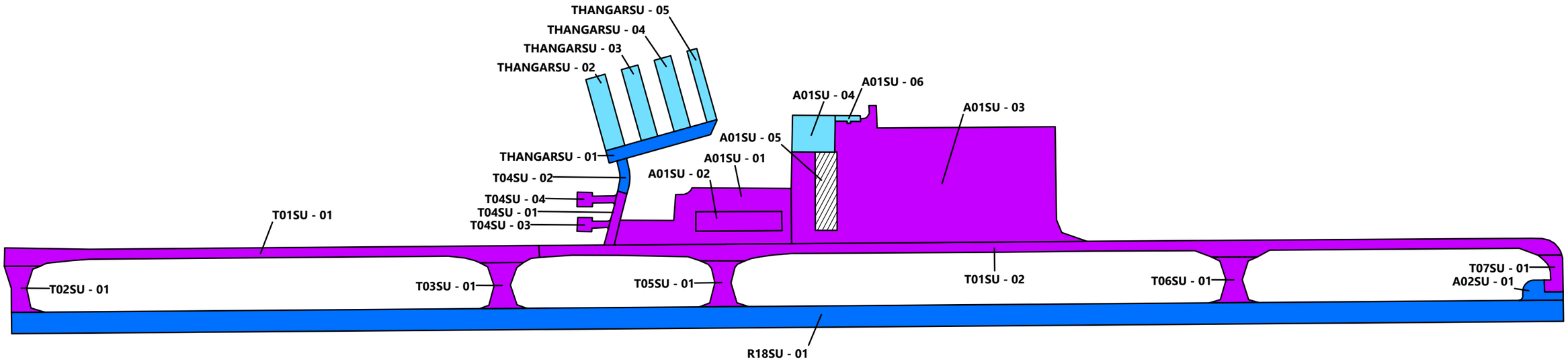
The PCI is based on the type, severity, and quantity of each distress found in an inspected sample unit. Further discussion of distress types for flexible and rigid pavement is provided in Appendix B and summarized in Table 1B. The results of the PCI survey are displayed using a seven-category rating scale in accordance with ASTM D5340. Details of the ASTM PCI rating scale are provided in Table 3-1 below.

Table 3-1: ASTM PCI RATING SCALE

PCI Color Legend	PCI Range	PCI Rating and Definition
	86 – 100	GOOD: Pavement has minor or no distresses and should require only routine maintenance.
	71 – 85	SATISFACTORY: Pavement has scattered low-severity distresses that should require only routine maintenance.
	56 – 70	FAIR: Pavement has a combination of generally low- and medium-severity distresses. Maintenance and repair needs may range from routine to major.
	41 – 55	POOR: Pavement has low-, medium-, and high-severity distresses that probably cause some operational problems. M&R needs will be major.
	26 – 40	VERY POOR: Pavement has predominantly medium- and high-severity distresses that cause considerable maintenance and operational problems. M&R needs will be major.
	11 – 25	SERIOUS: Pavement has mainly high-severity distresses that may affect operational safety; immediate repairs are needed.
	0 – 10	FAILED: Pavement deterioration has progressed to the point that safe aircraft operations are no longer possible; complete reconstruction is required.

3.2 Pavement Condition Index Survey Results

The area-weighted average PCI for all airport pavements at Sunriver Airport is approximately 84. The section PCIs ranged from a low of 57 to a high of 100. The primary distresses observed during the inspection were weathering, longitudinal and transverse cracking, fatigue (alligator) cracking, and patching for AC-surfaced pavements, and spalling, linear cracking, shrinkage cracking, and faulting for PCC-surfaced pavements. Section PCIs following our pavement survey are displayed below spatially on the 2022 PCI Survey Results, Figure 3.1. Section A01SU-05 was not surveyed due to obstructions.



*NOTE: UNABLE TO SURVEY A01SU-05 DUE TO OBSTRUCTION

2022 SECTION PCI

- (86 - 100) GOOD
- (71 - 85) SATISFACTORY
- (56 - 70) FAIR
- (41 - 55) POOR
- (26 - 40) VERY POOR
- (11 - 25) SERIOUS
- (0 - 10) FAILED

N

0 350 700 Feet

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STATEWIDE PAVEMENT EVALUATION PROGRAM

2022 PCI SURVEY RESULTS

SUNRIVER AIRPORT

DEC. 2022 JOB NO. 6593-B FIG. 3.1

The condition distribution of the network by the percent of total pavement area is provided on the Sunriver Airport Pavement Condition Rating by Percent of Pavement Area, Figure 3.2. A summary of the pavement condition results by branch and section are included in Tables 2B and 3B of Appendix B, respectively. A comparison between the previous inspection and the 2022 inspection is provided in Table 4B in Appendix B. The re-inspection report that includes inspection details for individual sample units is provided in Table 1E in Appendix E.

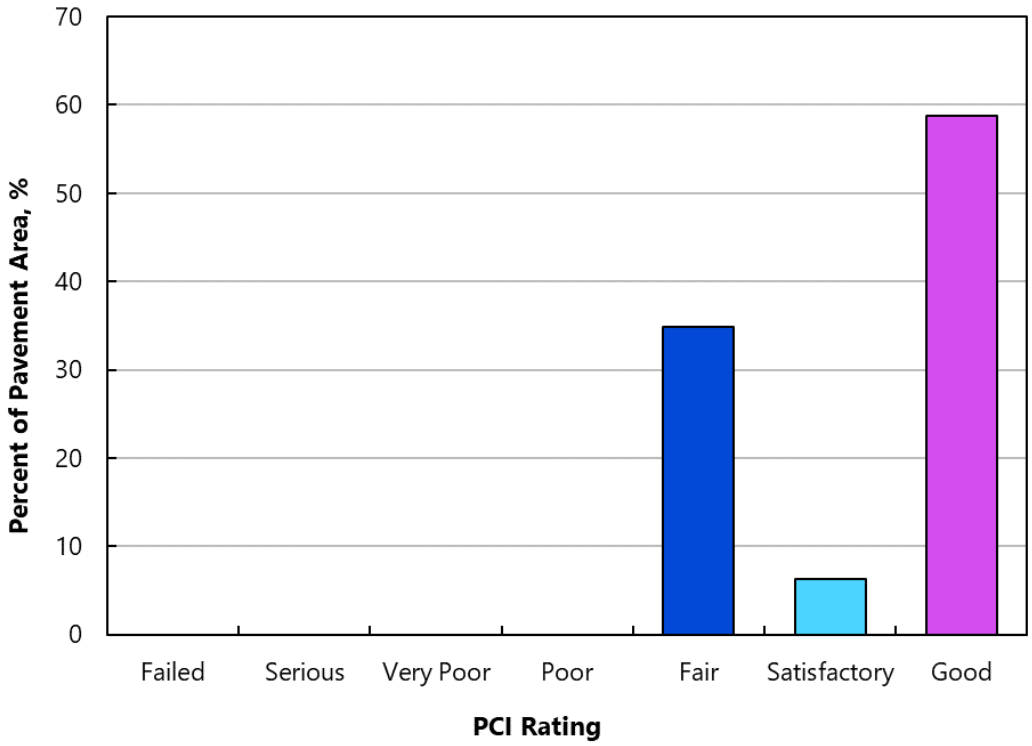


Figure 3.2 – SUNRIVER AIRPORT PAVEMENT CONDITION RATING BY PERCENT OF PAVEMENT AREA

4 FUTURE PAVEMENT CONDITION ANALYSIS

4.1 Introduction

In addition to assessing the current condition of pavement, it is very important from a planning standpoint to be able to predict with reasonable accuracy the future condition. Additional details regarding our future pavement condition analysis, including pavement condition prediction models, are provided in Appendix C. PCI performance curves developed for Sunriver Airport are displayed on Figures 1C through 3C in Appendix C.

4.2 Future Condition Analysis

Using the condition prediction models discussed above, the projected condition of each pavement section was determined for 5- and 10-year periods. Based on this analysis, we project the PCI to decrease from a current value of 84 to a value of 72 in the year 2027

and 61 in the year 2032, if no maintenance or rehabilitation work is performed. The projected pavement condition in 5 years and 10 years for each pavement section at Sunriver Airport is displayed spatially on the Future Pavement Condition, Figure 4.1 and listed in Table 1C in Appendix C, along with the past and present PCI values for the pavement network.

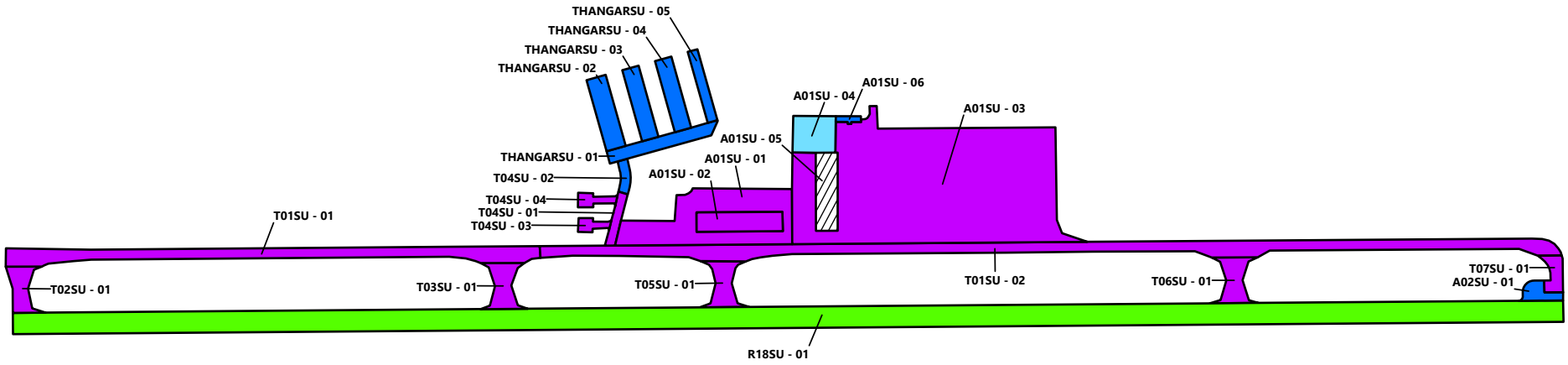
4.3 Functional Remaining Life

The functional remaining life is the practical amount of time a pavement is in service before requiring rehabilitation, as estimated based solely on visual condition. This is not to be confused with structural remaining life, which requires analysis of the structural capacity of a pavement and typically a field exploration and testing program that includes core explorations and falling weight deflectometer (FWD) deflection tests.

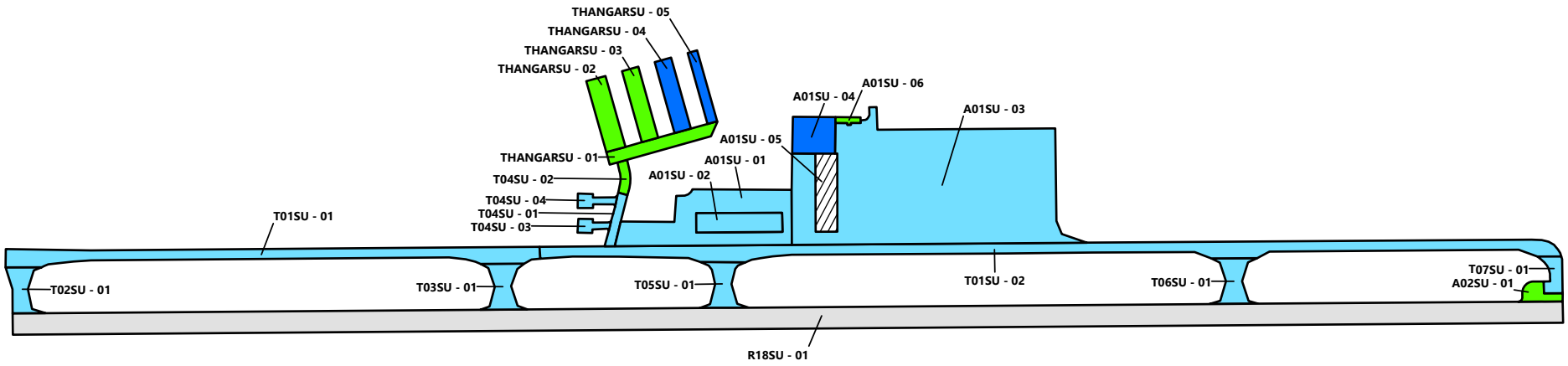
We calculated two forms of functional remaining life based on the current visual condition surveys of the pavement at Sunriver Airport. The first type of functional remaining life is the time until rehabilitation such as an overlay is needed. The critical PCI, further discussed in Section C.3 of Appendix C, is the threshold used for this type of functional remaining life analysis. The second type of functional remaining life is the time until the pavement is no longer operational due to high foreign object debris (FOD) potential and increased safety concerns for trafficking aircraft. A PCI of 40 was set as the trigger point for the end of the pavement's functional service life with regard to FOD potential.

The two types of functional remaining life for each section at Sunriver Airport are summarized in Table 2C in Appendix C.

PREDICTED CONDITION IN 2027



PREDICTED CONDITION IN 2032



*NOTE: UNABLE TO SURVEY A01SU-05 DUE TO OBSTRUCTION

SECTION PCI

- (86 - 100) GOOD
- (71 - 85) SATISFACTORY
- (56 - 70) FAIR
- (41 - 55) POOR
- (26 - 40) VERY POOR
- (11 - 25) SERIOUS
- (0 - 10) FAILED

N

0 600 1,200 FEET

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STATEWIDE PAVEMENT EVALUATION PROGRAM

FUTURE PAVEMENT CONDITION
SUNRIVER AIRPORT

DEC. 2022 JOB NO. 6593-B FIG. 4.1

5 MAINTENANCE AND REHABILITATION PROJECT RECOMMENDATIONS

5.1 Introduction

We evaluated Maintenance and Rehabilitation (M&R) needs, as determined from the PAVER analysis results, in order to develop localized maintenance, global maintenance, and rehabilitation needs. Details of our M&R work priority and unit costs for work activities are provided in Tables 1D and 2D, respectively, in Appendix D.

Based on the 2022 PCI-survey results, the Sunriver Airport Pavement Network General Treatment Type Distribution Based on PCI, Figure 5.1 displays a breakdown of the Sunriver Airport network pavement condition by percent of pavement area and general M&R treatment categories. Approximately 65%, 35%, and 0% of the area require preservation treatments, rehabilitation, and reconstruction, respectively.

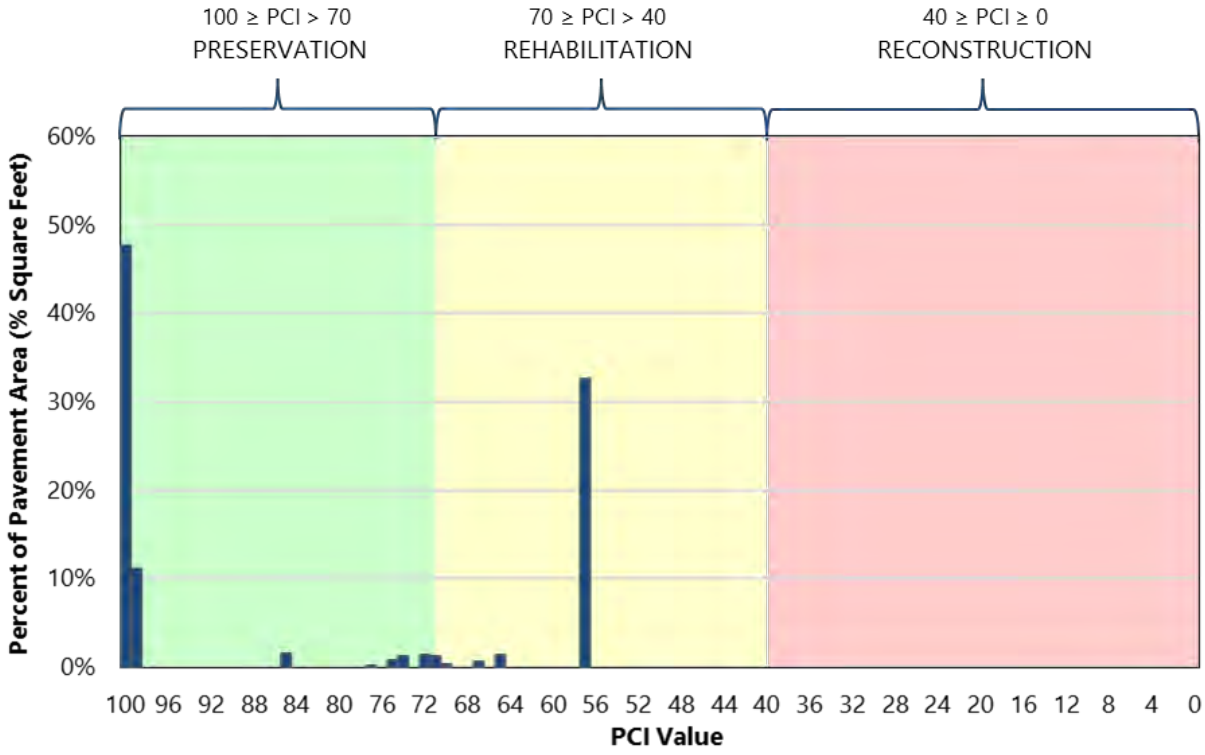


Figure 5.1 – SUNRIVER AIRPORT PAVEMENT NETWORK GENERAL TREATMENT TYPE DISTRIBUTION BASED ON PCI

5.2 Recommended Localized Maintenance

Localized maintenance refers to activities such as crack sealing and patching, which should be performed annually in order to properly maintain aging pavements. Using the PAVER Localized Distress Maintenance Analysis tool, we developed a list of recommended localized maintenance. This list is shown in Table 3D in Appendix D and is independent of the global maintenance and rehabilitation projects associated with the five-year global

maintenance and rehabilitation work plan. A summary of the approximate total localized maintenance quantities is provided in Table 5-1 below.

Table 5-1: LOCALIZED MAINTENANCE QUANTITIES

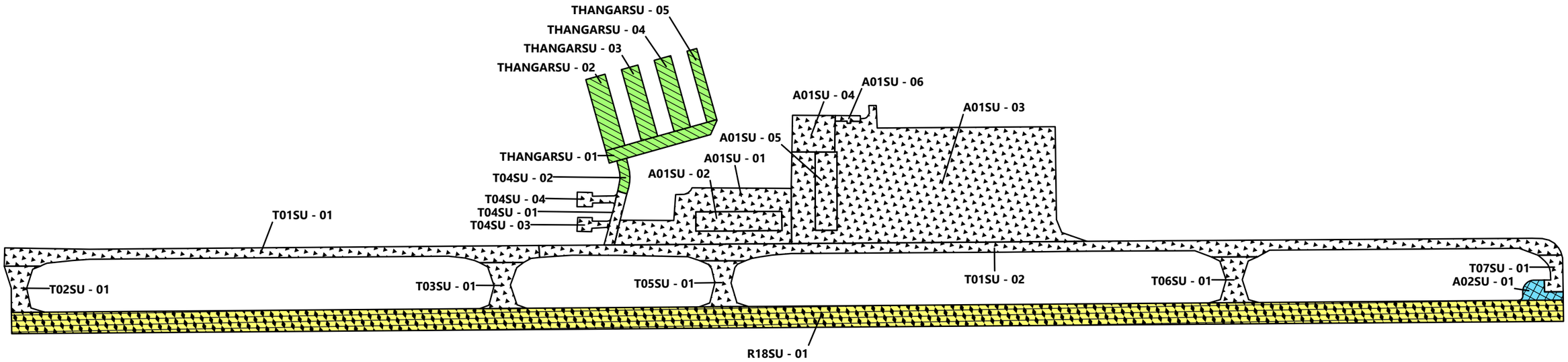
Localized Maintenance Operation	Approximate Quantity
Asphalt Concrete Crack Sealing	21,218 linear feet
Portland Cement Concrete Crack Sealing	150 linear feet
Asphalt Concrete Full-Depth Patching	6,805 square feet

5.3 Global Maintenance and Rehabilitation Plan

To develop the five-year work plan, we first ran the eliminate backlog scenario with the PAVER M&R Work Planning Module in order to generate a list, organized by year, of global M&R projects. We then reviewed the project list and refined it into practical construction projects for each year. A summary of global M&R quantities is provided in Table 5-2 below, and maps of the project locations by year are shown on the 5-Year Pavement Management Plan, Figure 5.2. The complete list of recommended global M&R projects is presented in Table 4D in Appendix D.

Table 5-2: GLOBAL MAINTENANCE AND REHABILITATION (M&R) QUANTITIES

Global Maintenance or Rehabilitation Operation	Quantity, square feet
Overlay	409,820
Fog Seal	7,016
Slurry Seal	80,173



ACTION TIMING

- 2023
- 2024
- 2025
- 2026
- 2027

ACTION

- FOG SEAL
- SLURRY SEAL
- OVERLAY
- RECONSTRUCTION
- ROUTINE MAINTENANCE

N

0 350 700 Feet

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STATEWIDE PAVEMENT EVALUATION PROGRAM

5-YEAR PAVEMENT MANAGEMENT PLAN
SUNRIVER AIRPORT

DEC. 2022 JOB NO. 6593-B FIG. 5.2

6 LIMITATIONS

This report has been prepared to assist the Oregon Department of Aviation (ODA) with pavement-related project planning for the Sunriver Airport. The scope is limited to the specific pavement areas described within this report. The conclusions and recommendations provided in this report are based on information provided by ODA, estimated costs, and an understanding of the pavement conditions based solely on visual assessment. The global maintenance and rehabilitation recommendations and project selections provided in this report, as well as their corresponding cost estimates, are based on a practical grouping of projects and an estimate of the structural requirements. It is possible that recommendations based on a structural evaluation would differ materially from the recommendations given herein. Therefore, the information included in this report should be used solely for project planning purposes, and it should be understood that rehabilitation costs may vary from the cost estimates given within this report.

Because the condition of the airport pavement network is dynamic, an effective maintenance and rehabilitation program should be reviewed and updated on a regular basis. In addition to regularly surveying and updating the pavement condition, completed construction activities should be tracked in the PAVER database. If Sunriver Airport would like to know more about the results presented in this report, please contact the undersigned.

Submitted for GRI,

RENEWS: 06/2023

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This document has been submitted electronically.

APPENDIX A

Pavement Inventory Reports and Maps

APPENDIX A

PAVEMENT INVENTORY REPORTS AND MAPS

A.1 PAVEMENT NETWORK

Sunriver Airport is located in Sunriver, Oregon, and is owned and operated by Sunriver Resort. The pavement network/facilities at Sunriver Airport serve a variety of general aviation and air taxi aircraft. Sunriver Airport consists of one runway, one primary parallel taxiway, and multiple connector taxiways and aprons. The airside pavements at Sunriver Airport are comprised of asphalt concrete (AC) and portland cement concrete (PCC) pavements.

The current airport pavement management system (APMS) network at Sunriver Airport has an approximate area of 1,280,000 square feet of paved airside facilities. The pavement network has previously been divided (by others) into a hierarchical order of branches, sections, and sample units that facilitate inspection and maintenance planning. The pavement facilities summarized by branch and section are listed in Tables 1A and 2A, respectively. Pavement sections and the sample unit layout for each section are shown on Figure 1A in this appendix.

A.2 BRANCHES

A branch, as defined in the PAVER system, is a facility that is a readily identifiable part of a pavement system and has a distinct function. For airports, branches typically consist of individual runways, taxiways, and aprons. The current pavement network for Sunriver Airport contains 11 branches, tabulated in Table 1A and shown on Figure 1A.

A.3 SECTIONS AND SAMPLE UNITS

A pavement section is the smallest management unit used when considering the application and selection of maintenance and rehabilitation (M&R) repairs and treatments and is defined by Section 2.1.8 of ASTM International (ASTM) D5340 as *"a contiguous pavement area having uniform construction, maintenance, usage history, and condition."* All sections should also have the same traffic volume and load intensity. The current pavement network included in the PAVER database for Sunriver Airport contains 24 sections that are managed by Sunriver Resort, which are tabulated in Table 2A and shown spatially on Figure 1A.

PAVER assigns a rank, which designates that pavement's prioritization in receiving maintenance and repair. The highest use or priority pavements, such as runways, taxiways, and terminal aprons, are ranked *Primary*, while the surrounding aprons and shoulders are

ranked *Secondary* and low-use areas are ranked *Tertiary*. The ranks for all sections are shown on Table 2A.

To facilitate the visual survey of the airport pavement, each section is further subdivided into smaller areas called sample units. Similar sizing of these units is critical, and studies have found that maintaining the size of the sample units to within 40% of the established normal distribution reduces the standard error of the average pavement condition index (PCI) values. To meet this criterion, the ASTM method recommends sample units for flexible pavements be 5,000 ± 2,000 square feet and 20 slabs ± 8 slabs for rigid pavements. The delineation of sample units for each section is displayed on Figure 1A.

A.4 SAMPLE UNIT DELINEATION

For an APMS survey, a PCI confidence level of 92% and an allowable error (e) of eight PCI points are used for all airport pavements. To determine the number of sample units that need to be inspected to achieve the required confidence level and allowable error, the following equation is used:

$$n = \frac{N \times s^2}{\left(e^2/_4\right)(N-1)+s^2} \qquad \text{(Equation 1)}$$

where:

n = number of sample units to be inspected
N = total number of samples in the pavement sections
e = allowable error
s = section standard deviation

For the 2022 Sunriver Airport PCI survey, Table 3A on the next page was used as a guideline in developing sampling rates for flexible and rigid pavement that reflect similar rates used for other large airport pavement networks. In general, this sampling rate distribution provides a 92% confidence level with a standard error of eight PCI points.

Sample unit locations at Sunriver Airport were selected using a systematic random sampling model method. This technique is implemented by first determining the number of sample units needed based on the confidence interval calculated using Equation 1. The first sample unit is randomly placed in the section and then the remaining sample units are systematically spaced throughout the section at an equal distance apart.

Table 1A – SUNRIVER AIRPORT PAVEMENT BRANCHES

Facility Designation (Branch ID)	Branch Name	Number of Sections	Approximate Area, square feet
A01SU	Apron 01 Sunriver	6	488,849
A02SU	Apron 02 Sunriver	1	7,016
R18SU	Runway 18/36 Sunriver	1	409,820
T01SU	Taxiway 01 Sunriver	2	217,530
T02SU	Taxiway 02 Sunriver	1	11,900
T03SU	Taxiway 03 Sunriver	1	14,230
T04SU	Taxiway 04 Sunriver	4	19,210
T05SU	Taxiway 05 Sunriver	1	13,690
T06SU	Taxiway 06 Sunriver	1	13,970
T07SU	Taxiway 07 Sunriver	1	7,431
THANGARSU	T-Hangar Taxiways Sunriver	5	76,038

Table 2A - SUNRIVER AIRPORT CURRENT PAVEMENT INVENTORY

BranchID	Branch Name	Branch Use	SectionID	From	To	Rank	Length, feet	Width, feet	Approximate Area, square feet	LCD	Surface Type
A01SU	Apron 01 Sunriver	APRON	01	T04SU-01	A01SU-03	P	625	175	76,472	7/1/2020	AC
A01SU	Apron 01 Sunriver	APRON	02	A01SU-01	A01SU-01	P	300	67	20,575	7/1/2020	PCC
A01SU	Apron 01 Sunriver	APRON	03	A01SU-01	T01SU-02	P	925	400	349,900	7/1/2020	AC
A01SU	Apron 01 Sunriver	APRON	04	A01SU-03	W. Edge	P	130	150	19,409	7/1/2020	PCC
A01SU	Apron 01 Sunriver	APRON	05	A01SU-04	East edge of PCC	P	275	75	20,625	7/1/2020	PCC
A01SU	Apron 01 Sunriver	APRON	06	A01SU-04	End	P	90	20	1,868	7/1/2020	PCC
A02SU	Apron 02 Sunriver	APRON	01	T07SU-01	R18SU-01	P	100	70	7,016	7/1/2019	AC
R18SU	Runway 18/36 Sunriver	RUNWAY	01	R36 End	R18SU-02	P	5,460	75	409,820	9/2/2009	AC
T01SU	Taxiway 01 Sunriver	TAXIWAY	01	R36 End	T03SU-01	P	1,880	35	76,490	7/1/2019	AC
T01SU	Taxiway 01 Sunriver	TAXIWAY	02	T03SU-01	R18 End	P	3,590	35	141,040	7/1/2019	AC
T02SU	Taxiway 02 Sunriver	TAXIWAY	01	T01SU-01	R18SU-01	P	160	75	11,900	7/1/2019	AC
T03SU	Taxiway 03 Sunriver	TAXIWAY	01	T01SU-02	R18SU-01	P	160	75	14,230	7/1/2019	AC
T04SU	Taxiway 04 Sunriver	TAXIWAY	01	T04SU-02	T01SU-02	S	190	35	6,361	7/1/2020	AC
T04SU	Taxiway 04 Sunriver	TAXIWAY	02	T04SU-01	SU-01	S	120	35	4,135	1/1/1970	AC
T04SU	Taxiway 04 Sunriver	TAXIWAY	03	T04SU-01	End	S	115	25	3,980	7/1/2020	AC
T04SU	Taxiway 04 Sunriver	TAXIWAY	04	T04SU-01	End	S	135	25	4,734	7/1/2020	AC
T05SU	Taxiway 05 Sunriver	TAXIWAY	01	T01SU-02	R18SU-01	P	160	75	13,690	7/1/2019	AC
T06SU	Taxiway 06 Sunriver	TAXIWAY	01	T01SU-02	R18SU-01	P	160	75	13,970	7/1/2019	AC
T07SU	Taxiway 07 Sunriver	TAXIWAY	01	T01SU-01	R18SU-01	P	130	45	7,431	7/1/2019	AC
THANGARSU	T-Hangar Taxiways Sunriver	TAXIWAY	01	T04	End	S	388	45	17,460	1/1/1970	AC
THANGARSU	T-Hangar Taxiways Sunriver	TAXIWAY	02	THANGAR-01	End	S	260	70	18,181	1/1/1970	AC
THANGARSU	T-Hangar Taxiways Sunriver	TAXIWAY	03	THANGAR-01	End	S	260	60	15,584	1/1/1970	AC
THANGARSU	T-Hangar Taxiways Sunriver	TAXIWAY	04	THANGAR-01	End	S	260	60	15,584	1/1/1970	AC
THANGARSU	T-Hangar Taxiways Sunriver	TAXIWAY	05	THANGAR-01	End	S	260	35	9,229	1/1/1970	AC

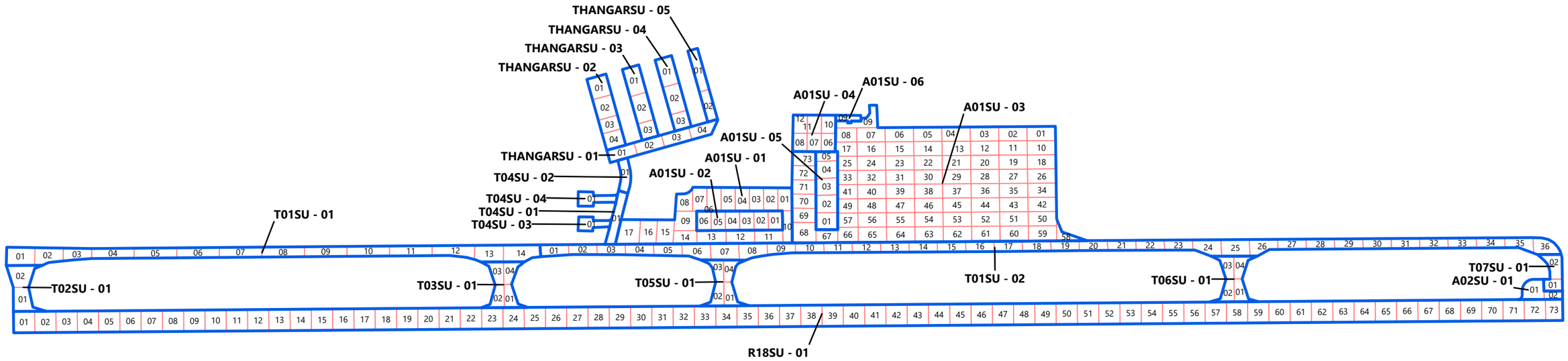
P = Primary pavement, S = Secondary pavement
LCD = Last Construction Date. The date of the last major rehabilitation (e.g. overlay)
AC = Asphalt Concrete, PCC = Portland Cement Concrete

Table 3A: EXAMPLE SAMPLE RATES FOR AC AND PCC PAVEMENTS

AC Sampling Rate	
Total Number of Sample Units, N	**Sample Units to Survey, n**
1	1
2-3	2
4-6	3
7-13	4
14-38	5
39+	6

PCC Sampling Rate	
Total Number of Sample Units, N	**Sample Units to Survey, n**
1	1
2	2
3-4	3
5-6	4
7-8	5
9-11	6
12-14	7
15-19	8
20-27	9
28-38	10
39-58	11
59-104	12
105-313	13
314+	14

Note: AC = Asphalt Concrete
PCC = Portland Cement Concrete



SECTION

SAMPLE UNIT

N

0 350 700

Feet

GRI

SAMPLE UNIT LAYOUT

SUNRIVER AIRPORT

DEC. 2022 JOB NO. 6593-B FIG. 1A

APPENDIX B

Pavement Condition Index Survey Results

APPENDIX B

PAVEMENT CONDITION INDEX SURVEY RESULTS

B.1 METHODOLOGY

As previously discussed, the PCI is a measure of the pavement's functional surface condition and provides a methodology for assessing the causes of distress and whether the distress is related to a load or climatic conditions. Although the PCI is not a direct measure of structural capacity, it provides a suggestion of the structural needs of the pavement.

The PCI is based on the type, severity, and quantity of each distress found in an inspected sample unit. The results are displayed using a seven-category rating scale in accordance with ASTM D5340. Flexible pavement (e.g., AC and AAC) and rigid pavement (e.g., PCC) distress types are presented in Table 1B. A summary of the pavement condition results by branch and section are included in Tables 2B and 3B of Appendix B, respectively.

Table 1B: PAVER DISTRESS CODES FOR FLEXIBLE AND RIGID PAVEMENT

Flexible Pavement			Rigid Pavement		
PAVER Code	Pavement Distress	Related Cause	PAVER Code	Pavement Distress	Related Cause
41	Alligator Cracking	Load	61	Blow-Up	Load
42	Bleeding	Other	62	Corner Break	Load
43	Block Cracking	Climate/ Durability	63	Longitudinal, Transverse, & Diagonal Cracks	Climate/ Durability
44	Corrugation	Other	64	Durability Cracking	Climate/ Durability
45	Depression	Other	65	Joint Seal Damage	Other
46	Jet Blast	Other	66	Small Patch	Other
47	Joint Reflection Cracking	Climate/ Durability	67	Large Patch	Other
48	Longitudinal & Transverse Cracking	Climate/ Durability	68	Pop Outs	Other
49	Oil Spillage	Other	69	Pumping	Other
50	Patching	Climate/ Durability	70	Scaling	Other
51	Polished Aggregate	Other	71	Faulting	Other
52	Raveling	Climate/ Durability	72	Shattered Slab	Load

Flexible Pavement		
PAVER Code	Pavement Distress	Related Cause
53	Rutting	Load
54	Shoving	Other
55	Slippage Cracking	Other
56	Swelling	Other
57	Weathering	Climate/ Durability

Rigid Pavement		
PAVER Code	Pavement Distress	Related Cause
73	Shrinkage Cracking	Other
74	Joint Spalls	Other
75	Corner Spalls	Other
76	Alkali-Silica Reactivity (ASR)	Other

To obtain the section PCI, we extrapolated the PCI of each selected sample unit over the entire section area. Distresses found in sample units classified as "additional" – defined as nonrepresentative instead of random - are not extrapolated over the entire section but merely added to the extrapolated quantity. The PCI rating scale presented previously in Table 3-1 of Section 3.1 and are based on ASTM D5340.

Section 4.1 of ASTM D5340 governing PCI surveys offers this caution:

*"The PCI is a numerical indicator that rates the surface condition of the pavement. The PCI provides a measure of the **present condition** of the pavement based on the distress observed on the surface of the pavement, which also indicates the structural integrity and surface operational condition (localized roughness and safety). The PCI **cannot** measure the structural capacity; neither does it provide a direct measurement of skid resistance or roughness. It provides an objective and rational basis for determining maintenance and repair needs and priorities. Continuous monitoring of the PCI is used to establish the rate of pavement deterioration, which permits early identification of major rehabilitation needs. The PCI provides feedback on pavement performance for validation or improvement of current pavement design and maintenance procedures."*

Based on the limitations of the PCI method, it is imperative that engineers and planners treat the PCI as a tool that will assist them during the M&R planning process. Any major project should always be preceded by an up-to-date, detailed, 100% project-level inspection of the pavement in order to reevaluate maintenance needs prior to the project design process.

B.2 DISTRESS TYPES

Distress tends to fall into one of the following four cause categories:

- **Load-related:** Flexible pavement distresses include alligator/fatigue cracking, corrugation, depression, polished aggregate, rutting, and slippage cracking. Rigid

pavement distresses include corner breaks, longitudinal cracking, divided slabs, polished aggregate, pumping, and joint spalling.

- **Climate- and durability-related:** Flexible pavement distresses include bleeding, block cracking, joint reflection cracking, longitudinal and transverse (L&T) cracking, swelling, and raveling/weathering. Rigid pavement distresses include blow-ups, durability cracking, longitudinal cracking, pop-outs, pumping, scaling, shrinkage cracks, and joint and corner spalling.

- **Moisture- and drainage-related:** Flexible pavement distresses include alligator/ fatigue cracking, depressions, potholes, and swelling. Rigid pavement distresses include corner breaks, divided slabs, and pumping.

- **Other factors:** Oil spillage, jet blast erosion, bleeding, patching, and concrete slab joint faulting.

As described above, a distress may be the result of more than one cause. For example, depressions may be caused by incorrect compaction during construction or by subgrade softening due to environmental factors. In addition, a distress may be initiated by one cause but may progress to a distress of higher severity by another cause. Therefore, engineering judgment is critical in analyzing the actual cause or causes of the distress.

B.3 PAVEMENT CONDITION INDEX SURVEY RESULTS

The evaluated Sunriver Airport pavement network consists of 11 branches and 24 sections. A total of 71 sample units were visually inspected in the field. Data from the inspected sample units were input into the PAVER database, and a resultant PCI for each section was computed. Additional details regarding the PCI and distress types observed for each surveyed sample unit are provided in the Re-Inspection Report, in Appendix E. Based on the 2022 PCI survey, the area-weighted average PCI for the entire pavement network at Sunriver Airport is approximately 84, which corresponds to a PCI rating of Satisfactory.

To investigate the rate of deterioration of each pavement section we compared the PCI results from the 2022 survey to the PCI results from the previous inspection. The variation in PCI between inspections for Sunriver Airport pavement sections is outlined in Table 4B in this appendix.

Table 2B - SUNRIVER AIRPORT CURRENT BRANCH CONDITION REPORT

Branch ID	Number of Sections	Approximate Area, square feet	Use	Area Weighted Average Branch PCI	PCI Category
A01SU	6	488,849	APRON	99	Good
A02SU	1	7,016	APRON	67	Good
R18SU	1	409,820	RUNWAY	57	Good
T01SU	2	217,530	TAXIWAY	99	Satisfactory
T02SU	1	11,900	TAXIWAY	100	Good
T03SU	1	14,230	TAXIWAY	100	Satisfactory
T04SU	4	19,210	TAXIWAY	94	Fair
T05SU	1	13,690	TAXIWAY	100	Fair
T06SU	1	13,970	TAXIWAY	100	Good
T07SU	1	7,431	TAXIWAY	100	Good
THANGARSU	5	76,038	TAXIWAY	71	Good

Use Category	Number of Sections	Total Area, square feet	Area Weighted Average PCI
APRON	7	495,865	99
RUNWAY	1	409,820	57
TAXIWAY	16	373,999	93
ALL	**24**	**1,279,684**	**84**

Table 3B - SUNRIVER AIRPORT 2022 PAVEMENT CONDITION INDEX SURVEY RESULTS

BranchID	SectionID	Last Construction Date	Surface Type	Use	Last Inspection Date	Age at Inspection	PCI	PCI Category	PCI % Climate	PCI % Load	PCI % Other
A01SU	01	7/1/2020	AC	APRON	3/1/2022	2	100	Good	100	0	0
A01SU	02	7/1/2020	PCC	APRON	3/1/2022	2	100	Good	0	0	0
A01SU	03	7/1/2020	AC	APRON	3/1/2022	2	100	Good	100	0	0
A01SU	04	7/1/2020	PCC	APRON	3/1/2022	2	85	Satisfactory	0	44	56
A01SU	05	7/1/2020	PCC	APRON	3/1/2022	2	--	--	--	--	--
A01SU	06	7/1/2020	PCC	APRON	3/1/2022	2	77	Satisfactory	0	70	30
A02SU	01	7/1/2019	AC	APRON	3/1/2022	3	67	Fair	64	36	0
R18SU	01	9/2/2009	AC	RUNWAY	3/1/2022	12	57	Fair	46	54	0
T01SU	01	7/1/2019	AC	TAXIWAY	3/1/2022	3	100	Good	100	0	0
T01SU	02	7/1/2019	AC	TAXIWAY	3/1/2022	3	99	Good	100	0	0
T02SU	01	7/1/2019	AC	TAXIWAY	3/1/2022	3	100	Good	100	0	0
T03SU	01	7/1/2019	AC	TAXIWAY	3/1/2022	3	100	Good	100	0	0
T04SU	01	7/1/2020	AC	TAXIWAY	3/1/2022	2	100	Good	100	0	0
T04SU	02	1/1/1970	AC	TAXIWAY	3/1/2022	52	70	Fair	56	44	0
T04SU	03	7/1/2020	AC	TAXIWAY	3/1/2022	2	100	Good	100	0	0
T04SU	04	7/1/2020	AC	TAXIWAY	3/1/2022	2	100	Good	100	0	0
T05SU	01	7/1/2019	AC	TAXIWAY	3/1/2022	3	100	Good	56	44	0
T06SU	01	7/1/2019	AC	TAXIWAY	3/1/2022	3	100	Good	100	0	0
T07SU	01	7/1/2019	AC	TAXIWAY	3/1/2022	3	100	Good	100	0	0
THANGARSU	01	1/1/1970	AC	TAXIWAY	3/1/2022	52	65	Fair	100	0	0
THANGARSU	02	1/1/1970	AC	TAXIWAY	3/1/2022	52	72	Satisfactory	100	0	0
THANGARSU	03	1/1/1970	AC	TAXIWAY	3/1/2022	52	71	Satisfactory	100	0	0
THANGARSU	04	1/1/1970	AC	TAXIWAY	3/1/2022	52	74	Satisfactory	100	0	0
THANGARSU	05	1/1/1970	AC	TAXIWAY	3/1/2022	52	75	Satisfactory	100	0	0

Abbreviations:
PCI = Pavement Condition Index, AC = Asphalt Concrete, PCC = Portland Cement Concrete
Note: Unable to inspect A01SU-05 due to obstruction

Table 4B - SUNRIVER AIRPORT COMPARISON OF PREVIOUS INSPECTION AND 2022 RESULTS

Branch ID	Section ID	Surface Type[1]	Approximate Area, square feet	LCD[2]	2017 Survey			2022 Survey		Age[3]	Δ PCI/yr[4]	Rate of Deterioration
					PCI	PCI Category	Insp. Date	PCI	PCI Category			
A01SU	01	AC	76,472	7/1/2020	64	Fair	6/17/2017	100	Good	-3	7.65	NONE
A01SU	02	PCC	20,575	7/1/2020	11	Serious	6/17/2017	100	Good	-3	18.91	NONE
A01SU	03	AC	349,900	7/1/2020	37	Very Poor	6/17/2017	100	Good	-3	13.38	NONE
A01SU	04	PCC	19,409	7/1/2020	11	Serious	6/17/2017	85	Satisfactory	-3	15.72	NONE
A01SU	05	PCC	20,625	7/1/2020	-	-	-	--	--	-	-	-
A01SU	06	PCC	1,868	7/1/2020	-	-	-	77	Satisfactory	-	-	-
A02SU	01	AC	7,016	7/1/2019	-	-	-	67	Fair	-	-	-
R18SU	01	AC	409,820	9/2/2009	83	Satisfactory	6/17/2017	57	Fair	8	-5.52	HIGH
T01SU	01	AC	76,490	7/1/2019	17	Serious	6/17/2017	100	Good	-2	17.63	NONE
T01SU	02	AC	141,040	7/1/2019	52	Poor	6/17/2017	99	Good	-2	9.99	NONE
T02SU	01	AC	11,900	7/1/2019	-	-	-	100	Good	-	-	-
T03SU	01	AC	14,230	7/1/2019	63	Fair	6/17/2017	100	Good	-2	7.86	NONE
T04SU	01	AC	6,361	7/1/2020	-	-	-	100	Good	-	-	-
T04SU	02	AC	4,135	1/1/1970	73	Satisfactory	6/17/2017	70	Fair	47	-0.64	NORMAL
T04SU	03	AC	3,980	7/1/2020	-	-	-	100	Good	-	-	-
T04SU	04	AC	4,734	7/1/2020	-	-	-	100	Good	-	-	-
T05SU	01	AC	13,690	7/1/2019	84	Satisfactory	6/17/2017	100	Good	-2	3.40	NONE
T06SU	01	AC	13,970	7/1/2019	-	-	-	100	Good	-	-	-
T07SU	01	AC	7,431	7/1/2019	-	-	-	100	Good	-	-	-
THANGARSU	01	AC	17,460	1/1/1970	72	Satisfactory	6/17/2017	65	Fair	47	-1.49	NORMAL
THANGARSU	02	AC	18,181	1/1/1970	74	Satisfactory	6/17/2017	72	Satisfactory	47	-0.42	NORMAL
THANGARSU	03	AC	15,584	1/1/1970	77	Satisfactory	6/17/2017	71	Satisfactory	47	-1.27	NORMAL
THANGARSU	04	AC	15,584	1/1/1970	82	Satisfactory	6/17/2017	74	Satisfactory	47	-1.70	NORMAL
THANGARSU	05	AC	9,229	1/1/1970	82	Satisfactory	6/17/2017	75	Satisfactory	47	-1.49	NORMAL

Abbreviations:

[1] AC = Asphalt Concrete, PCC = Portland Cement Concrete

[2] LCD = Last construction date. The date of the last major pavement rehabilitation (e.g. AC overlay)

[3] Age = Pavement age in years at the time of the PCI survey in 2017

[4] Δ PCI/yr = Change in PCI points per year between 2017 survey and 2022 survey

Note: Unable to inspect A01SU-05 due to obstruction

GRI

APPENDIX C

Future Pavement Condition Analysis

APPENDIX C

FUTURE PAVEMENT CONDITION ANALYSIS

C.1 METHODOLOGY

In addition to assessing the current condition of a pavement, it is very important from a planning standpoint to be able to predict with reasonable accuracy its future condition. In a pavement management plan (PMP), this is done with the aid of a prediction model. When an APMS is initially implemented, the default models are typically used to predict the future condition of a pavement. However, after PCI surveys are completed, the historical data are then used to refine the models, so they better represent the deterioration of a particular class of pavement based on local climatic conditions, loading, material sources, construction procedures, etc. The importance of accurate prediction models is part of the reason it is essential to conduct periodic, routine surveys in order to track the rate of deterioration.

In PAVER, the pavement deterioration curves are developed based on the "family" model procedure. A pavement "family" is defined as a group of pavements with similar deterioration characteristics. The procedure for developing the prediction models is:

1. Define the pavement families.
2. Review the data.
3. Conduct a data outlier analysis.
4. Model the data.

C.2 PREDICTION MODELS

We developed separate condition prediction models for each pavement "family" at Sunriver Airport. The delineation is based on branch use, surface type, section rank, and structural design life. We use three distinct models for the following "families" of pavements at Sunriver Airport. For each model, we reviewed the data in order to filter out any suspicious or inaccurate data or any data that fall outside boundary values set by PAVER. After outliers are removed and the data are checked for accuracy and reasonableness, the PAVER program calculates a best-fit curve using a fourth-order, polynomial-constrained, least-squares analysis procedure. This best-fit curve for each family is used in the analysis to predict the average behavior of all sections within each "family." Our condition prediction models for each "family" are provided on Figures 1C through 4C below.

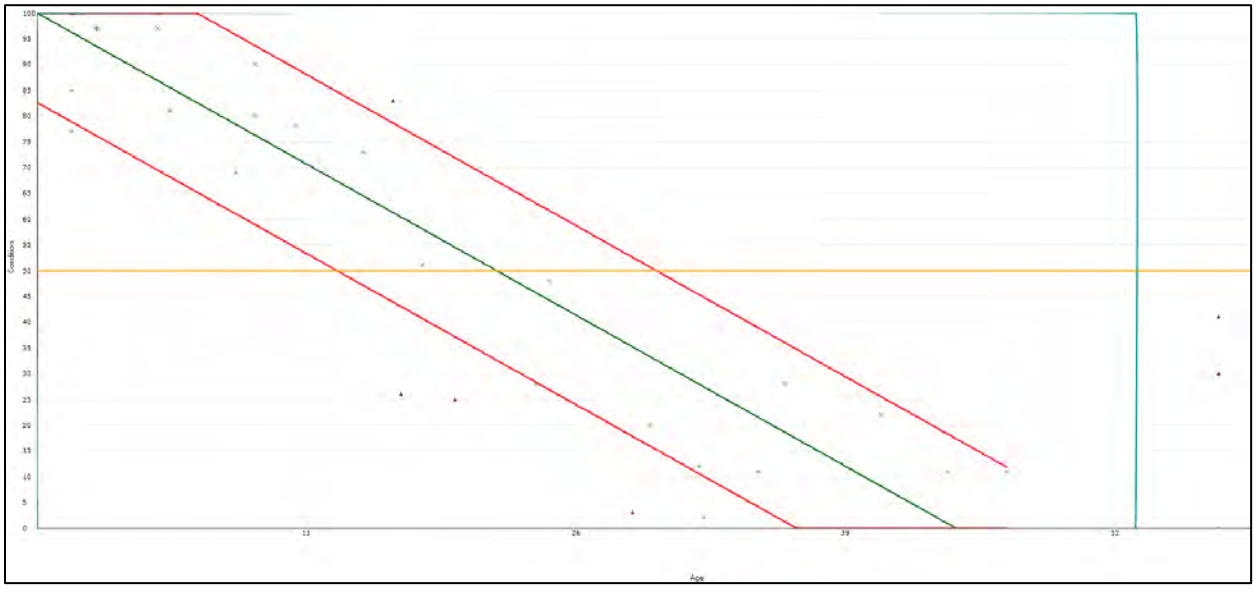


Figure 1C – CONDITION PREDICTION MODEL FOR CENTRAL CATEGORY 4/5 PCC (ALL USES)

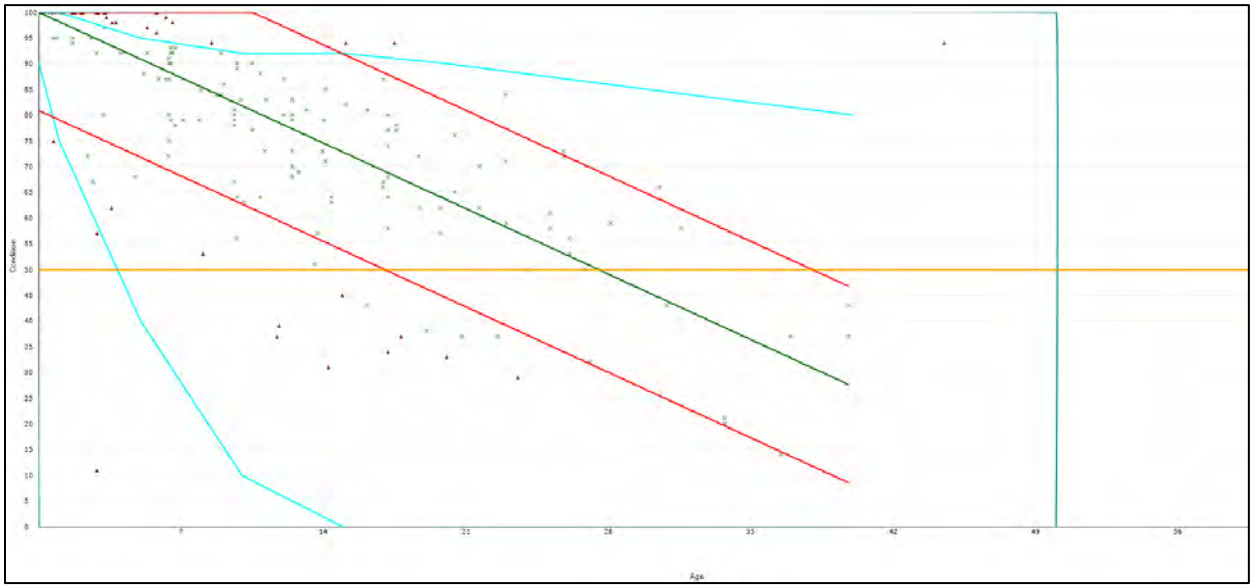


Figure 2C - CONDITION PREDICTION MODEL FOR CENTRAL CATEGORY 4 AC AND AAC APRONS

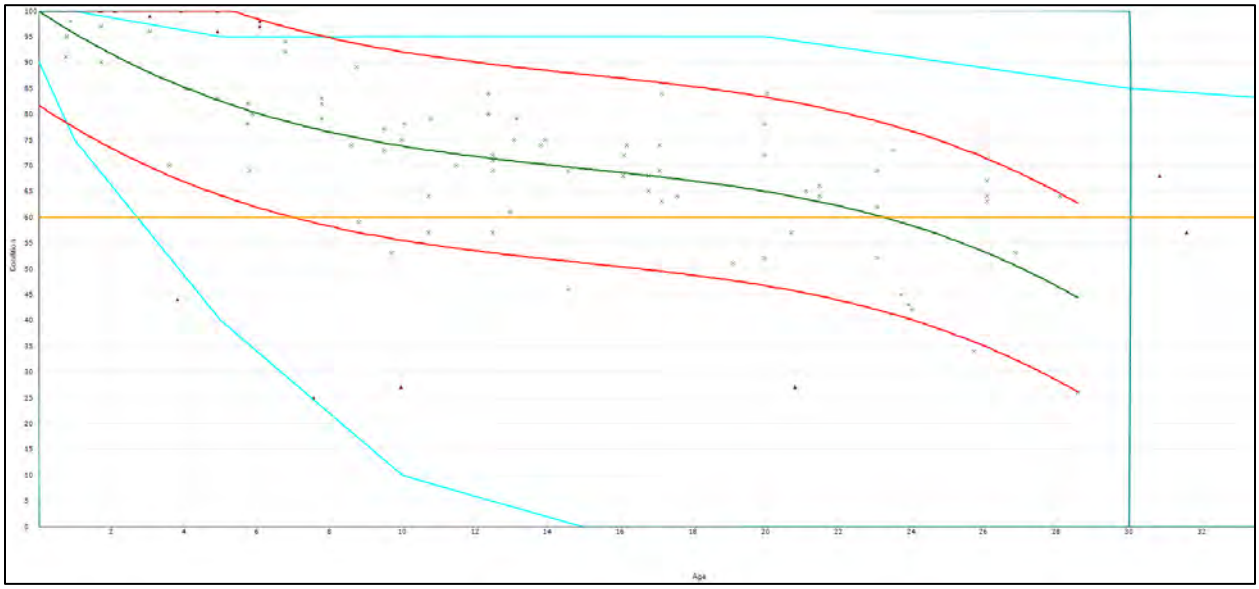


Figure 3C - CONDITION PREDICTION MODEL FOR CENTRAL CATEGORY 4 AC AND AAC RUNWAYS

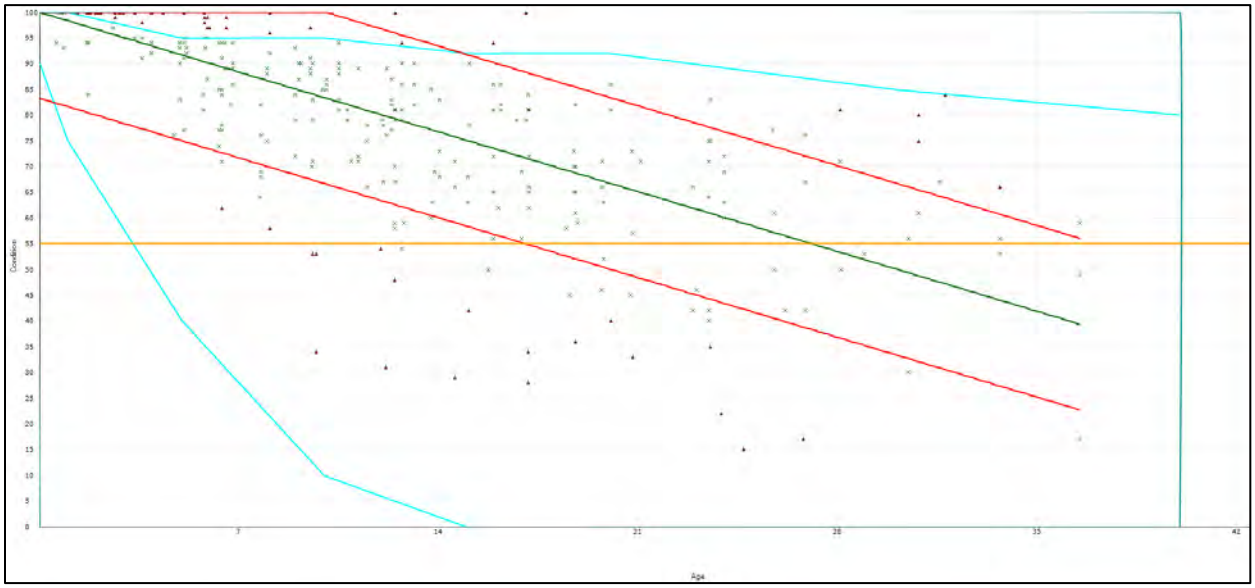


Figure 4C - CONDITION PREDICTION MODEL FOR CENTRAL CATEGORY 4 AC AND AAC TAXIWAYS

C.3 CRITICAL PCI

Each of the condition-prediction models have an assigned critical PCI. The critical PCI is the point at which the pavement condition begins to deteriorate more quickly over time. As the condition deteriorates to a worse state, major M&R is triggered because the cost to apply localized M&R increases significantly. Pavement sections with PCI above the critical value are given a higher priority for funding during budget analysis in order to

prevent them from deteriorating to the point where more costly rehabilitation is necessary. We used the following critical PCI values at Sunriver Airport:

- Runways – 60
- Taxiways/Taxilanes – 55
- Aprons – 50

C.4 FUTURE CONDITION ANALYSIS

As previously discussed, the projected condition of each pavement section was determined for 5-year and 10-year periods. The projected pavement conditions in 5 years and 10 years for each pavement section at Sunriver Airport, along with the conditions at the previous inspection, are listed in Table 1C.

C.5 FUNCTIONAL REMAINING LIFE

As mentioned above, functional remaining life is the practical amount of time a pavement is in service before requiring rehabilitation, as estimated based solely on visual condition. This is not to be confused with structural remaining life, which requires analysis of the structural capacity of a pavement.

We calculated two forms of functional remaining life based on the current visual condition surveys of the pavement at Sunriver Airport, the time until rehabilitation, and the time until the pavement is no longer operational due to high foreign object debris potential and increased safety concerns for trafficking aircraft (PCI less than 40). The results of the functional life analysis are provided in Table 2C.

Table 1C - PAST, PRESENT, AND FUTURE PCI

BranchID	SectionID	Past Inspection PCI 2017	Current PCI 2022	Predicted Future PCI 2027	Predicted Future PCI 2032
A01SU	01	64	100	91	82
A01SU	02	11	100	89	77
A01SU	03	37	100	91	82
A01SU	04	11	85	74	62
A01SU	05	-	-	-	-
A01SU	06	-	77	66	54
A02SU	01	-	67	58	49
R18SU	01	83	57	41	24
T01SU	01	17	100	92	83
T01SU	02	52	99	91	82
T02SU	01	-	100	92	83
T03SU	01	63	100	92	83
T04SU	01	-	100	92	83
T04SU	02	73	70	62	53
T04SU	03	-	100	92	83
T04SU	04	-	100	92	83
T05SU	01	84	100	92	83
T06SU	01	-	100	92	83
T07SU	01	-	100	92	83
THANGARSU	01	72	65	57	48
THANGARSU	02	74	72	64	55
THANGARSU	03	77	71	63	54
THANGARSU	04	82	74	66	57
THANGARSU	05	82	75	67	58

Abbreviations:
PCI = Pavement Condition Index

Note: Unable to inspect A01SU-05 due to obstruction

Table 2C - SUNRIVER AIRPORT FUNCTIONAL REMAINING LIFE ANALYSIS

Branch ID	Section ID	Surface Type	Current PCI	Years to Major M&R	Major M&R Trigger PCI[1]	Years to End of Functional Service
A01SU	01	AC	100	> 20	50	> 20
A01SU	02	PCC	100	> 20	50	> 20
A01SU	03	AC	100	> 20	50	> 20
A01SU	04	PCC	85	11 - 15	50	> 20
A01SU	05	PCC	--	--	50	--
A01SU	06	PCC	77	11 - 15	50	16 - 20
A02SU	01	AC	67	6 - 10	50	11 - 15
R18SU	01	AC	57	0 - 5	60	0 - 5
T01SU	01	AC	100	> 20	55	> 20
T01SU	02	AC	99	> 20	55	> 20
T02SU	01	AC	100	> 20	55	> 20
T03SU	01	AC	100	> 20	55	> 20
T04SU	01	AC	100	> 20	55	> 20
T04SU	02	AC	70	6 - 10	55	16 - 20
T04SU	03	AC	100	> 20	55	> 20
T04SU	04	AC	100	> 20	55	> 20
T05SU	01	AC	100	> 20	55	> 20
T06SU	01	AC	100	> 20	55	> 20
T07SU	01	AC	100	> 20	55	> 20
THANGARSU	01	AC	65	6 - 10	55	11 - 15
THANGARSU	02	AC	72	6 - 10	55	> 20
THANGARSU	03	AC	71	6 - 10	55	16 - 20
THANGARSU	04	AC	74	11 - 15	55	> 20
THANGARSU	05	AC	75	11 - 15	55	> 20

Abbreviations:
M&R = Maintenacne and Rehabilitation, AC = Asphalt Concrete, PCC = Portland Cement Concrete
[1] Major M&R Trigger PCI = Critical PCI

Note: Unable to inspect A01SU-05 due to obstruction

APPENDIX D

Unit Cost Data and Maintenance and Rehabilitation Plan

APPENDIX D

UNIT COST DATA AND MAINTENANCE AND REHABILITATION PLAN

D.1 ANALYSIS METHODOLOGY

We evaluated the M&R needs, as determined from the PAVER analysis results, in order to develop project recommendations for the next five years. The purpose of this analysis is to determine the M&R needs of the Sunriver Airport pavement network condition over time. We used PAVER v7 software to develop network-level project recommendations for the next five years.

The PAVER M&R Work Planning Module identifies when and where M&R is required and how much it will cost. M&R plans can be developed either by assuming an annual budget or by identifying specific constraints, such as a condition goal to determine the budget required to meet the goal. The M&R work planning analysis was based on a five-year period beginning on August 1, 2023. A backlog elimination analysis scenario was selected to generate a list of global maintenance and rehabilitation projects in order to optimize the allocation of capital and establish preservation-based project recommendations. The repair strategies considered for pavement sections in our analysis are as follows:

- Reconstruction – Considered for pavements with a PCI less than 40.
- Flexible Overlay – Considered for pavements between 40 PCI and the critical PCI, and for pavements exhibiting significant load-related distresses.
- Global Maintenance – Treatments (fog seal, slurry seal, thin AC overlay) applied to an entire pavement section with the intent of slowing the rate of deterioration.
- Localized Maintenance – Maintenance performed on a routine basis such as crack sealing, wide crack repair, and patching.

It should be noted that the five-year list of recommended projects only includes the highest-cost maintenance items and does not include routine localized maintenance (e.g., crack sealing) work that should also be conducted in addition to and concurrently with the five-year work plan.

D.1.1 Pavement Rank and Use Prioritization

Pavement sections are assigned a rank to establish their relative importance in the overall pavement network, which is most commonly defined by their use (e.g., Taxiway, Apron, Runway). The PAVER analysis uses the combination of the section rank and the branch use

to define the priority of each section during the M&R analysis. Table 1D displays the branch use and section rank prioritization schema we used for analysis.

Table 1D: M&R WORK PRIORITY BY BRANCH USE AND SECTION RANK

Branch Use	Section Rank		
	Primary	Secondary	Tertiary
RUNWAY	1	3	6
TAXIWAY	2	5	8
APRON	4	7	9

D.2 MAINTENANCE POLICIES AND UNIT COSTS

The distress-maintenance policies are policies that determine what type of work should be applied to a specific distress type and severity. For example, on an AC pavement, a medium-severity longitudinal/transverse crack would be repaired by crack sealing. Policies for all the distress types and severities are established by ASTM D5340.

Although our work scope does not include budget analysis, we did assign construction costs to the maintenance work so that PAVER would allocate M&R projects that were approximately equal in cost for each year of the five-year period. The anticipated cost of performing M&R is based on cost tables that relate M&R work type cost to PCI. We reviewed the unit costs from the 2017 report and updated them by reviewing the bid tabulations for recent projects within the vicinity of Sunriver Airport and information provided by the project team. The costs for reconstruction are based on the existing pavement sections present within each branch use at Sunriver Airport. The costs represent the fully-loaded costs and include aspects of the project such as administration, contingencies, mobilization, and striping. The cost tables used in the analysis are presented in Table 2D below.

Table 2D: SUNRIVER AIRPORT UNIT COST DATA

Type of M&R	Work Type	Unit Cost	Work Unit
Major M&R	Complete Reconstruction with AC	$11.10	Sq Ft
	Cold Mill and Overlay – 3 Inches Thick	$4.90	Sq Ft
Global M&R	Surface Treatment - Slurry Seal	$0.33	Sq Ft
	Surface Treatment - Fog Seal	$0.20	Sq Ft
Localized Preventive M&R	Crack Sealing - AC	$2.00	Ft
	Crack Sealing - PCC	$15.00	Ft
	Crack Sealing – Wide Cracks	$33.00	Ft
	AC Patching – Full Depth	$50.00	Sq Ft
	PCC Patching – Full Depth	$100.00	Sq Ft

D.3 RECOMMENDED LOCALIZED MAINTENANCE

In order to properly maintain aging pavements, localized M&R activities such as crack sealing and patching should be performed on a routine basis. A list of recommended localized maintenance activities is provided in Table 3D of this appendix.

D.4 RECOMMENDED GLOBAL MAINTENANCE AND REHABILITATION PROJECTS

Global maintenance and rehabilitation projects refer to activities such as slurry seal and thin AC overlays, as well as thick AC overlays and reconstruction. A list of recommended global M&R activities is provided in Table 4D of this appendix.

Table 3D - SUNRIVER AIRPORT NETWORK MAINTENANCE REPORT

Network	Branch ID	Section ID	Distress	Severity	Action	Work Quantity	Unit	Unit Cost	Work Cost	Section Total
Sunriver	A01SU	04	Linear Cracking	Low	Crack Sealing - PCC	130	Ft	$15.00	$1,949	$1,949
Sunriver	A01SU	06	Linear Cracking	Low	Crack Sealing - PCC	10	Ft	$15.00	$146	$293
Sunriver	A01SU	06	Linear Cracking	Medium	Crack Sealing - PCC	10	Ft	$15.00	$146	
Sunriver	A02SU	01	Alligator Cracking	Medium	Patching - AC Deep	42	SqFt	$50.00	$2,100	$2,824
Sunriver	A02SU	01	Long. & Trans. Cracking	Low	Crack Sealing - AC	185	Ft	$2.00	$370	
Sunriver	A02SU	01	Long. & Trans. Cracking	Medium	Crack Sealing - AC	177	Ft	$2.00	$354	
Sunriver	R18SU	01	Alligator Cracking	Low	Crack Sealing - AC	176	Ft	$2.00	$353	$374,376
Sunriver	R18SU	01	Alligator Cracking	Medium	Patching - AC Deep	6,763	SqFt	$50.00	$338,129	
Sunriver	R18SU	01	Long. & Trans. Cracking	Low	Crack Sealing - AC	5,100	Ft	$2.00	$10,200	
Sunriver	R18SU	01	Long. & Trans. Cracking	Medium	Crack Sealing - AC	12,847	Ft	$2.00	$25,694	
Sunriver	T01SU	02	Long. & Trans. Cracking	Low	Crack Sealing - AC	104	Ft	$2.00	$208	$208
Sunriver	T04SU	02	Alligator Cracking	Low	Crack Sealing - AC	20	Ft	$2.00	$41	$279
Sunriver	T04SU	02	Long. & Trans. Cracking	Medium	Crack Sealing - AC	119	Ft	$2.00	$238	
Sunriver	THANGARSU	01	Long. & Trans. Cracking	Low	Crack Sealing - AC	79	Ft	$2.00	$158	$1,899
Sunriver	THANGARSU	01	Long. & Trans. Cracking	Medium	Crack Sealing - AC	870	Ft	$2.00	$1,741	
Sunriver	THANGARSU	02	Long. & Trans. Cracking	Low	Crack Sealing - AC	18	Ft	$2.00	$36	$1,029
Sunriver	THANGARSU	02	Long. & Trans. Cracking	Medium	Crack Sealing - AC	496	Ft	$2.00	$992	
Sunriver	THANGARSU	03	Long. & Trans. Cracking	Low	Crack Sealing - AC	39	Ft	$2.00	$78	$878
Sunriver	THANGARSU	03	Long. & Trans. Cracking	Medium	Crack Sealing - AC	400	Ft	$2.00	$800	
Sunriver	THANGARSU	04	Long. & Trans. Cracking	Medium	Crack Sealing - AC	362	Ft	$2.00	$725	$725
Sunriver	THANGARSU	05	Long. & Trans. Cracking	Low	Crack Sealing - AC	13	Ft	$2.00	$26	$449
Sunriver	THANGARSU	05	Long. & Trans. Cracking	Medium	Crack Sealing - AC	211	Ft	$2.00	$423	

Long. = Longitudinal; Trans. = Transverse; AC = Asphalt Concrete; PCC = Portland Cement Concrete; Ft = Feet; SqFt = Square Feet

Table 4D - FIVE-YEAR GLOBAL MAINTENANCE AND REHABILITATION PLAN

Action Year	Branch ID	Section ID	Branch Use	Surface Type	Current PCI	Action	Area, square feet	Unit Cost per square foot	Total Cost
2024	R18SU	01	RUNWAY	AC	57	Overlay	409,820	$4.90	$2,007,998
2025	T04SU	02	TAXIWAY	AC	70	Slurry Seal	4,135	$0.33	$1,365
	THANGARSU	01	TAXIWAY	AC	65	Slurry Seal	17,460	$0.33	$5,762
	THANGARSU	02	TAXIWAY	AC	72	Slurry Seal	18,181	$0.33	$6,000
	THANGARSU	03	TAXIWAY	AC	71	Slurry Seal	15,584	$0.33	$5,143
	THANGARSU	04	TAXIWAY	AC	74	Slurry Seal	15,584	$0.33	$5,143
	THANGARSU	05	TAXIWAY	AC	75	Slurry Seal	9,229	$0.33	$3,046
2026	A02SU	01	APRON	AC	67	Fog Seal	7,016	$0.20	$1,403

Abbreviations:
PCI = Pavement Condition Index, AC = Asphalt Concrete

Cost Summary	
2023 Total Project Cost	$0
2024 Total Project Cost	$2,007,998
2025 Total Project Cost	$26,457
2026 Total Project Cost	$1,403
2027 Total Project Cost	$0
Total 5-Year Project Cost	**$2,035,858**

APPENDIX E

Re-Inspection Report

Re-Inspection Report

ODA_WOC3_9-1-2022_PostBendAnalysis
Generated Date 9/30/2022

Network: Sunriver **Name:** Sunriver

Branch: A01SU **Name:** Apron 01 Sunriver **Use:** APRON **Area:** 488,849 SqFt

Section: 01 of 6 **From:** T04SU-01 **To:** A01SU-03 **Last Const.:** 7/1/2020

Surface: AC **Family:** 2022_Central_Cat4/5_Apron_AC/AAC **Zone:** S21 **Category:** L **Rank:** P

Area: 76,472 SqFt **Length:** 625 Ft **Width:** 175 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments: Twin/Jet Ramp

Work Date: 9/1/1985	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 9/2/1985	**Work Type:** New Construction - AC	**Code:** NC-AC	**Is Major M&R:** True
Work Date: 9/1/1997	**Work Type:** Overlay - AC Thin	**Code:** OL-AT	**Is Major M&R:** True
Work Date: 9/1/1997	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/1/1999	**Work Type:** Surface Treatment - Seal Coat (Global MR)	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 9/1/2003	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/1/2012	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/2/2012	**Work Type:** Patching - AC Deep	**Code:** PA-AD	**Is Major M&R:** False
Work Date: 9/1/2015	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/2/2015	**Work Type:** Patching - AC Deep	**Code:** PA-AD	**Is Major M&R:** False
Work Date: 7/1/2020	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True
Work Date: 7/1/2020	**Work Type:** Subbase - Aggregate	**Code:** SB-AG	**Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 17 **Surveyed:** 5

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 4038.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 05 **Type:** R **Area:** 4156.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 11 **Type:** R **Area:** 4590.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 13 **Type:** R **Area:** 4740.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 16 **Type:** R **Area:** 5154.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: A01SU **Name:** Apron 01 Sunriver **Use:** APRON **Area:** 488,849 SqFt

Section: 02 of 6 **From:** A01SU-01 **To:** A01SU-01 **Last Const.:** 7/1/2020

Surface: PCC **Family:** 2022_Central_Cat4/5_All Uses_PCC **Zone:** S21 **Category:** L **Rank:** P

Area: 20,575 SqFt **Length:** 300 Ft **Width:** 67 Ft

Slabs: 120 **Slab Length:** 12 Ft **Slab Width:** 12 Ft **Joint Length:** 2,916 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 9/1/1970	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 9/2/1970	**Work Type:** Surface Course - Double Bitum.	**Code:** SU-DB	**Is Major M&R:** True
Work Date: 9/3/1970	**Work Type:** Surface Treatment - Seal Coat (Global MR)	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 9/1/1997	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 7/1/2020	**Work Type:** Complete Reconstruction - PCC	**Code:** CR-PC	**Is Major M&R:** True
Work Date: 7/1/2020	**Work Type:** Subbase - Aggregate	**Code:** SB-AG	**Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 6 **Surveyed:** 4

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 20.00 Slabs **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 02 **Type:** R **Area:** 20.00 Slabs **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 04 **Type:** R **Area:** 20.00 Slabs **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 05 **Type:** R **Area:** 20.00 Slabs **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: A01SU **Name:** Apron 01 Sunriver **Use:** APRON **Area:** 488,849 SqFt

Section: 03 of 6 **From:** A01SU-01 **To:** T01SU-02 **Last Const.:** 7/1/2020

Surface: AC **Family:** 2022_Central_Cat4/5_Apron_AC/AAC **Zone:** S21 **Category:** L **Rank:** P

Area: 349,900 SqFt **Length:** 925 Ft **Width:** 400 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 9/1/1970	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 9/2/1970	**Work Type:** Surface Course - Double Bitum.	**Code:** SU-DB	**Is Major M&R:** True
Work Date: 9/1/1999	**Work Type:** Overlay - AC Thin	**Code:** OL-AT	**Is Major M&R:** True
Work Date: 9/1/2003	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 7/1/2020	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True
Work Date: 7/1/2020	**Work Type:** Subbase - Aggregate	**Code:** SB-AG	**Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 73 **Surveyed:** 7

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 5000.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 16 **Type:** R **Area:** 5000.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 20 **Type:** R **Area:** 5000.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 30 **Type:** R **Area:** 5000.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 48 **Type:** R **Area:** 5000.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 60 **Type:** R **Area:** 5357.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 68 **Type:** R **Area:** 6019.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: A01SU **Name:** Apron 01 Sunriver **Use:** APRON **Area:** 488,849 SqFt

Section: 04 of 6 **From:** A01SU-03 **To:** W. Edge **Last Const.:** 7/1/2020

Surface: PCC **Family:** 2022_Central_Cat4/5_All Uses_PCC **Zone:** S21 **Category:** L **Rank:** P

Area: 19,409 SqFt **Length:** 130 Ft **Width:** 150 Ft

Slabs: 110 **Slab Length:** 14 Ft **Slab Width:** 12 Ft **Joint Length:** 2,613 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 9/1/1970	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 9/2/1970	**Work Type:** Surface Course - Double Bitum.	**Code:** SU-DB	**Is Major M&R:** True
Work Date: 9/1/1982	**Work Type:** Overlay - AC Thin	**Code:** OL-AT	**Is Major M&R:** True
Work Date: 9/1/1997	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/1/1999	**Work Type:** Surface Treatment - Seal Coat (Global MR)	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 7/1/2020	**Work Type:** Complete Reconstruction - PCC	**Code:** CR-PC	**Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 6 **Surveyed:** 4

Conditions: **PCI:** 85

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 20.00 Slabs **PCI:** 76

Sample Comments:

74	JOINT SPALL	L	3.00	Slabs
63	LINEAR CR	L	2.00	Slabs
71	FAULTING	L	4.00	Slabs

Sample Number: 02 **Type:** R **Area:** 20.00 Slabs **PCI:** 89

Sample Comments:

63	LINEAR CR	L	1.00	Slabs
75	CORNER SPALL	L	1.00	Slabs
74	JOINT SPALL	L	2.00	Slabs
73	SHRINKAGE CR	N	1.00	Slabs

Sample Number: 03 **Type:** R **Area:** 20.00 Slabs **PCI:** 81

Sample Comments:

74	JOINT SPALL	L	2.00	Slabs
63	LINEAR CR	L	3.00	Slabs
71	FAULTING	L	1.00	Slabs

Sample Number: 06 **Type:** R **Area:** 20.00 Slabs **PCI:** 94

Sample Comments:

63	LINEAR CR	L	1.00	Slabs
74	JOINT SPALL	L	1.00	Slabs

Network: Sunriver **Name:** Sunriver

Branch: A01SU **Name:** Apron 01 Sunriver **Use:** APRON **Area:** 488,849 SqFt

Section: 05 of 6 **From:** A01SU-04 **To:** East edge of PCC **Last Const.:** 7/1/2020

Surface: PCC **Family:** DEFAULT **Zone:** **Category:** **Rank:** P

Area: 20,625 SqFt **Length:** 275 Ft **Width:** 75 Ft

Slabs: 127 **Slab Length:** 12 Ft **Slab Width:** 13 Ft **Joint Length:** 2,887 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2020 **Work Type:** Subbase - Aggregate **Code:** SB-AG **Is Major M&R:** False

Work Date: 7/1/2020 **Work Type:** New Construction - PCC **Code:** NC-PC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 5 **Surveyed:** 3

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 20.00 Slabs **PCI:** 100

Sample Comments: Could not survey due to snow covering PCC slabs

<No Distress>

Sample Number: 03 **Type:** R **Area:** 20.00 Slabs **PCI:** 100

Sample Comments: Could not survey due to snow covering PCC slabs

<No Distress>

Sample Number: 04 **Type:** R **Area:** 20.00 Slabs **PCI:** 100

Sample Comments: Could not survey due to snow covering PCC slabs

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: A01SU **Name:** Apron 01 Sunriver **Use:** APRON **Area:** 488,849 SqFt

Section: 06 of 6 **From:** A01SU-04 **To:** End **Last Const.:** 7/1/2020

Surface: PCC **Family:** 2022_Central_Cat4/5_All Uses_PCC **Zone:** **Category:** **Rank:** P

Area: 1,868 SqFt **Length:** 90 Ft **Width:** 20 Ft

Slabs: 20 **Slab Length:** 10 Ft **Slab Width:** 9 Ft **Joint Length:** 261 Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2020 **Work Type:** New Construction - Initial **Code:** NU-IN **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 77

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 20.00 Slabs **PCI:** 77

Sample Comments:

63	LINEAR CR	M	1.00	Slabs
74	JOINT SPALL	L	1.00	Slabs
63	LINEAR CR	L	1.00	Slabs
75	CORNER SPALL	L	2.00	Slabs
73	SHRINKAGE CR	N	1.00	Slabs

Network: Sunriver **Name:** Sunriver

Branch: A02SU **Name:** Apron 02 Sunriver **Use:** APRON **Area:** 7,016 SqFt

Section: 01 of 1 **From:** T07SU-01 **To:** R18SU-01 **Last Const.:** 7/1/2019

Surface: AC **Family:** 2022_Central_Cat4/5_Apron_AC/AAC **Zone:** **Category:** **Rank:** P

Area: 7,016 SqFt **Length:** 100 Ft **Width:** 70 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2019 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 7/1/2019 **Work Type:** Subbase - Crushed Aggregate **Code:** SU-CA **Is Major M&R:** False

Work Date: 7/1/2019 **Work Type:** Base Course - Aggregate **Code:** BA-AG **Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 2 **Surveyed:** 2

Conditions: **PCI:** 67

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 5126.00 SqFt **PCI:** 64

Sample Comments:

48	L & T CR	M	147.00	Ft
57	WEATHERING	L	5126.00	SqFt
48	L & T CR	L	140.00	Ft
41	ALLIGATOR CR	M	20.00	SqFt

Sample Number: 02 **Type:** R **Area:** 1890.00 SqFt **PCI:** 76

Sample Comments:

57	WEATHERING	L	945.00	SqFt
48	L & T CR	L	45.00	Ft
48	L & T CR	M	30.00	Ft

Network: Sunriver **Name:** Sunriver

Branch: R18SU **Name:** Runway 18/36 Sunriver **Use:** RUNWAY **Area:** 409,820 SqFt

Section: 01 of 1 **From:** R36 End **To:** R18SU-02 **Last Const.:** 9/2/2009

Surface: AC **Family:** 2022_Central_Cat4/5_RW_AC/AAC **Zone:** S21 **Category:** L **Rank:** P

Area: 409,820 SqFt **Length:** 5,460 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 9/1/1970	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 9/1/1985	**Work Type:** Overlay - AC Thin	**Code:** OL-AT	**Is Major M&R:** True
Work Date: 9/1/2009	**Work Type:** Base Course - Pulverized AC	**Code:** BA-PA	**Is Major M&R:** False
Work Date: 9/2/2009	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True
Work Date: 9/1/2012	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/1/2015	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 73 **Surveyed:** 6

Conditions: **PCI:** 57

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 5625.00 SqFt **PCI:** 53

Sample Comments: Created by Inspection Schedule

41	ALLIGATOR CR	M	30.00	SqFt
48	L & T CR	M	212.00	Ft
48	L & T CR	L	32.00	Ft
41	ALLIGATOR CR	M	90.00	SqFt
57	WEATHERING	L	5625.00	SqFt

Sample Number: 24 **Type:** R **Area:** 5625.00 SqFt **PCI:** 70

Sample Comments: Created by Inspection Schedule

48	L & T CR	M	48.00	Ft
50	PATCHING	L	58.00	SqFt
57	WEATHERING	L	5625.00	SqFt
48	L & T CR	L	8.00	Ft
48	L & T CR	M	75.00	Ft
48	L & T CR	M	40.00	Ft
48	L & T CR	L	30.00	Ft

Sample Number: 34 **Type:** R **Area:** 5625.00 SqFt **PCI:** 69

Sample Comments: Created by Inspection Schedule

57	WEATHERING	L	5625.00	SqFt
48	L & T CR	M	118.00	Ft
48	L & T CR	L	38.00	Ft
48	L & T CR	L	30.00	Ft
48	L & T CR	M	105.00	Ft

Sample Number: 46 **Type:** R **Area:** 5625.00 SqFt **PCI:** 42

Sample Comments: Created by Inspection Schedule

48	L & T CR	L	75.00	Ft
41	ALLIGATOR CR	L	40.00	SqFt
57	WEATHERING	L	5625.00	SqFt
48	L & T CR	M	50.00	Ft
41	ALLIGATOR CR	M	160.00	SqFt
48	L & T CR	L	12.00	Ft
48	L & T CR	M	75.00	Ft

Sample Number: 58 **Type:** R **Area:** 5625.00 SqFt **PCI:** 35

Sample Comments: Created by Inspection Schedule

48	L & T CR	L	120.00	Ft

50	PATCHING	L	75.00	SqFt
48	L & T CR	L	75.00	Ft
48	L & T CR	M	150.00	Ft
57	WEATHERING	L	5625.00	SqFt
41	ALLIGATOR CR	M	250.00	SqFt

Sample Number: 68 **Type:** R **Area:** 5625.00 SqFt **PCI:** 75

Sample Comments: Created by Inspection Schedule

50	PATCHING	L	42.00	SqFt
48	L & T CR	M	75.00	Ft
48	L & T CR	M	95.00	Ft
50	PATCHING	L	70.00	SqFt
48	L & T CR	M	15.00	Ft

Network: Sunriver **Name:** Sunriver

Branch: T01SU **Name:** Taxiway 01 Sunriver **Use:** TAXIWAY **Area:** 217,530 SqFt

Section: 01 of 2 **From:** R36 End **To:** T03SU-01 **Last Const.:** 7/1/2019

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** P

Area: 76,490 SqFt **Length:** 1,880 Ft **Width:** 35 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 9/1/1970	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 9/2/1970	**Work Type:** Surface Course - Double Bitum.	**Code:** SU-DB	**Is Major M&R:** True
Work Date: 9/1/1990	**Work Type:** Overlay - AC Thin	**Code:** OL-AT	**Is Major M&R:** True
Work Date: 9/1/1997	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/1/2008	**Work Type:** Crack Seal - Wide Cracks	**Code:** CS-WD	**Is Major M&R:** False
Work Date: 9/2/2008	**Work Type:** Patching - AC Deep	**Code:** PA-AD	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True
Work Date: 7/1/2019	**Work Type:** Subbase - Aggregate	**Code:** SB-AG	**Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 14 **Surveyed:** 5

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 6378.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 04 **Type:** R **Area:** 5270.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 07 **Type:** R **Area:** 5260.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 10 **Type:** R **Area:** 5255.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 13 **Type:** R **Area:** 5852.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: T01SU **Name:** Taxiway 01 Sunriver **Use:** TAXIWAY **Area:** 217,530 SqFt

Section: 02 of 2 **From:** T03SU-01 **To:** R18 End **Last Const.:** 7/1/2019

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** P

Area: 141,040 SqFt **Length:** 3,590 Ft **Width:** 35 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 9/1/1985	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 9/2/1985	**Work Type:** New Construction - AC	**Code:** NC-AC	**Is Major M&R:** True
Work Date: 9/1/1997	**Work Type:** Overlay - AC Thin	**Code:** OL-AT	**Is Major M&R:** True
Work Date: 9/1/1999	**Work Type:** Surface Treatment - Seal Coat (Global MR)	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 9/1/2003	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/1/2008	**Work Type:** Crack Seal - Wide Cracks	**Code:** CS-WD	**Is Major M&R:** False
Work Date: 9/2/2008	**Work Type:** Patching - AC Deep	**Code:** PA-AD	**Is Major M&R:** False
Work Date: 9/1/2012	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/1/2015	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/2/2015	**Work Type:** Patching - AC Deep	**Code:** PA-AD	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Subbase - Aggregate	**Code:** SB-AG	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 36 **Surveyed:** 5

Conditions: **PCI:** 99

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 3886.00 SqFt **PCI:** 96

Sample Comments:

48 L & T CR L 15.00 Ft

Sample Number: 13 **Type:** R **Area:** 3546.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 19 **Type:** R **Area:** 3525.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 25 **Type:** R **Area:** 5865.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 31 **Type:** R **Area:** 3525.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: T02SU **Name:** Taxiway 02 Sunriver **Use:** TAXIWAY **Area:** 11,900 SqFt

Section: 01 of 1 **From:** T01SU-01 **To:** R18SU-01 **Last Const.:** 7/1/2019

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** P

Area: 11,900 SqFt **Length:** 160 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 9/1/1970	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 9/2/1970	**Work Type:** Surface Course - Double Bitum.	**Code:** SU-DB	**Is Major M&R:** True
Work Date: 9/1/1997	**Work Type:** Overlay - AC Thin	**Code:** OL-AT	**Is Major M&R:** True
Work Date: 9/1/1999	**Work Type:** Surface Treatment - Seal Coat (Global MR)	**Code:** ST-SC	**Is Major M&R:** False
Work Date: 9/1/2008	**Work Type:** Crack Seal - Wide Cracks	**Code:** CS-WD	**Is Major M&R:** False
Work Date: 9/2/2008	**Work Type:** Patching - AC Deep	**Code:** PA-AD	**Is Major M&R:** False
Work Date: 9/1/2012	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/1/2015	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True
Work Date: 7/1/2019	**Work Type:** Subbase - Aggregate	**Code:** SB-AG	**Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 2 **Surveyed:** 2

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 5657.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 02 **Type:** R **Area:** 6243.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: T03SU **Name:** Taxiway 03 Sunriver **Use:** TAXIWAY **Area:** 14,230 SqFt

Section: 01 of 1 **From:** T01SU-02 **To:** R18SU-01 **Last Const.:** 7/1/2019

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** P

Area: 14,230 SqFt **Length:** 160 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 9/1/1970	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 9/2/1970	**Work Type:** Surface Course - Double Bitum.	**Code:** SU-DB	**Is Major M&R:** True
Work Date: 9/1/1985	**Work Type:** Overlay - AC Thin	**Code:** OL-AT	**Is Major M&R:** True
Work Date: 9/1/1997	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 9/1/2008	**Work Type:** Crack Seal - Wide Cracks	**Code:** CS-WD	**Is Major M&R:** False
Work Date: 9/2/2008	**Work Type:** Patching - AC Deep	**Code:** PA-AD	**Is Major M&R:** False
Work Date: 9/1/2012	**Work Type:** Crack Sealing - AC	**Code:** CS-AC	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Complete Reconstruction - AC	**Code:** CR-AC	**Is Major M&R:** True
Work Date: 7/1/2019	**Work Type:** Subbase - Aggregate	**Code:** SB-AG	**Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 4 **Surveyed:** 3

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 3436.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 02 **Type:** R **Area:** 3565.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 03 **Type:** R **Area:** 3703.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: T04SU **Name:** Taxiway 04 Sunriver **Use:** TAXIWAY **Area:** 19,210 SqFt

Section: 01 of 4 **From:** T04SU-02 **To:** T01SU-02 **Last Const.:** 7/1/2020

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** **Category:** **Rank:** S

Area: 6,361 SqFt **Length:** 190 Ft **Width:** 35 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2020 **Work Type:** Subbase - Aggregate **Code:** SB-AG **Is Major M&R:** False

Work Date: 7/1/2020 **Work Type:** Complete Reconstruction - AC **Code:** CR-AC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 6361.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: T04SU **Name:** Taxiway 04 Sunriver **Use:** TAXIWAY **Area:** 19,210 SqFt

Section: 04 of 4 **From:** T04SU-01 **To:** End **Last Const.:** 7/1/2020

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** **Category:** **Rank:** S

Area: 4,734 SqFt **Length:** 135 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2020 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 7/1/2020 **Work Type:** Subbase - Aggregate **Code:** SB-AG **Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 4734.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: T04SU **Name:** Taxiway 04 Sunriver **Use:** TAXIWAY **Area:** 19,210 SqFt

Section: 03 of 4 **From:** T04SU-01 **To:** End **Last Const.:** 7/1/2020

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** **Category:** **Rank:** S

Area: 3,980 SqFt **Length:** 115 Ft **Width:** 25 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2020 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Work Date: 7/1/2020 **Work Type:** Subbase - Aggregate **Code:** SB-AG **Is Major M&R:** False

Last Insp. Date: 3/1/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 3980.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: T04SU **Name:** Taxiway 04 Sunriver **Use:** TAXIWAY **Area:** 19,210 SqFt

Section: 02 of 4 **From:** T04SU-01 **To:** SU-01 **Last Const.:** 1/1/1970

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** S

Area: 4,135 SqFt **Length:** 120 Ft **Width:** 35 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1970 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 1 **Surveyed:** 1

Conditions: **PCI:** 70

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 4135.00 SqFt **PCI:** 70

Sample Comments:

41	ALLIGATOR CR	L	20.00	SqFt
48	L & T CR	M	119.00	Ft
57	WEATHERING	L	4135.00	SqFt
41	ALLIGATOR CR	L	18.00	SqFt

Network: Sunriver **Name:** Sunriver

Branch: T05SU **Name:** Taxiway 05 Sunriver **Use:** TAXIWAY **Area:** 13,690 SqFt

Section: 01 of 1 **From:** T01SU-02 **To:** R18SU-01 **Last Const.:** 7/1/2019

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** P

Area: 13,690 SqFt **Length:** 160 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 6/1/2014	**Work Type:** New Construction - AC	**Code:** NC-AC	**Is Major M&R:** True
Work Date: 7/1/2019	**Work Type:** Subbase - Aggregate	**Code:** SB-AG	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** Base Course - Aggregate	**Code:** BA-AG	**Is Major M&R:** False
Work Date: 7/1/2019	**Work Type:** New Construction - AC	**Code:** NC-AC	**Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 4 **Surveyed:** 3

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 3573.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 02 **Type:** R **Area:** 3627.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 04 **Type:** R **Area:** 3231.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: T06SU **Name:** Taxiway 06 Sunriver **Use:** TAXIWAY **Area:** 13,970 SqFt

Section: 01 of 1 **From:** T01SU-02 **To:** R18SU-01 **Last Const.:** 7/1/2019

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** **Category:** **Rank:** P

Area: 13,970 SqFt **Length:** 160 Ft **Width:** 75 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2019 **Work Type:** Base Course - Aggregate **Code:** BA-AG **Is Major M&R:** False

Work Date: 7/1/2019 **Work Type:** Subbase - Aggregate **Code:** SB-AG **Is Major M&R:** False

Work Date: 7/1/2019 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 4 **Surveyed:** 3

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 02 **Type:** R **Area:** 3470.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 03 **Type:** R **Area:** 3513.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 04 **Type:** R **Area:** 3570.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: T07SU **Name:** Taxiway 07 Sunriver **Use:** TAXIWAY **Area:** 7,431 SqFt

Section: 01 of 1 **From:** T01SU-01 **To:** R18SU-01 **Last Const.:** 7/1/2019

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** **Category:** **Rank:** P

Area: 7,431 SqFt **Length:** 130 Ft **Width:** 45 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 7/1/2019 **Work Type:** Base Course - Aggregate **Code:** BA-AG **Is Major M&R:** False

Work Date: 7/1/2019 **Work Type:** Subbase - Aggregate **Code:** SB-AG **Is Major M&R:** False

Work Date: 7/1/2019 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 2 **Surveyed:** 2

Conditions: **PCI:** 100

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 2744.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Sample Number: 02 **Type:** R **Area:** 4687.00 SqFt **PCI:** 100

Sample Comments:

<No Distress>

Network: Sunriver **Name:** Sunriver

Branch: THANGARSU **Name:** T-Hangar Taxiways Sunriver **Use:** TAXIWAY **Area:** 76,038 SqFt

Section: 02 of 5 **From:** THANGAR-01 **To:** End **Last Const.:** 1/1/1970

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** S

Area: 18,181 SqFt **Length:** 260 Ft **Width:** 70 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1970 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 4 **Surveyed:** 3

Conditions: **PCI:** 72

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 5250.00 SqFt **PCI:** 71

Sample Comments: Created by Inspection Schedule

50	PATCHING	L	108.00	SqFt
48	L & T CR	M	150.00	Ft
57	WEATHERING	L	5250.00	SqFt

Sample Number: 02 **Type:** R **Area:** 5250.00 SqFt **PCI:** 72

Sample Comments: Created by Inspection Schedule

50	PATCHING	L	200.00	SqFt
48	L & T CR	M	119.00	Ft
48	L & T CR	L	14.00	Ft
57	WEATHERING	L	5250.00	SqFt

Sample Number: 03 **Type:** R **Area:** 3500.00 SqFt **PCI:** 72

Sample Comments: Created by Inspection Schedule

57	WEATHERING	L	3500.00	SqFt
48	L & T CR	M	113.00	Ft
50	PATCHING	L	27.00	SqFt

Network: Sunriver **Name:** Sunriver

Branch: THANGARSU **Name:** T-Hangar Taxiways Sunriver **Use:** TAXIWAY **Area:** 76,038 SqFt

Section: 01 of 5 **From:** T04 **To:** End **Last Const.:** 1/1/1970

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** S

Area: 17,460 SqFt **Length:** 388 Ft **Width:** 45 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1970 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 4 **Surveyed:** 3

Conditions: **PCI:** 65

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 4500.00 SqFt **PCI:** 66

Sample Comments: Created by Inspection Schedule

57	WEATHERING	L	4500.00	SqFt
50	PATCHING	L	54.00	SqFt
48	L & T CR	M	220.00	Ft

Sample Number: 02 **Type:** R **Area:** 4500.00 SqFt **PCI:** 62

Sample Comments: Created by Inspection Schedule

48	L & T CR	L	61.00	Ft
50	PATCHING	L	100.00	SqFt
57	WEATHERING	L	4500.00	SqFt
48	L & T CR	M	200.00	Ft

Sample Number: 03 **Type:** R **Area:** 4500.00 SqFt **PCI:** 68

Sample Comments: Created by Inspection Schedule

48	L & T CR	M	253.00	Ft
57	WEATHERING	L	4500.00	SqFt

Network: Sunriver **Name:** Sunriver

Branch: THANGARSU **Name:** T-Hangar Taxiways Sunriver **Use:** TAXIWAY **Area:** 76,038 SqFt

Section: 03 of 5 **From:** THANGAR-01 **To:** End **Last Const.:** 1/1/1970

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** S

Area: 15,584 SqFt **Length:** 260 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1970 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 3 **Surveyed:** 2

Conditions: **PCI:** 71

Inspection Comments:

Sample Number: 02 **Type:** R **Area:** 6000.00 SqFt **PCI:** 71

Sample Comments: Created by Inspection Schedule

57	WEATHERING	L	6000.00	SqFt
48	L & T CR	M	164.00	Ft
50	PATCHING	L	135.00	SqFt

Sample Number: 03 **Type:** R **Area:** 3584.00 SqFt **PCI:** 71

Sample Comments: Created by Inspection Schedule

50	PATCHING	L	159.00	SqFt
57	WEATHERING	L	3584.00	SqFt
48	L & T CR	L	24.00	Ft
48	L & T CR	M	82.00	Ft

Network: Sunriver **Name:** Sunriver

Branch: THANGARSU **Name:** T-Hangar Taxiways Sunriver **Use:** TAXIWAY **Area:** 76,038 SqFt

Section: 04 of 5 **From:** THANGAR-01 **To:** End **Last Const.:** 1/1/1970

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** S

Area: 15,584 SqFt **Length:** 260 Ft **Width:** 60 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1970 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 3 **Surveyed:** 2

Conditions: **PCI:** 74

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 6000.00 SqFt **PCI:** 73

Sample Comments: Created by Inspection Schedule

50	PATCHING	L	76.00	SqFt
57	WEATHERING	L	6000.00	SqFt
48	L & T CR	M	154.00	Ft

Sample Number: 02 **Type:** R **Area:** 6000.00 SqFt **PCI:** 76

Sample Comments: Created by Inspection Schedule

48	L & T CR	M	125.00	Ft
50	PATCHING	L	51.00	SqFt
57	WEATHERING	L	6000.00	SqFt

Network: Sunriver **Name:** Sunriver

Branch: THANGARSU **Name:** T-Hangar Taxiways Sunriver **Use:** TAXIWAY **Area:** 76,038 SqFt

Section: 05 of 5 **From:** THANGAR-01 **To:** End **Last Const.:** 1/1/1970

Surface: AC **Family:** 2022_Central_Cat4/5_Taxiway_AC/AAC **Zone:** S21 **Category:** L **Rank:** S

Area: 9,229 SqFt **Length:** 260 Ft **Width:** 35 Ft

Slabs: **Slab Length:** Ft **Slab Width:** Ft **Joint Length:** Ft

Shoulder: **Street Type:** **Grade:** 0 **Lanes:** 0

Section Comments:

Work Date: 1/1/1970 **Work Type:** New Construction - AC **Code:** NC-AC **Is Major M&R:** True

Last Insp. Date: 3/1/2022 **TotalSamples:** 2 **Surveyed:** 2

Conditions: **PCI:** 75

Inspection Comments:

Sample Number: 01 **Type:** R **Area:** 5309.00 SqFt **PCI:** 77

Sample Comments: Created by Inspection Schedule

57	WEATHERING	L	5309.00	SqFt
50	PATCHING	L	35.00	SqFt
48	L & T CR	M	97.00	Ft

Sample Number: 02 **Type:** R **Area:** 3120.00 SqFt **PCI:** 72

Sample Comments: Created by Inspection Schedule

48	L & T CR	M	96.00	Ft
57	WEATHERING	L	3120.00	SqFt
48	L & T CR	L	12.00	Ft
50	PATCHING	L	12.00	SqFt

APPENDIX F

Work History Report

Pavement Database: ODA_WOC3_8-20-2022_PostSurvey

Network: Sunriver **Branch:** A01SU Apron 01 Sunriver **Section:** 01 **Surface:**AC

L.C.D. 7/1/2020 **Use:** APRON **Rank:** P **Length:** 625.00 (Ft) **Width:** 175.00 (Ft) **True Area:** 76472.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	CR-AC	Complete Reconstruction - AC	0.00	4.00	☑	
7/1/2020	SB-AG	Subbase - Aggregate	0.00	12.00	☐	
9/2/2015	PA-AD	Patching - AC Deep	0.00	0.00	☐	PMP 2015
9/1/2015	CS-AC	Crack Sealing - AC	0.00	0.00	☐	PMP 2015
9/2/2012	PA-AD	Patching - AC Deep	0.00	0.00	☐	PMP 2012, includes Joint Repair
9/1/2012	CS-AC	Crack Sealing - AC	0.00	0.00	☐	PMP 2012
9/1/2003	CS-AC	Crack Sealing - AC	0.00	0.10	☐	
9/1/1999	ST-SC	Surface Treatment - Seal Coat (Global MR)	0.00	0.10	☐	
9/1/1997	OL-AT	Overlay - AC Thin	0.00	2.00	☑	
9/1/1997	CS-AC	Crack Sealing - AC	0.00	0.10	☐	UNKNOWN DATE
9/2/1985	NC-AC	New Construction - AC	0.00	2.00	☑	
9/1/1985	BA-AG	Base Course - Aggregate	0.00	24.00	☐	

Network: Sunriver **Branch:** A01SU Apron 01 Sunriver **Section:** 02 **Surface:**PCC

L.C.D. 7/1/2020 **Use:** APRON **Rank:** P **Length:** 300.00 (Ft) **Width:** 67.00 (Ft) **True Area:** 20575.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	CR-PC	Complete Reconstruction - PCC	172,000.00	6.00	☑	
7/1/2020	SB-AG	Subbase - Aggregate	0.00	10.00	☐	
9/1/1997	CS-AC	Crack Sealing - AC	0.00	0.10	☐	UNKNOWN DATE
9/3/1970	ST-SC	Surface Treatment - Seal Coat (Global MR)	0.00	0.10	☐	UNKNOWN
9/2/1970	SU-DB	Surface Course - Double Bitum.	0.00	1.50	☑	
9/1/1970	BA-AG	Base Course - Aggregate	0.00	14.00	☐	Cinder

Network: Sunriver **Branch:** A01SU Apron 01 Sunriver **Section:** 03 **Surface:**AC

L.C.D. 7/1/2020 **Use:** APRON **Rank:** P **Length:** 925.00 (Ft) **Width:** 400.00 (Ft) **True Area:** 349900.0001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	CR-AC	Complete Reconstruction - AC	0.00	4.00	☑	
7/1/2020	SB-AG	Subbase - Aggregate	0.00	12.00	☐	
9/1/2003	CS-AC	Crack Sealing - AC	0.00	0.10	☐	
9/1/1999	OL-AT	Overlay - AC Thin	0.00	1.50	☑	
9/2/1970	SU-DB	Surface Course - Double Bitum.	0.00	1.50	☑	
9/1/1970	BA-AG	Base Course - Aggregate	0.00	14.00	☐	Cinder

Pavement Database: ODA_WOC3_8-20-2022_PostSurvey

Network: Sunriver **Branch:** A01SU Apron 01 Sunriver **Section:** 04 **Surface:**PCC
L.C.D. 7/1/2020 **Use:** APRON **Rank:** P **Length:** 130.00 (Ft) **Width:** 150.00 (Ft) **True Area:** 19409.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	CR-PC	Complete Reconstruction - PCC	353,800.00	0.00	☑	
9/1/1999	ST-SC	Surface Treatment - Seal Coat (Global MR)	0.00	0.10	☐	
9/1/1997	CS-AC	Crack Sealing - AC	0.00	0.10	☐	UNKNOWN DATE
9/1/1982	OL-AT	Overlay - AC Thin	0.00	0.00	☑	UNKNOWN
9/2/1970	SU-DB	Surface Course - Double Bitum.	0.00	1.50	☑	
9/1/1970	BA-AG	Base Course - Aggregate	0.00	14.00	☐	Cinder

Network: Sunriver **Branch:** A01SU Apron 01 Sunriver **Section:** 05 **Surface:**PCC
L.C.D. 7/1/2020 **Use:** APRON **Rank:** P **Length:** 275.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 20625.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	NC-PC	New Construction - PCC	0.00	6.00	☑	
7/1/2020	SB-AG	Subbase - Aggregate	0.00	22.00	☐	

Network: Sunriver **Branch:** A01SU Apron 01 Sunriver **Section:** 06 **Surface:**PCC
L.C.D. 7/1/2020 **Use:** APRON **Rank:** P **Length:** 90.00 (Ft) **Width:** 20.00 (Ft) **True Area:** 1868.000000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	NU-IN	New Construction - Initial	0.00	0.00	☑	

Network: Sunriver **Branch:** A02SU Apron 02 Sunriver **Section:** 01 **Surface:**AC
L.C.D. 7/1/2019 **Use:** APRON **Rank:** P **Length:** 100.00 (Ft) **Width:** 70.00 (Ft) **True Area:** 7016.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2019	NC-AC	New Construction - AC	0.00	4.00	☑	
7/1/2019	BA-AG	Base Course - Aggregate	0.00	6.00	☐	
7/1/2019	SU-CA	Subbase - Crushed Aggregate	0.00	24.00	☐	

Network: Sunriver **Branch:** R18SU Runway 18/36 Sun **Section:** 01 **Surface:**AC
L.C.D. 9/2/2009 **Use:** RUNWAY **Rank:** P **Length:** 5,460.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 409820.0001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
9/1/2015	CS-AC	Crack Sealing - AC	0.00	0.00	☐	PMP 2015
9/1/2012	CS-AC	Crack Sealing - AC	0.00	0.00	☐	PMP 2012
9/2/2009	CR-AC	Complete Reconstruction - AC	0.00	2.50	☑	
9/1/2009	BA-PA	Base Course - Pulverized AC	0.00	8.00	☐	
9/1/1985	OL-AT	Overlay - AC Thin	0.00	2.00	☑	
9/1/1970	BA-AG	Base Course - Aggregate	0.00	9.50	☐	Cinder

Network: Sunriver **Branch:** T01SU Taxiway 01 Sunriv **Section:** 01 **Surface:**AC

L.C.D. 7/1/2019 **Use:** TAXIWAY **Rank:** P **Length:** 1,880.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 76490.00002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2019	BA-AG	Base Course - Aggregate	0.00	6.00	☐	
7/1/2019	CR-AC	Complete Reconstruction - AC	0.00	4.00	☑	
7/1/2019	SB-AG	Subbase - Aggregate	0.00	24.00	☐	
9/2/2008	PA-AD	Patching - AC Deep	0.00	0.00	☐	PMP 2008
9/1/2008	CS-WD	Crack Seal - Wide Cracks	0.00	0.00	☐	PMP 2008
9/1/1997	CS-AC	Crack Sealing - AC	0.00	0.10	☐	
9/1/1990	OL-AT	Overlay - AC Thin	0.00	0.00	☑	UNKNOWN DEPTH
9/2/1970	SU-DB	Surface Course - Double Bitum.	0.00	1.50	☑	
9/1/1970	BA-AG	Base Course - Aggregate	0.00	14.00	☐	Cinder

Network: Sunriver **Branch:** T01SU Taxiway 01 Sunriv **Section:** 02 **Surface:**AC

L.C.D. 7/1/2019 **Use:** TAXIWAY **Rank:** P **Length:** 3,590.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 141040.0000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2019	BA-AG	Base Course - Aggregate	0.00	6.00	☐	
7/1/2019	CR-AC	Complete Reconstruction - AC	0.00	4.00	☑	
7/1/2019	SB-AG	Subbase - Aggregate	0.00	24.00	☐	
9/2/2015	PA-AD	Patching - AC Deep	0.00	0.00	☐	PMP 2015
9/1/2015	CS-AC	Crack Sealing - AC	0.00	0.00	☐	PMP 2015
9/1/2012	CS-AC	Crack Sealing - AC	0.00	0.00	☐	PMP 2012
9/2/2008	PA-AD	Patching - AC Deep	0.00	0.00	☐	PMP 2008
9/1/2008	CS-WD	Crack Seal - Wide Cracks	0.00	0.00	☐	PMP 2008
9/1/2003	CS-AC	Crack Sealing - AC	0.00	0.10	☐	
9/1/1999	ST-SC	Surface Treatment - Seal Coat (Global MR)	0.00	0.10	☐	
9/1/1997	OL-AT	Overlay - AC Thin	0.00	2.00	☑	
9/2/1985	NC-AC	New Construction - AC	0.00	2.00	☑	
9/1/1985	BA-AG	Base Course - Aggregate	0.00	24.00	☐	

Network: Sunriver **Branch:** T02SU Taxiway 02 Sunriv **Section:** 01 **Surface:**AC

L.C.D. 7/1/2019 **Use:** TAXIWAY **Rank:** P **Length:** 160.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 11900.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2019	BA-AG	Base Course - Aggregate	0.00	6.00	☐	
7/1/2019	CR-AC	Complete Reconstruction - AC	0.00	4.00	☑	
7/1/2019	SB-AG	Subbase - Aggregate	0.00	24.00	☐	
9/1/2015	CS-AC	Crack Sealing - AC	0.00	0.00	☐	PMP 2015
9/1/2012	CS-AC	Crack Sealing - AC	0.00	0.00	☐	PMP 2012
9/2/2008	PA-AD	Patching - AC Deep	0.00	0.00	☐	PMP 2008
9/1/2008	CS-WD	Crack Seal - Wide Cracks	0.00	0.00	☐	PMP 2008
9/1/1999	ST-SC	Surface Treatment - Seal Coat (Global MR)	0.00	0.10	☐	
9/1/1997	OL-AT	Overlay - AC Thin	0.00	0.00	☑	UNKNOWN
9/2/1970	SU-DB	Surface Course - Double Bitum.	0.00	1.50	☑	
9/1/1970	BA-AG	Base Course - Aggregate	0.00	14.00	☐	Cinder

Pavement Database: ODA_WOC3_8-20-2022_PostSurvey

Network: Sunriver **Branch:** T03SU Taxiway 03 Sunriv **Section:** 01 **Surface:**AC

L.C.D. 7/1/2019 **Use:** TAXIWAY **Rank:** P **Length:** 160.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 14230.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2019	BA-AG	Base Course - Aggregate	0.00	6.00	☐	
7/1/2019	CR-AC	Complete Reconstruction - AC	0.00	4.00	☑	
7/1/2019	SB-AG	Subbase - Aggregate	0.00	24.00	☐	
9/1/2012	CS-AC	Crack Sealing - AC	0.00	0.00	☐	PMP 2012
9/2/2008	PA-AD	Patching - AC Deep	0.00	0.00	☐	PMP 2008
9/1/2008	CS-WD	Crack Seal - Wide Cracks	0.00	0.00	☐	PMP 2008
9/1/1997	CS-AC	Crack Sealing - AC	0.00	0.10	☐	
9/1/1985	OL-AT	Overlay - AC Thin	0.00	2.00	☑	
9/2/1970	SU-DB	Surface Course - Double Bitum.	0.00	1.50	☑	
9/1/1970	BA-AG	Base Course - Aggregate	0.00	14.00	☐	Cinder

Network: Sunriver **Branch:** T04SU Taxiway 04 Sunriv **Section:** 01 **Surface:**AC

L.C.D. 7/1/2020 **Use:** TAXIWAY **Rank:** S **Length:** 190.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 6361.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	CR-AC	Complete Reconstruction - AC	31,805.00	4.00	☑	
7/1/2020	SB-AG	Subbase - Aggregate	0.00	12.00	☐	

Network: Sunriver **Branch:** T04SU Taxiway 04 Sunriv **Section:** 02 **Surface:**AC

L.C.D. 1/1/1970 **Use:** TAXIWAY **Rank:** S **Length:** 120.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 4135.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1970	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: Sunriver **Branch:** T04SU Taxiway 04 Sunriv **Section:** 03 **Surface:**AC

L.C.D. 7/1/2020 **Use:** TAXIWAY **Rank:** S **Length:** 115.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 3980.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	NC-AC	New Construction - AC	0.00	4.00	☑	
7/1/2020	SB-AG	Subbase - Aggregate	0.00	12.00	☐	

Network: Sunriver **Branch:** T04SU Taxiway 04 Sunriv **Section:** 04 **Surface:**AC

L.C.D. 7/1/2020 **Use:** TAXIWAY **Rank:** S **Length:** 135.00 (Ft) **Width:** 25.00 (Ft) **True Area:** 4734.000001 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2020	NC-AC	New Construction - AC	0.00	4.00	☑	
7/1/2020	SB-AG	Subbase - Aggregate	0.00	12.00	☐	

Network: Sunriver **Branch:** T05SU Taxiway 05 Sunriv **Section:** 01 **Surface:**AC

L.C.D. 7/1/2019 **Use:** TAXIWAY **Rank:** P **Length:** 160.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 13690.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2019	NC-AC	New Construction - AC	0.00	4.00	☑	
7/1/2019	BA-AG	Base Course - Aggregate	0.00	6.00	☐	
7/1/2019	SB-AG	Subbase - Aggregate	0.00	24.00	☐	
6/1/2014	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: Sunriver **Branch:** T06SU Taxiway 06 Sunriv **Section:** 01 **Surface:**AC

L.C.D. 7/1/2019 **Use:** TAXIWAY **Rank:** P **Length:** 160.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 13970.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2019	NC-AC	New Construction - AC	0.00	4.00	☑	
7/1/2019	BA-AG	Base Course - Aggregate	0.00	6.00	☐	
7/1/2019	SB-AG	Subbase - Aggregate	0.00	24.00	☐	

Network: Sunriver **Branch:** T07SU Taxiway 07 Sunriv **Section:** 01 **Surface:**AC

L.C.D. 7/1/2019 **Use:** TAXIWAY **Rank:** P **Length:** 130.00 (Ft) **Width:** 45.00 (Ft) **True Area:** 7431.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2019	NC-AC	New Construction - AC	0.00	4.00	☑	
7/1/2019	BA-AG	Base Course - Aggregate	0.00	6.00	☐	
7/1/2019	SB-AG	Subbase - Aggregate	0.00	24.00	☐	

Network: Sunriver **Branch:** THANGARS T-Hangar Taxiway **Section:** 01 **Surface:**AC

L.C.D. 1/1/1970 **Use:** TAXIWAY **Rank:** S **Length:** 388.00 (Ft) **Width:** 45.00 (Ft) **True Area:** 17460.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1970	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: Sunriver **Branch:** THANGARS T-Hangar Taxiway **Section:** 02 **Surface:**AC

L.C.D. 1/1/1970 **Use:** TAXIWAY **Rank:** S **Length:** 260.00 (Ft) **Width:** 70.00 (Ft) **True Area:** 18181.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1970	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: Sunriver **Branch:** THANGARS T-Hangar Taxiway **Section:** 03 **Surface:**AC

L.C.D. 1/1/1970 **Use:** TAXIWAY **Rank:** S **Length:** 260.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 15584.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1970	NC-AC	New Construction - AC	0.00	0.00	☑	

Network: Sunriver **Branch:** THANGARS T-Hangar Taxiway **Section:** 04 **Surface:**AC

L.C.D. 1/1/1970 **Use:** TAXIWAY **Rank:** S **Length:** 260.00 (Ft) **Width:** 60.00 (Ft) **True Area:** 15584.00000 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1970	NC-AC	New Construction - AC	0.00	0.00	☑	

Pavement Database: ODA_WOC3_8-20-2022_PostSurvey

Network: Sunriver **Branch:** THANGARS T-Hangar Taxiway **Section:** 05 **Surface:**AC

L.C.D. 1/1/1970 **Use:** TAXIWAY **Rank:** S **Length:** 260.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 9229.000002 (SqFt

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
1/1/1970	NC-AC	New Construction - AC	0.00	0.00	☑	

Summary:

Work Description	Section Count	Area Total (SqFt)	Thickness Avg (in)	Thickness STD (in)
Base Course - Aggregate	17	1,405,603.00	11.15	5.91
Base Course - Pulverized AC	1	409,820.00	8.00	0.00
Complete Reconstruction - AC	8	1,086,213.00	3.81	0.50
Complete Reconstruction - PCC	2	39,984.00	3.00	3.00
Crack Seal - Wide Cracks	4	243,660.00	0.00	0.00
Crack Sealing - AC	17	2,067,282.00	0.05	0.05
New Construction - AC	15	362,196.00	1.87	1.86
New Construction - Initial	1	1,868.00	0.00	0.00
New Construction - PCC	1	20,625.00	6.00	0.00
Overlay - AC Thin	8	1,099,261.00	1.19	0.93
Patching - AC Deep	7	537,644.00	0.00	0.00
Subbase - Aggregate	14	761,398.00	18.57	6.02
Subbase - Crushed Aggregate	1	7,016.00	24.00	0.00
Surface Course - Double Bitum.	6	492,504.00	1.50	0.00
Surface Treatment - Seal Coat (Global MR)	5	269,396.00	0.10	0.00