# Application Instructions

# All Roads Transportation Safety (ARTS) Program

This application is for the Oregon Department of Transportation (ODOT)'s Highway Safety Improvement Program (HSIP) – All Roads Transportation Safety (ARTS). Applicants should submit their applications to their respective ODOT Region Traffic Offices directed to the attention of the Region Traffic Manager (RTM), by the designated deadline listed for each Region on the ARTS website. Applications that are delivered after close of business on the due date or are postmarked later than the due date will not be accepted.

Applicants must download the PDF application from the Traffic-Roadway Section (TRS) Highway Safety website. Applicants must prepare their electronic (preferable) **or** hard-copy applications as defined in these instructions. Failure to follow this procedure may result in application rejection.  All applications must be submitted within the application window (September 11<sup>th</sup> 2020 through December 14<sup>th</sup> 2020) at the following weblink:

- http://odot2020arts.com/

Application submittals **shall** include the following completed documents:
- The application form
- Cost Estimate
- Crash data reports (in EXCEL format from ODOT's Crash Reporting website or output from the Tableau Dataviewer)
- Benefit-Cost worksheet(s) **or** Cost-Effectiveness worksheet(s)
- Aerial Vicinity Map / Location Map
- Traffic Analysis (Traffic counts, NCHRP 562 spreadsheet, etc.)

Applicant submittals **should** include the following documents:
- Field Scope Verification (pictures, field observations, user behavior)
- Conceptual Layout or Project Concept Drawing
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Applicant submittals **may** include the following documents:
- Collision Diagram(s)
- Additional background information (road safety audit findings, previous scoping documents, planning studies, letters of support, etc.)

It is important to read through the Application Instructions before attempting to prepare and submit the application. Applicants are highly encouraged to utilize and follow these instructions in a step-by-step process as they complete their application(s).

While Agencies are encouraged to apply for multiple projects, ODOT will only accept **one proposed project per application.** Multiple locations can be included into one application only if the same countermeasures are applied to every location. The benefit-cost analysis or cost effectiveness index shall also incorporate all of the locations. Also, while Systemic and Hotspot countermeasures may be applicable at the same location, the supporting benefit/cost spreadsheet can only include countermeasures of the application type. Once approved for funding, the measures can be combined under one project if desired.

## General Preparations

Applicants are expected to submit applications based on a data-driven, comprehensive safety evaluation of their agency's roadway infrastructure, traffic volumes and crash data. Prior to filling out the PDF application, it is recommended that applicants complete some general preparations:

1) Read the ARTS Program Guidelines and other related documents.

   Becomes familiar with the crash reduction factors (under the Crash Reduction Factors section) eligible

for this program. Review SPIS and the ODOT Implementation Plans (under the Safety Improvement Plans section) for Roadway Departure, Intersection and Bicycle and Pedestrian safety. These documents provide proposed roadway (both State and Non-State) locations and segments that would benefit from implementation of systemic countermeasures.

- o Safety Priority Index System (SPIS): Developed by ODOT, the Safety Priority Index System is a flagging tool that identifies public roadway segments experiencing unusually high crash occurrences.  Annual reports are generated (both State and Non-State), listing roadway segments with a calculated SPIS score based on crash rate, frequency, and severity over the prior three calendar years.  A higher SPIS score indicates higher potential safety needs for the identified roadway segment.

- o Systemic Roadway Departure Plan: Identifies additional areas in which roadway departure (RwD) safety can be increased.

- o Systemic Intersection Plan: Provides specifics on countermeasure implementation actions, key steps, schedules, and investments needed to achieve Oregon's 2012 TSAP goal of reducing the number of fatalities in Oregon to less than 360 by 2030.

- o Systemic Bicycle and Pedestrian Plan: Identifies corridors with the most potential for reducing frequency and severity of pedestrian and bicycle crashes.  Identifies priority locations and potential countermeasure options.

2) Become familiar with the ODOT Crash Reporting website and the excel crash decoder in addition to the benefit-cost and cost-effectiveness worksheets (all under the Analysis Tools and Forms section).

# Part 1: Basic Project Information

Hot Spot or Systemic
Select application type (choose only one), if the application is Systemic; further identify the type (intersection, road departure or bicycle/pedestrian)

Agency
Type in the full name of the agency applying.

ODOT Region
Type in the ODOT Region that the proposed project will take place in. If you do not know which Region the project is in, consult ODOT's TransGIS website. Use the menus to choose Display – Layer Catalog – Boundaries – ODOT Regions. (This site will also help you find geographic coordinates, if you need them to describe your project.)

County
Type in the name of the County where the proposed project will be located. If there are multiple counties, please note this.

City (if applicable)
Type in the City that the proposed project will take place in. Type "None" if not applicable. If there are multiple cities, please note this.

Under Contact Information

Project Engineer and Sponsor's Name, Title, Phone number and email address:
The Project Engineer is the technical lead who is developing the project. The Project Sponsor is the person with budget authority who can commit to funding the match for the project. This information will be used to coordinate with the agency relating to funding results and later for project delivery questions.

Under Project Information
Provide road name(s), intersection cross street names, and/or geographical references of where the project is

located. For proposed projects on the State Highway System, please provide highway names, numbers, and mile points. If there are multiple locations please list them in the space provided or attach a separate document summarizing the locations.

Example for Non-State Roads: *"On Park Ave between 5th St and 10th St".*
Example for State Highways: *"On Eugene-Springfield Highway (Hwy 227) MP 1.52 – MP 3.86" or "On Tualatin Valley Highway (OR8) from Murray Blvd to SW 198th Ave".*

Project Cost
This value is taken directly from the Cost Estimate worksheet and is the same cost inputted in "Estimated Project Cost" cell on the Benefit/Cost Analysis worksheet.

Project Benefit/Cost Ratio
This value is taken directly from the "B/C Ratio" cell on the Benefit/Cost Analysis worksheet or from the "Cost-Effectiveness Index" cell in the CEI worksheet. Agencies shall only submit applications with Benefit/Cost ratios of 1.0 or greater. Applications submitted with Benefit/Cost ratios less than 1.0 will be rejected and will not be evaluated.

Countermeasures
Select from the drop-down list the countermeasure(s) this project will include. Please note that a maximum four countermeasures can be included in each application and the selected countermeasures shall be selected from the approved CRF list.

Scope (Project) Description

This description contains all intended work with the proposed project, including work not represented by the countermeasures listed. Be specific about the scope of work.

Example: *"Protected/permissive LT phasing on both Holgate and 112th"* or *"Install supplemental signal heads at the intersections of Butner @ Cedar Hills Blvd and Skyline @ Sunset Hwy-Skyline Blvd"*

Other Stakeholders/Coordination
This description contains information for other stakeholders or coordination efforts needed to complete the project.

## Additional Project Information

Identify the focus area of the proposed project

The application types are separated into two categories; Hotspot and Systemic. The systemic portion of the ARTS program is further divided into three focus areas: roadway departure, intersection and bicycle and pedestrian. Similarly, the available countermeasures eligible for each type of application is also divided into these categories. Choose the focus area based on the **primary** countermeasure you have identified for inclusion in your proposed project. The **primary** countermeasure is identified by the countermeasure that will have the highest cost associated with the proposed project. It is important to note that while Systemic and Hotspot countermeasures may be applicable at the same location, the supporting benefit/cost spreadsheet can only include countermeasures of the application type. Once approved for funding, the measures can be combined under one project if desired.

Links to the following resources are listed below:

SPIS Lists (Local & Highway)
Oregon Adjustable Safety Index System (OASIS)
Systemic Implementation Plans (under Safety Improvement Plans section)

Links to these resources can also be found on the ARTS webpage.

**Project Funding Sources**

Match Requirement
The Federal Highway Safety Improvement Program (HSIP) currently requires a 7.78% match for projects. Within the ARTS program ODOT will require participating agencies to contribute match to the project. This will require local agencies to come up with the 7.78% non-federal cash match.

If you are unable to contribute this match, please call your appropriate Region Traffic Office **after** you have submitted your application to discuss potential alternatives.

# Application Attachments

Please double check all attachments are included in this application.

1. Cost Estimate(Required)
   a) Applicants must provide a thorough cost estimate.
   b) The detailed engineer's estimate must demonstrate how the total construction cost is being split among countermeasures, other safety related improvements and non-safety-related improvements. The estimate should include other costs such as PE, CE, mobilization, ADA upgrades, etc.
   c) If your cost estimate is inadequate, you may be asked to revise the cost estimate or provide additional justification.

2. Benefit/Cost Analysis or Cost-Effectiveness Index Worksheet (Required)
   a) Benefit/Cost Analysis is required for ALL proposed Hot Spot, Roadway Departure or Intersection projects
   b) Cost-Effectiveness Index is required for ALL proposed Bicycle and Pedestrian projects
   c) Applicants must use the appropriate worksheets provided by ODOT.

3. Aerial Vicinity Map / Location Map (Required)
   a) The application reviewers and the Program Managers must be able to quickly pinpoint the project's location in the state and local agency. This map needs to show where the project is located within the overall agency. It is not intended to show the specific project limits.

4. Field Scoping Verification (Highly Recommended)
   a) Notes from a site visit or mini road safety audit to observe user behavior. Notes can include other countermeasures that were considered but not recommended and why. A site visit can help verify the defined problem and support the recommended solution.
   b) Pictures or a note that pictures can be provided as requested

5. Crash Report(s) (Required)
   a) Required for ALL projects.
   b) Must be electronic copy, if one cannot be provided please contact your Region Traffic Office.
   c) Applicants must include a list of crashes that matches the crashes applied to a countermeasure in the benefit/cost or cost-effectiveness calculations. This list shall be a direct output from the ODOT Crash Data System.
   d) If the output list includes crashes that were not appropriate to include in the project B/C calculations, these crashes must be crossed through or removed.

6. Traffic Analysis (Required where applicable)
   a) Required when the project includes an improvement that requires an engineering study to warrant the installation of certain traffic control devices, e.g., bike signals, pedestrian hybrid beacons, etc. When applications include traffic control features like these, it is the applicants' responsibility to ensure all requirements of the latest MUTCD are met. Failure to include required warrants completed per MUTCD will result in the project being disqualified.
   b) Other traffic analysis can include traffic counts, NCHRP 562 spreadsheet, pedestrian study, capacity analysis, etc.

7. Conceptual Layout or Project Concept Drawing  (Recommended)

a) The limits of all planned construction items/activities must be shown, including any non-safety elements of the project that will be going to construction.

b) Show individual limits of each countermeasure utilized in the benefit/cost or cost-effectiveness calculations for the application.

8. Collision Diagram(s) (Optional)
   a) If your agency already has a completed collision diagram for the project location(s), please attach them. This will help the reviewers to quickly identify crash patterns.

9. Additional Background Information (Optional)

   a) These may be used to help illustrate the safety concerns within the project limits, to provide justification and support for the cost estimate, to show other work that has been done in support of the project and support future scoping efforts.

   b) These should be directly related to documenting the merits of the need, purpose and scope of the project.

This application form is set-up to allow you to save the required documents within the application form.
1. Save a copy of the application form to your local drive
2. Add the required attachments
   a. Please note, the form will not accept macro enabled spreadsheets, please make sure the excel extension is **.xlsx**
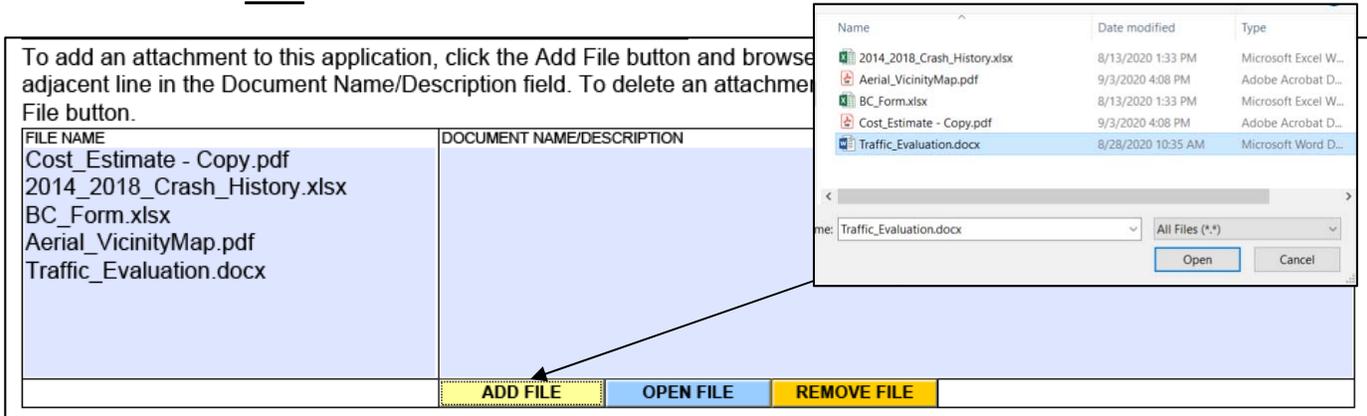


3. Save the file and upload to the ARTS application website (http://odot2020arts.com/applications/ )

# Project Commitment

It is the agency's responsibility to ensure the ARTS application is reviewed and supported. Applications that do not contain the required Project Sponsor name, title and signature will be disqualified. This signature can be provided electronically or through a hard copy application.

The agency's transportation manager attesting to the information in the application must take care to ensure they follow these application instructions, including but not limited to: all data is accurate and represents the total scope and costs; countermeasures are applied consistently; and, crash data is accurately shown and applied to countermeasures.

# Submitting the Application

Applications must be **received** by the due date listed in the **application window** (September 14th – December 11th). Please make sure that all application are submitted to the appropriate ARTS Region tab by the due date and time. Applications should be submitted at http://odot2020arts.com/applications/ .

| SUMMER 2020 | SEPTEMBER 14 – DECEMBER 11TH | WINTER-SPRING 2020 |
|---|---|---|
| OUTREACH TO LOCAL AGENCIES | APPLICATION SUBMISSION | PROJECT SELECTION FOR SCOPING |

If you have any questions about **how to fill out or submit the ARTS application**, please feel free to contact the appropriate Region representative or Consultant for your project:

| ODOT Region | Contact | Phone Number | Email Address |
|---|---|---|---|
| Region 1 | Sam Sharma | 503-731-3427 | Shyam.SHARMA@odot.state.or.us |
| Region 2 | Amanda Salyer | 503.986.5808 | Amanda.SALYER@odot.state.or.us |
| Region 3 White City | Dan Dorrell | 541.864.8825 | Dan.w.Dorrell@odot.state.or.us |
| Region 3 Roseburg | Aaron Brooks | 541.957.3517 | Aaron.Brooks@odot.state.or.us |
| Region 4 | Dan Serpico | 541.388.6170 Ext. 237 | Daniel.S.SERPICO@odot.state.or.us |
| Region 5 | Jeff Wise | 541.963.1902 | Jeff.WISE@odot.state.or.us |
|  | Don Fine | 541-963-1594 | Donald.g.fine@odot.state.or.us |

Or Christina McDaniel-Wilson (christina.a.mcdaniel-wilson@odot.state.or.us) ODOT TRS at 503-986-3573 or contact Lacy Brown (lacy.brown@dksassociates.com) with DKS Associates at (503)391-8773.