

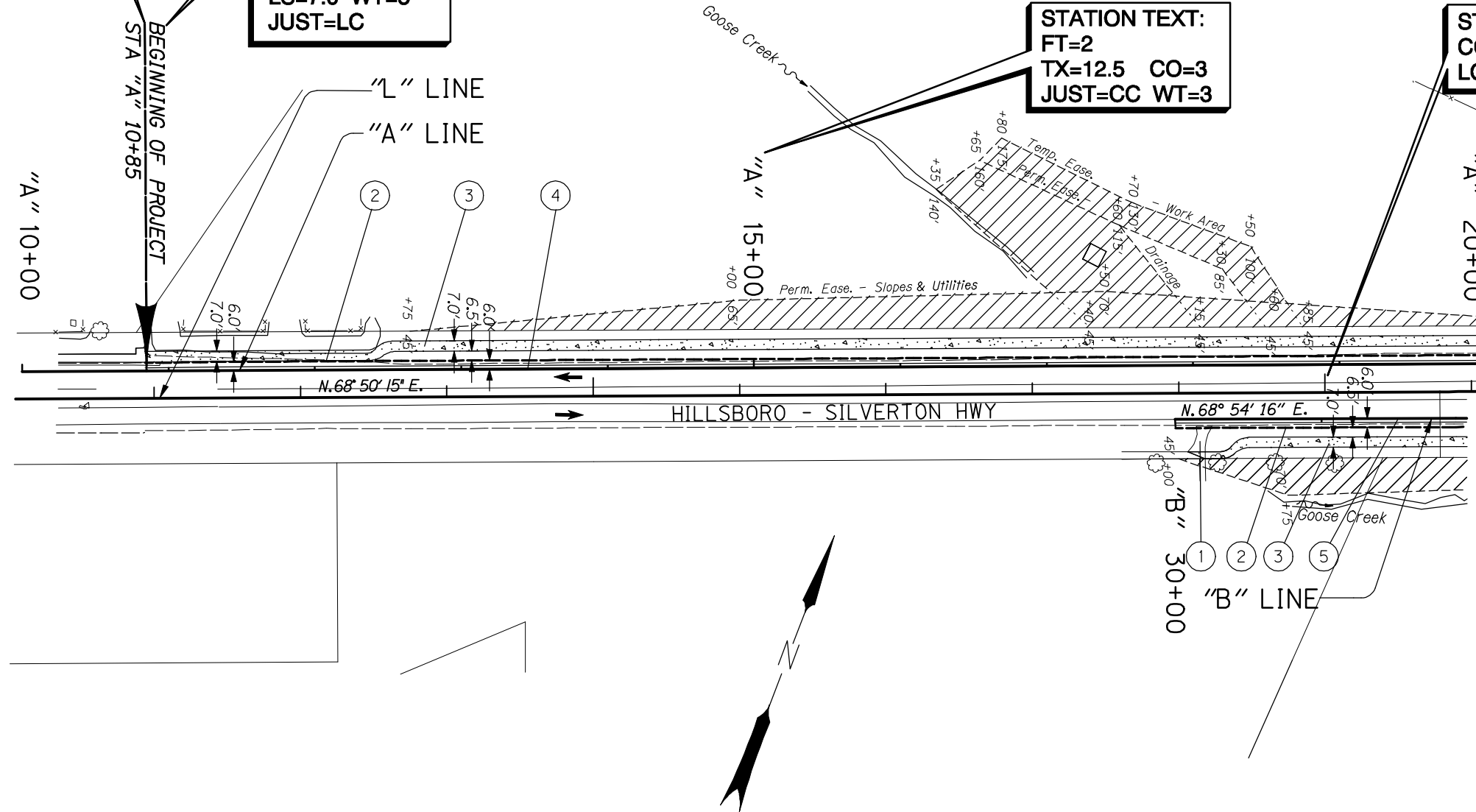
**PROJECT LIMIT LINE:**  
CO=4 WT=5  
LC=pro subgrade\_V8

**PROJECT LIMIT TEXT:**  
FT=24  
TX=9.4 CO=4  
LS=7.0 WT=5  
JUST=LC

**STATION TEXT:**  
FT=2  
TX=12.5 CO=3  
JUST=CC WT=3

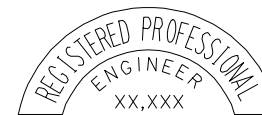
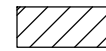
**STATION TICS:**  
CO=3 WT=3  
LC=0

- ① Remove Extg. Sidewalk  
(For Details, See Sht. 2B-2)
- ② Const. Curb and Gutter - 1115'  
(See Drg. No. RD700)
- ③ Const. P.C. Conc. Walk - 854 Sq.Yd.  
(For Details, See Sht. 2B-2)  
(See Drg. No. RD720)
- ④ Sta. "A" 10+85 To Sta. "A" 22+49  
Place 4" White Line - 1250'  
(See Drg. No. TM525)
- ⑤ Sta. "B" 30+00 To Sta. "B" 52+60  
Place 4" White Line - 2260'



**CONSULTANT LOGO LOCATION**

Easements Noted Thus Indicate Right Of Way Files That Have Not Been Acquired As Of Printing Of The Plans. Acquisition Date Will Be No Later Than June 10, 2004. See Specs For More Information.



**OREGON DEPARTMENT OF TRANSPORTATION**  
ROADWAY ENGINEERING SECTION

**OR 214: WOODBURN-MT. ANGEL**  
**PHASE 2 (SIDEWALKS)**  
HILLSBORO - SILVERTON HIGHWAY  
MARION COUNTY

Design Team Leader - Clara O'Mara  
Designed By - George B. Smilin  
Drafted By - Lenda Hand

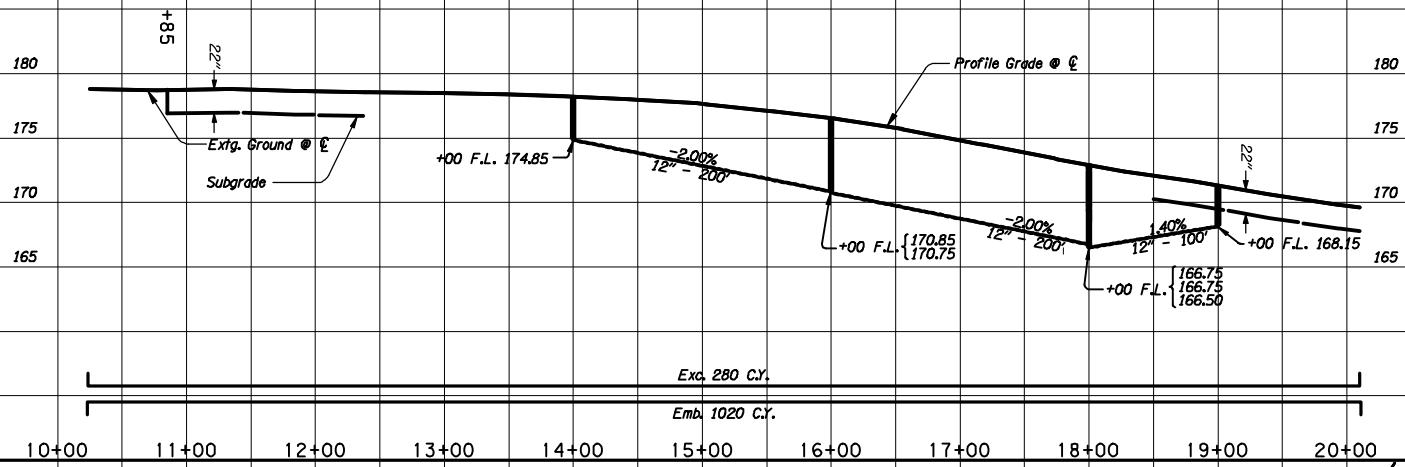
**ALIGNMENT AND**  
**GENERAL CONSTRUCTION**

SHEET NO.  
**3**

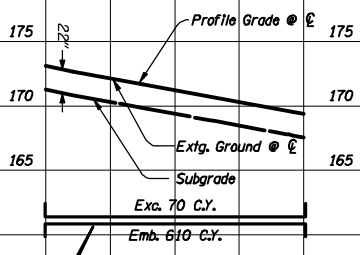
37V-69

Submittal Block Or  
"V" Number Stored  
In The Reference  
File KEYNUF.NAM

"A" LINE



"B" LINE



SEPARATION  
LINE:  
CO=3  
WT=5  
LC=0

EARTHWORK BRACKETS:  
CO=3  
LC=0 WT=4  
Or Use ODOT Menu/  
General/Earthwork Brackets

NOTE:

Finish Grade For Curb And Gutter Will Closely Match Extg. Pavement  
And Will Be Supplied At Regular Station Intervals For Construction Staking.



OREGON DEPARTMENT OF TRANSPORTATION  
ROADWAY ENGINEERING SECTION

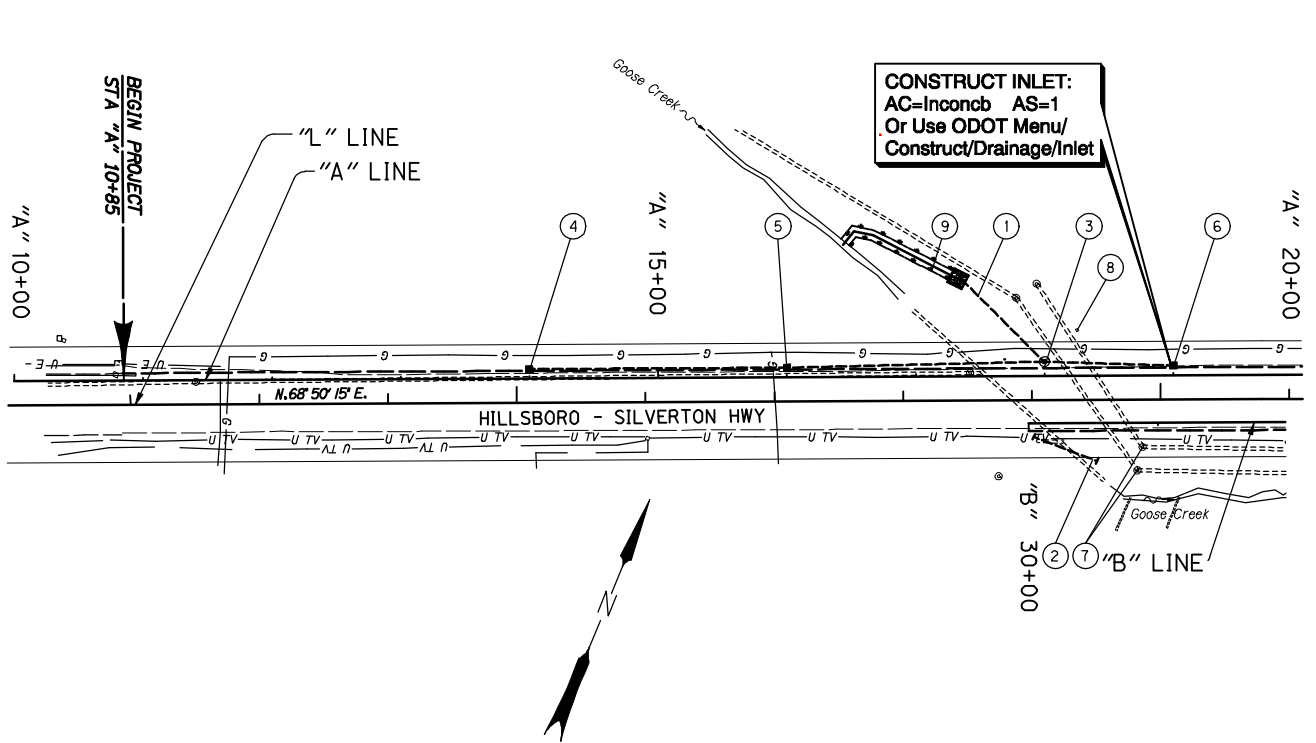
OR 214: WOODBURN-MT. ANGEL  
PHASE 2 (SIDEWALKS)  
HILLSBORO - SILVERTON HIGHWAY  
MARION COUNTY

Design Team Leader - Clara O'Mara  
Designed By - George B. Smlin  
Drafted By - Lenda Hand

PROFILES

SHEET  
NO.  
3A

11-2



- ① Sta. "A" 17+38.00, 73' Lt.  
Inst. 12" Storm Sew. Pipe - 88'  
10' Depth  
S = 0.0452'/Ft.  
I.E. (12" Outfall) - 162.52  
(For Details, See Shts. GH4 And GH5)  
Const. Paved End Slope - 20 Sq.Ft.  
(See Drgs. No. RD300 And RD320)
- ② Sta. "B" 30+03.00, 10' Rt. To Sta. 30+52.00, 29' Rt.  
Inst. 12" Culvert Pipe - 53'  
5' Depth  
S = 0.1132'/Ft.  
I.E. (12" In) - 171.00±  
I.E. (12" Outfall) - 165.00±  
(See Drgs. No. RD316 And RD318)
- ③ Sta. "A" 18+00.00, 11.00' Lt.  
Inst. Storm Sew. Manhole  
Rim Elev. = 172.5±  
I.E. (12" In) - 166.75 (W)  
I.E. (12" In) - 166.75 (E)  
I.E. (12" Out) - 166.50 (NW)  
(See Drg. No. RD342)
- ④ Sta. "A" 14+00.00, 6.00' Lt.  
Const. Type "CG-3" Conc. Inlet  
Inst. 12" Storm Sew. Pipe - 200'  
10' Depth  
S = 0.0200'/Ft.  
I.E. (12" Out) - 174.85  
(See Drg. No. RD372)
- ⑤ Sta. "A" 16+00.00, 6.00' Lt.  
Const. Type "CG-3" Conc. Inlet  
Inst. 12" Storm Sew. Pipe - 200'  
10' Depth  
S = 0.0200'/Ft.  
I.E. (12" In) - 170.85  
I.E. (12" Out) - 170.75

- ⑥ Sta. "A" 19+00.00, 6.00' Lt.  
Const. Type "CG-3" Conc. Inlet  
Inst. 12" Storm Sew. Pipe - 100'  
10' Depth  
S = 0.0140'/Ft.  
I.E. (12" Out) - 168.15
- ⑦ Adjust Manhole To Grade (Major Adjustment) - 2
- ⑧ Adjust Box to Grade
- ⑨ Const. Water Quality Swale No. 1  
(For Details, See Shts. GH4 and GH5)

**NOTES:**  
 1. Station/Offset Call-outs For Type "G-2" and "CG-3" Inlets Are To The Face of Curb.  
 Station/Offset Call-outs For Manholes Are To The Center of Structure.  
 2. Field Verify Elevation Call-outs Noted With "±".

		<b>OREGON DEPARTMENT OF TRANSPORTATION ROADWAY ENGINEERING SECTION</b>	
		<b>OR 214: WOODBURN-MT. ANGEL PHASE 2 (SIDEWALKS) HILLSBORO - SILVERTON HIGHWAY MARION COUNTY</b>	
		Design Team Leader - Clara O'Mara Designed By - George B. Smilin Drafted By - Lenda Hand	
		<b>DRAINAGE &amp; UTILITIES</b>	SHEET NO. <b>3B</b>

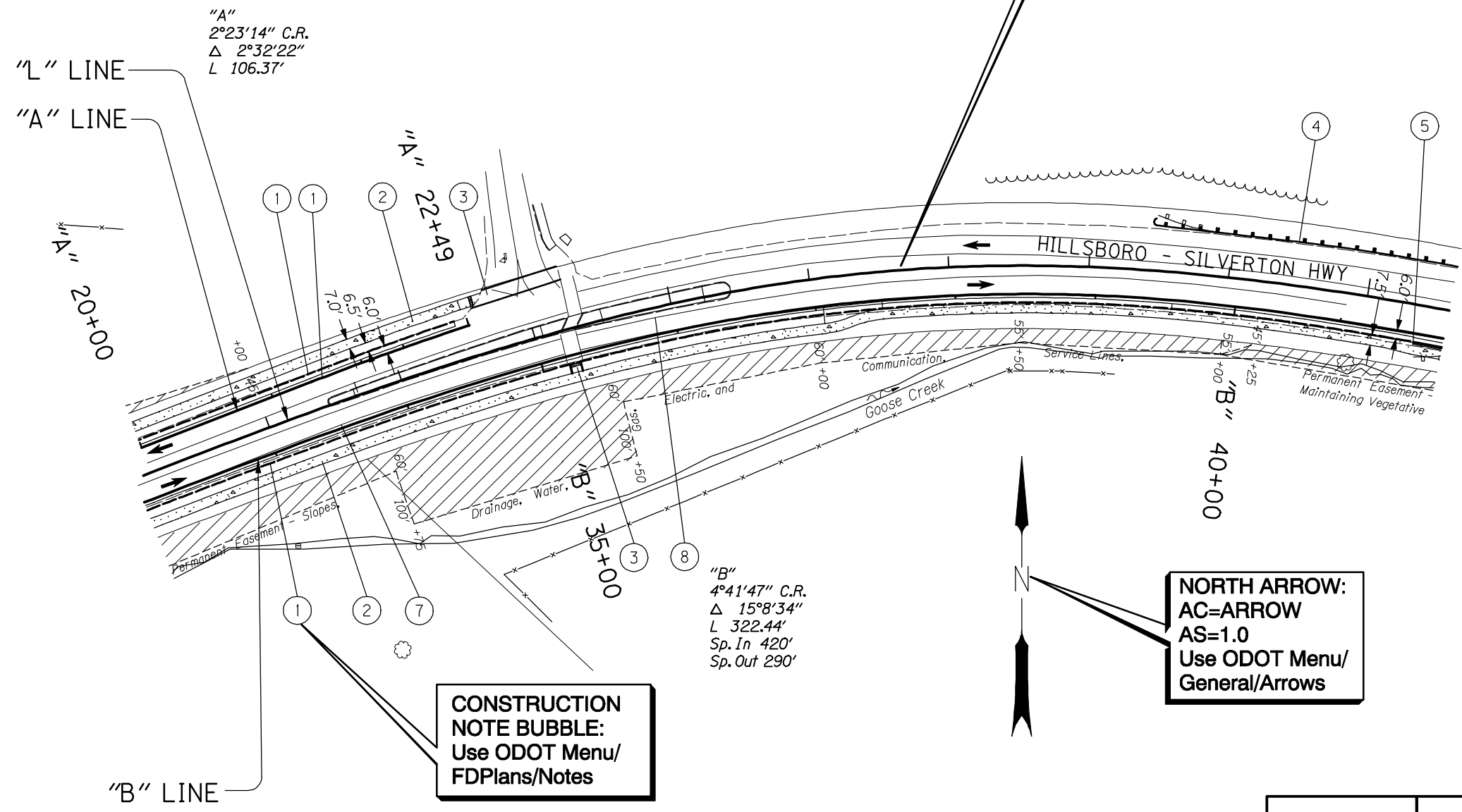
11-3

Sec. 07, T. 5 S, R. 1 W, W.M.

**SUBTITLE TEXT:**  
FT=33  
TH=11.5 CO=3  
TW=11.5 WT=0  
INT. SP.=0  
JUST=CT  
(Available From  
The Cache)

**CENTERLINE:**  
CO=3 WT=5  
LC=0

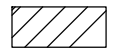
- ① Const. Curb and Gutter - 1153'  
(See Drg. No. RD700)
- ② Const. P.C. Conc. Walk - 941 Sq. Yd.  
(For Details, See Sht. 2B-4)
- ③ Const. Sidewalk Ramp - 2  
(For Details, See Sht. 2B-3)  
(See Drg. No. RD755)
- ④ Sta. "L" 1153+50 To Sta. "FR" 0+80  
Remove Extg. Guardrail - 1050'  
Const. Type 2A Guardrail - 1050'  
Radius As Noted  
Const. Guardrail Terminal,  
Non-Flared (Test Level 2) - 2  
(See Drg. Nos. RD400, RD405, RD415, RD420)
- ⑤ Sta. "B" 41+30 To Sta. "B" 43+30  
Remove Extg. Guardrail - 155'  
Const. ReflectORIZED Precast  
Conc. Shldr. Barrier - 200'  
Pin and Grout Barrier  
Inst. Conc. Barrier To Curb Transition - 2  
(See Drg. Nos. RD500 and RD540)
- ⑥ See Sht. 3, Note 4
- ⑦ Sta. "B" 32+00 To Sta. "B" 48+00  
Remove Extg. White Line (Fog Stripe) - 1600'  
See Sht. 3, Note 5
- ⑧ Sta. "B" 35+60, 12' Lt. To Sta. "B" 37+10, 12' Lt.  
Remove Extg. Yellow Line (Median Stripe) - 150'  
Place Two 4" Yellow Lines Narrow Double- 150'  
(See Drg. No. TM525)



**CONSTRUCTION  
NOTE BUBBLE:**  
Use ODOT Menu/  
FDPlans/Notes

**NORTH ARROW:**  
AC=ARROW  
AS=1.0  
Use ODOT Menu/  
General/Arrows

Easements Noted Thus Indicate Right Of Way Files That Have Not Been Acquired As Of  
Printing Of The Plans. Acquisition Date Will Be No Later Than June 10, 2004. See Specs  
For More Information.



	<b>OREGON DEPARTMENT OF TRANSPORTATION</b> <b>ROADWAY ENGINEERING SECTION</b>
	<b>OR 214: WOODBURN-MT. ANGEL</b> <b>PHASE 2 (SIDEWALKS)</b> HILLSBORO - SILVERTON HIGHWAY MARION COUNTY
	Design Team Leader - Clara O'Mara Designed By - George B. Smilin Drafted By - Lenda Hand
<b>ALIGNMENT AND GENERAL CONSTRUCTION</b>	
SHEET NO. <b>4</b>	

11-4

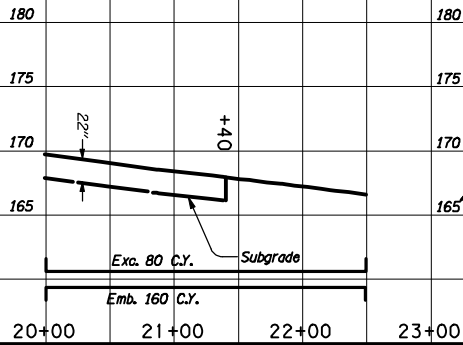
- ① Const. Curb and Gutter -- 1193'  
(See Drg. No. RD700)
- ② Const. P.C. Conc. Walk -- 941 Sq. Yd.  
(For Details, See Sht. XX)
- ③ Const. Sidewalk Ramp - 2  
(For Details, See Sht. 2B-4)  
(See Drg. No. ERD755)
- ④ Sta. "B" 34+00.00, 9.00' Rt. To Sta. "B" 34+00.00, 59' Rt.  
Inst. 12" Storm Sew. Pipe -- 40'  
S = 0.0735'/Ft.  
I.E. (12" Outfall) = 156.64  
Const. Paved End Slope -- 20 Sq. Ft.  
(See Drg. No. 320)
- ⑤ Inst. 12" Culvert Pipe -- 20'  
Connect To Extg. Culvert  
Match Extg. Slope  
Const. Paved End Slope -- 20 Sq. Ft.
- ⑥ Sta. "B" 34+00.00, 9.00' Rt.  
Const. Storm Sew. Manhole  
I.E. (12" In) = 159.83 (W)  
I.E. (12" In) = 159.83 (E)  
I.E. (12" Out) = 159.58 (S)  
(See Drg. No. ERD336)
- ⑦ Sta. "B" 33+00.00, 6.00' Rt.  
Const. Type "CG-3" Conc. Inlet  
I.E. (12" Out) = 164.33  
Inst. 12" Storm Sew. Pipe -- 100'  
S = 0.0450'/Ft.
- ⑧ Sta. "B" 35+00.00, 6.00' Rt.  
Const. Type "CG-3" Conc. Inlet  
I.E. (12" Out) = 160.33  
Inst. 12" Storm Sew. Pipe -- 100'  
S = 0.0050'/Ft.
- ⑨ Sta. "B" 38+00.00, 6.00' Rt.  
Const. Type "G-2" Conc. Inlet  
I.E. (12" Out) = 154.43  
Inst. 12" Storm Sew. Pipe -- 300'  
S = 0.0080'/Ft.  
(See Drg. No. RD364)
- ⑩ Sta. "B" 41+00.00, 6.00' Rt.  
Const. Type "G-2" Conc. Inlet  
I.E. (12" In) = 152.03  
I.E. (12" Out) = 151.93  
Inst. 12" Storm Sew. Pipe -- 100'  
S = 0.0080'/Ft.
- ⑪ Adjust Manholes To Grade (Minor Adjustment) - 3  
(See Drg. No. RD360)
- ⑫ Adjust Manholes To Grade (Major Adjustment) - 4  
(See Drg. No. RD360)
- ⑬ Sta. "B" 40+00 To Sta. "C" 71+15.40  
Const. Retaining Wall No. 1  
(For Details, See Sht. 2B)
- ⑭ Sta. "L" 1153+50 To Sta. "FR" 0+80  
Remove Extg. Guardrail - 1050'  
Const. Type 2A Guardrail - 1000'  
Radius = 75'  
Const. Guardrail Terminal,  
Non-Flared (TL-2) - 2  
(See Drg. Nos. RD400, RD405, RD415)
- ⑮ Sta. "C" 70+00 To Sta. "C" 72+00  
Const. Precast Conc. Shldr. Barrier -- 200'  
Pin and Grout Barrier  
Inst. Conc. Barrier To Curb Transition - 2  
(See Drg. Nos. RD500 and RD540)
- ⑯ Const. Water Quality Swale No. 2  
(For Details, See Sht. GH6)
- ⑰ Sta. "B" XX+XX To Sta. "B" XX+XX  
Remove Extg. White Line (Fog Stripe) - XX'  
Place 4" Nom. White Line - XX'  
(See Drg. No. TM525)
- ⑱ Sta. "B" XX+XX, 12' Lt. To Sta. "B" XX+XX, 12' Lt.  
Remove Extg. Yellow Line (Median Stripe) - XX'  
Place Two Way Left Turn, 4" Yellow Lines - XX'

**NOTES:**  
1. Station/Offset Call-outs For Type "G-2" and "CG-3" Inlets Are To The Face of Curb.  
Station/Offset Call-outs For Manholes Are To The Center of Structure.

<b>OREGON DEPARTMENT OF TRANSPORTATION</b> ROADWAY ENGINEERING SECTION	
<b>OR 214: WOODBURN-MT. ANGEL</b> <b>PHASE 2 (SIDEWALKS)</b> HILLSBORO - SILVERTON HIGHWAY MARION COUNTY	
Design Team Leader - Clara O'Mara Designed By - George B. Smilin Drafted By - Lenda Hand	
<b>GENERAL CONSTRUCTION</b>	SHEET NO. <b>4A</b>

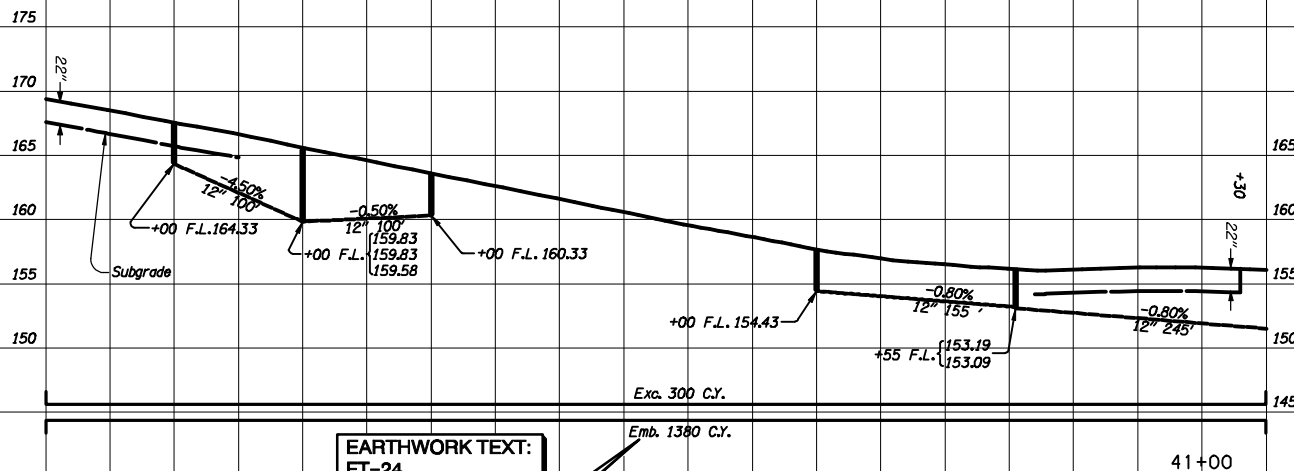
11-5

"A" LINE



ELEVATION TEXT:  
 FT=24  
 TX=7.5 CO=4  
 WT=2  
 JUST=LT

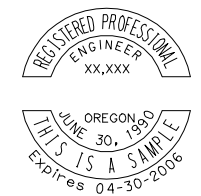
"B" LINE



EARTHWORK TEXT:  
 FT=24  
 TX=7.5 CO=4  
 LS=5 WT=2  
 JUST=CB

STATION REFERENCE:  
 FT=4 WT=2  
 TX=9.4 CO=4  
 JUST=CB

NOTE:  
 Finish Grade For Curb And Gutter Will Closely Match Ex'g.  
 Pavement And Will Be Supplied At Regular Station Intervals  
 For Construction Staking.



**OREGON DEPARTMENT OF TRANSPORTATION**  
 ROADWAY ENGINEERING SECTION

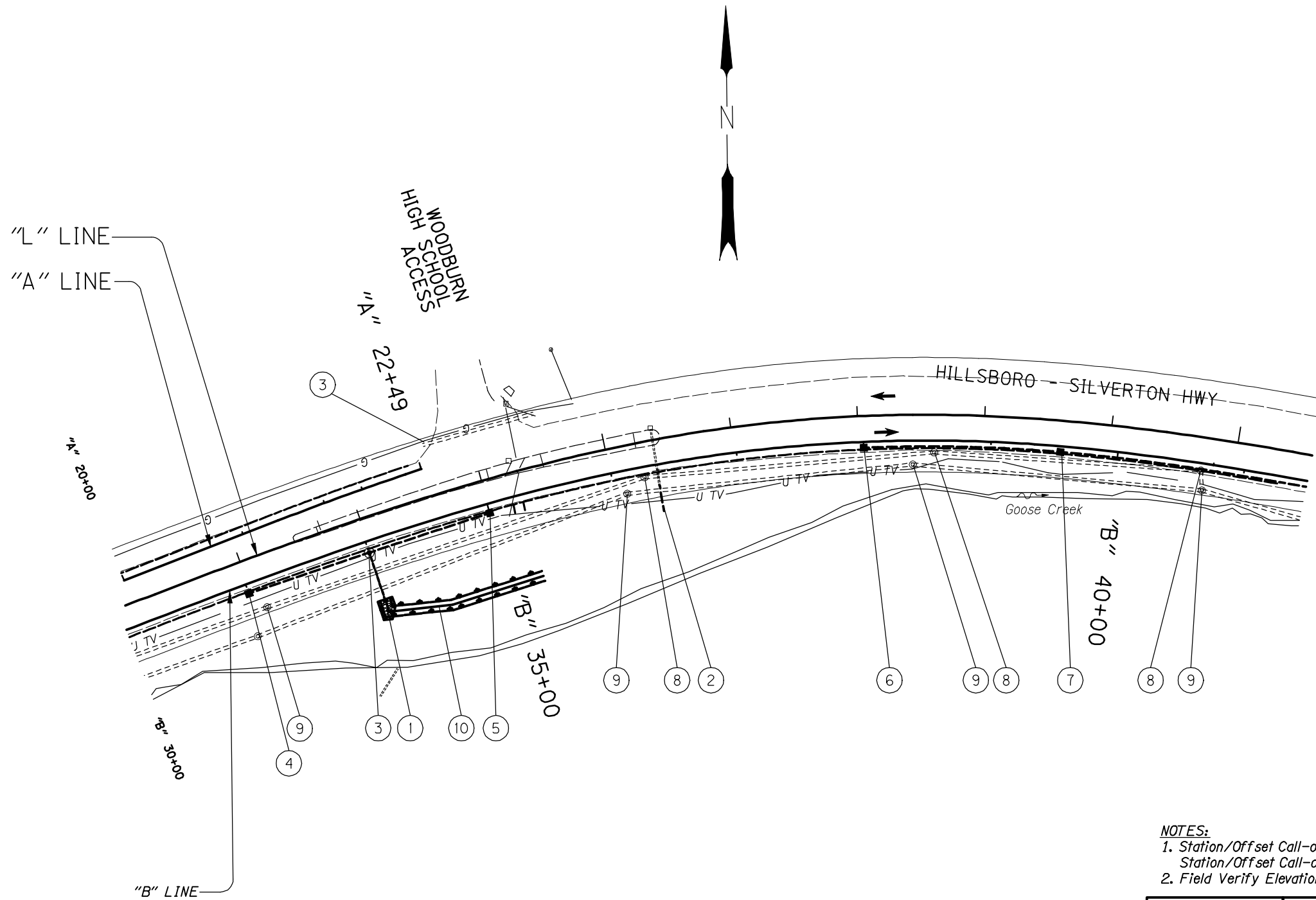
**OR 214 WOODBURN-MT. ANGEL**  
**PHASE 2 (SIDEWALKS)**  
 HILLSBORO - SILVERTON HIGHWAY  
 MARION COUNTY

Design Team Leader - Clara O'Mara  
 Designed By - George B. Smith  
 Drafted By - Lenda Hand

**PROFILES**

SHEET NO. **4B**

11-6



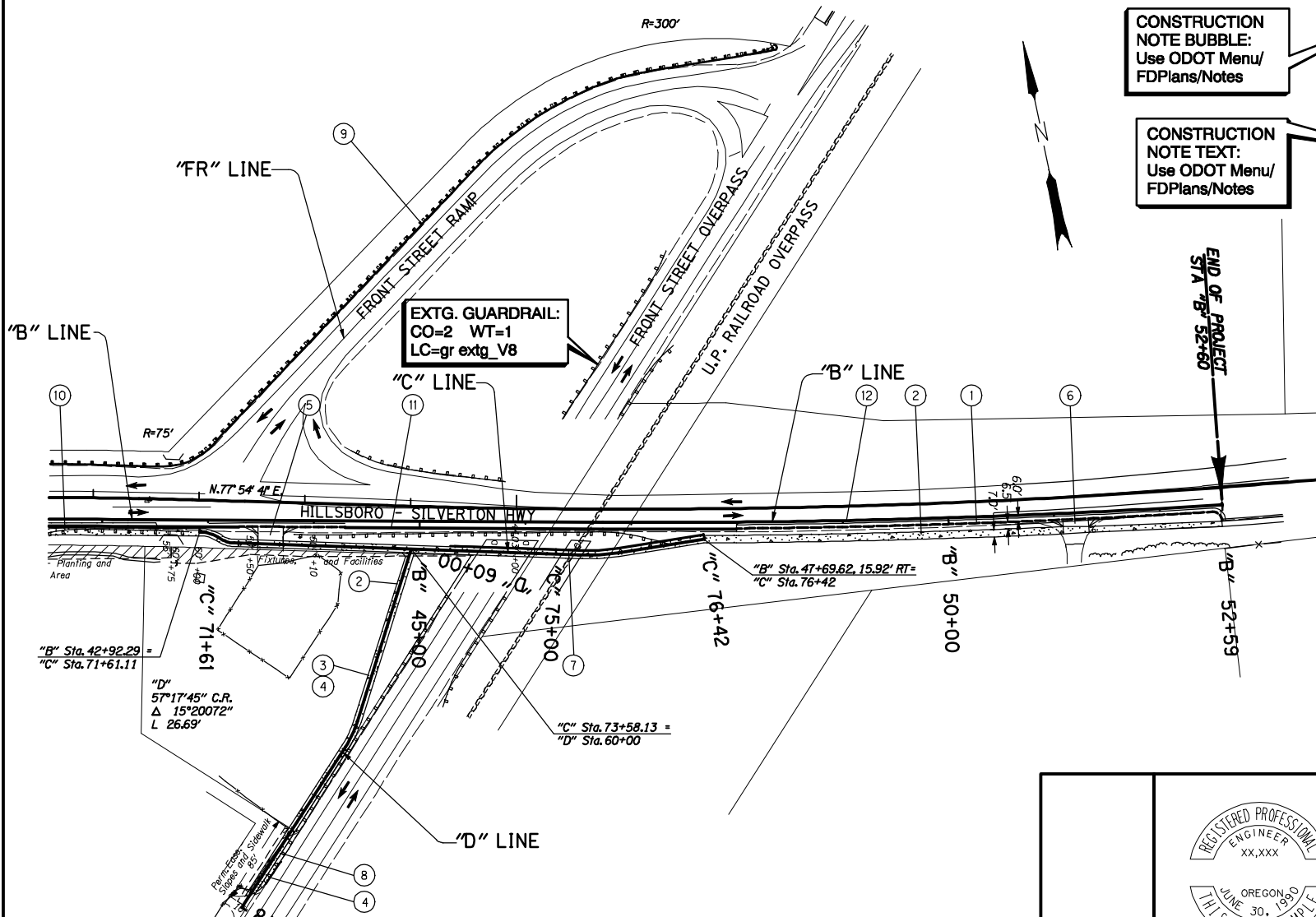
- ① Sta. "B" 34+00.00, 9' Rt. To Sta. "B" 34+00.00, 47' Rt.  
Inst. 12" Storm Pipe - 38'  
10' Depth  
S = 0.0763'/Ft.  
I.E. (12" Outfall) = 156.68  
Const. Paved End Slope - 20 Sq. Ft.  
(See Drg. No. RD320)
- ② Inst. 12" Culvert Pipe - 20'  
5' Depth  
Connect To Extg. Culvert  
Match Extg. Slope  
Const. Paved End Slope - 20 Sq. Ft.
- ③ Sta. "B" 34+00.00, 9.00' Rt.  
Const. Storm Manhole  
Rim Elev. = 166.0±  
I.E. (12" In) = 159.83 (W)  
I.E. (12" In) = 159.83 (E)  
I.E. (12" Out) = 159.58 (S)  
(See Drg. No. RD336)
- ④ Sta. "B" 33+00.00, 6.00' Rt.  
Const. Type "CG-3" Conc. Inlet  
Inst. 12" Storm Pipe - 100'  
10' Depth  
S = 0.0450'/Ft.  
I.E. (12" Out) = 164.33
- ⑤ Sta. "B" 35+00.00, 6.00' Rt.  
Const. Type "CG-3" Conc. Inlet  
Inst. 12" Storm Pipe - 100'  
10' Depth  
S = 0.0050'/Ft.  
I.E. (12" Out) = 160.33
- ⑥ Sta. "B" 38+00.00, 6.00' Rt.  
Const. Type "G-2" Conc. Inlet  
Inst. 12" Storm Pipe - 155'  
5' Depth  
S = 0.0080'/Ft.  
I.E. (12" Out) = 154.43  
(See Drg. No. RD364)

**NOTES:**  
 1. Station/Offset Call-outs For Type "G-2" and "CG-3" Inlets Are To The Face of Curb.  
 Station/Offset Call-outs For Manholes Are To The Center of Structure.  
 2. Field Verify Elevation Call-outs Noted With "±".

- ⑨ Adjust Manholes to Grade (Major Adjustment) - 4  
(See Drg. No. RD338)
- ⑩ Const. Water Quality Swale No. 2  
(For Details, See Shts. GH4 And GH6)
- ⑪ Plug And Abandon Exist. Culvert
- ⑦ Sta. "B" 39+55.00, 6.00' Rt.  
Const. Type "G-2" Conc. Inlet  
Inst. 12" Storm Pipe - 245'  
5' Depth  
S = 0.0080'/Ft.  
I.E. (12" In) = 153.19  
I.E. (12" Out) = 153.09
- ⑧ Adjust Manholes to Grade (Minor Adjustment) - 3  
(See Drg. No. RD338)

<b>OREGON DEPARTMENT OF TRANSPORTATION</b> ROADWAY ENGINEERING SECTION	
<b>OR 214: WOODBURN-MT. ANGEL</b> <b>PHASE 2 (SIDEWALKS)</b> HILLSBORO - SILVERTON HIGHWAY MARION COUNTY	
Design Team Leader - Clara O'Mara Designed By - George B. Smilin Drafted By - Lenda Hand	
<b>DRAINAGE &amp; UTILITIES</b>	SHEET NO. <b>4C</b>

11-7



**CONSTRUCTION NOTE BUBBLE:**  
Use ODOT Menu/  
FDPlans/Notes

**CONSTRUCTION NOTE TEXT:**  
Use ODOT Menu/  
FDPlans/Notes

- 1 Const. Curb and Gutter - 510'  
(See Drg. No. RD700)
- 2 Const. P.C. Conc. Walk - 1102 Sq.Yd.  
(For Details, See Shts. 2B-3 & 2B-4)
- 3 Sta. "D" 60+20 To Sta. "D" 63+65  
Const. Monolithic Sidewalk & Curtain  
Walls, Reinforced  
Structural Excavation = 450 C.Y.  
(For Details, See Sht. 2A-3)
- 4 Sta. "D" 61+00 To Sta. "D" 63+00  
Cont. Pedestrian Handrail - 200'  
(For Details, See Sht. 2B-6)
- 5 Const. P.C. Conc. Driveway  
W = 24'  
(For Details, See Sht. 2B-4)
- 6 Const. P.C. Conc. Driveway  
W = 24'  
(For Details, See Sht. 2B-4)
- 7 Sta. "C" 74+00 To Sta. "C" 76+20  
Const. MSE Retaining Wall - 1120 Sq. Ft.  
Structural Excavation = 150 C.Y.  
Const. Pedestrian Handrail - 220'  
(For Details, See Shts. 2B & 2B-6)
- 8 Sta. "D" 63+20 To Sta. "D" 63+45  
Remove Extg. Guardrail - 37.5'  
Inst. Non-Flared Terminal  
Test Level 2  
Connect To Extg. Guardrail  
(See Drg. No. RD420)
- 9 See Sht. 4, Note 4
- 10 See Sht. 4, Note 5
- 11 Sta. "B" 44+25 To Sta. "B" 48+00  
Remove Extg. Guardrail - 355'  
Const. Reflectorized Precast Conc. Shldr. Barrier  
375'  
Pin and Grout Barrier  
Inst. Conc. Barrier To Curb Transition - 2
- 12 See Sht. 3, Note 5 & Sht. 4, Note 7

**EXTG. GUARDRAIL:**  
CO=2 WT=1  
LC=gr extg\_V8

Easements Noted Thus Indicate Right Of Way Files That Have Not Been Acquired As Of  
Printing Of The Plans. Acquisition Date Will Be No Later Than June 10, 2004. See Specs  
For More Information.

REGISTERED PROFESSIONAL  
ENGINEER  
XX,XXX

JUNE 30, 1990  
OREGON  
THIS IS A SAMPLE  
Expires 04-30-2006

**OREGON DEPARTMENT OF TRANSPORTATION**  
ROADWAY ENGINEERING SECTION

**OR 214: WOODBURN-MT. ANGEL**  
**PHASE 2 (SIDEWALKS)**  
HILLSBORO - SILVERTON HIGHWAY  
MARION COUNTY

Design Team Leader - Clara O'Mara  
Designed By - George B. Smilin  
Drafted By - Lenda Hand

**ALIGNMENT AND  
GENERAL CONSTRUCTION**

SHEET  
NO.  
**5**


11-8



- ① Const. Curb and Gutter -- 463 Ft.  
(See Drg. No. RD700)
- ② Merry Christmas
- ③ Const. P.C. Conc. Walk -- 1102 Sq.Yd.  
(For Details, See Sht. XX)
- ④ Const. Monolithic P.C. Conc. Walk  
Inst. 6" Drain Pipe -- XX Ft.  
(For Details, See Sht. 2A)
- ⑤ Const. Sidewalk Ramp  
(For Details, See Sht. 2B-4)
- ⑥ Const. P.C. Conc. Driveway  
W = 24 Ft.  
(See Drg. No. ERD725, Option A)
- ⑦ Const. P.C. Conc. Driveway  
W = 24 Ft.  
(For Details, See Sht. 2B-5)
- ⑧ Sta. "B" 42+95.00, 32' Rt. To Sta. "B" 42+70.00, 33' Rt.  
Inst. 12" Storm Pipe -- 23 Ft.  
S = 0.1684 Ft./Ft.  
I.E. (12" Outfall) = 147.00±  
Const. Paved End Slope -- 20 Sq. Ft.  
(See Drg. No. RD320)
- ⑨ Sta. "B" 42+50.00, XX' Rt. To Sta. "B" 42+56.00, 16' Rt.  
Inst. 12" Storm Pipe -- 10 Ft.  
S = 0.3633 Ft./Ft.  
I.E. (12" Outfall) = 147.00±  
Const. Paved End Slope -- 20 Sq. Ft.
- ⑩ Sta. "B" 42+50.00, XX' Rt.  
Const. Water Quality Manhole  
I.E. (12" In) = 150.63  
I.E. (12" Out) = 150.38
- ⑪ Sta. "B" 42+70.00, 33' Rt.  
Const. Storm Manhole  
I.E. (12" In) = 151.12  
I.E. (12" Out) = 150.87  
(See Drg. No. \_\_\_\_\_)
- ⑫ Sta. "B" 42+00, 6' Rt.  
Const. Type "G-2" Conc. Inlet  
I.E. (12" In) = 151.13 (W)  
I.E. (12" Out) = 151.03 (E)  
Inst. 12" Storm Pipe -- 50 Ft.  
S = 0.0080 Ft./Ft.  
(See Drg. No. RD366)
- ⑬ Sta. "B" 43+06.00, 6' Rt.  
Const. Type "G-2" Conc. Inlet  
I.E. (12" Out) = 151.98  
Inst. 12" Storm Pipe -- 15 Ft.  
S = 0.0100 Ft./Ft.
- ⑭ Sta. "B" 43+21.00, 6' Rt.  
Const. Type "G-2" Conc. Inlet  
I.E. (12" In) = 151.83 (W)  
I.E. (12" In) = 151.59 (E)  
I.E. (12" Out) = 151.49 (SW)  
Inst. 12" Storm Pipe -- 37 Ft.  
S = 0.0100 Ft./Ft.
- ⑮ Sta. "B" 43+36.00, 6' Rt.  
Const. Type "G-2" Conc. Inlet  
I.E. (12" Out) = 151.74  
Inst. 12" Storm Pipe -- 15 Ft.  
S = 0.0100 Ft./Ft.
- ⑯ Bridge No. 19241  
Sta. "C" 71+15.40 To Sta. "C" 71+55.40  
(For Drg. Nos., See Sht. 1A)
- ⑰ See Sht. 4A, Note 13
- ⑱ Sta. "C" 74+00 To Sta. "C" 76+40  
Const. Retaining Wall No. 2 -- XX Sq. Ft.  
(For Details, See Sht. 2B-2)
- ⑲ Sta. "D" 63+00 to Sta. "D" 63+25  
Remove Extg. Guardrail -- 60 Ft.  
Inst. Non-Flared Terminal  
Test Level 2  
(See Drg. No. ERD420)
- ⑳ See Sht. 4A, Note 14
- ㉑ See Sht. 4A, Note 15
- ㉒ Sta. "C" 73+00 To Sta. "C" 76+25  
Const. Precast Conc. Shldr. Barrier - 325 Ft.  
Pin and Grout Barrier  
Inst. Conc. Barrier to Curb Transition - 2  
(See Drg. Nos. RD500 and RD540)

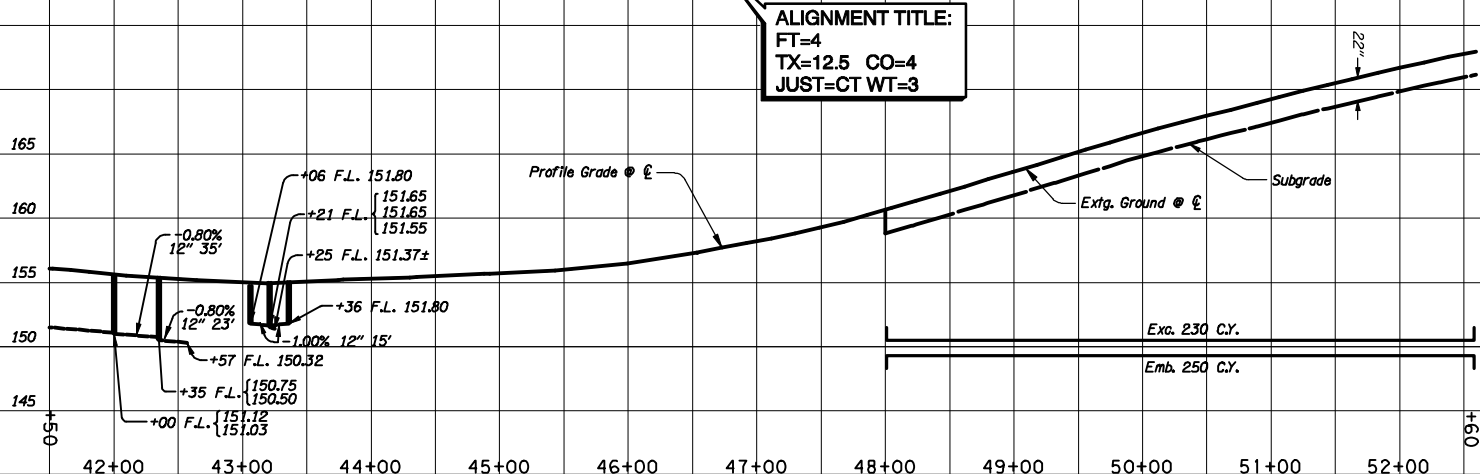
**NOTES:**

- 1. Station/Offset Call-outs For Type "G-2" and "CG-3" Inlets Are To The Face of Curb.  
Station/Offset Call-outs For Manholes Are To The Center of Structure.

		 <b>OREGON DEPARTMENT OF TRANSPORTATION</b> <b>ROADWAY ENGINEERING SECTION</b>
		<b>OR 214: WOODBURN-MT. ANGEL</b> <b>PHASE 2 (SIDEWALKS)</b> HILLSBORO - SILVERTON HIGHWAY MARION COUNTY
		Design Team Leader - Clara O'Mara Designed By - George B. Smilin Drafted By - Lenda Hand
		<b>GENERAL CONSTRUCTION</b>
		SHEET NO. <b>5A</b>

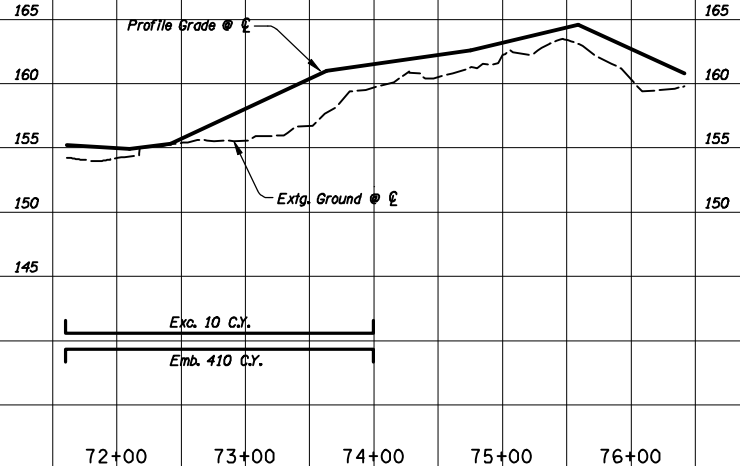
"B" LINE

ALIGNMENT TITLE:  
 FT=4  
 TX=12.5 CO=4  
 JUST=CT WT=3

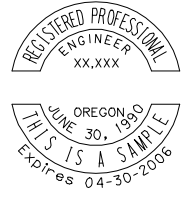
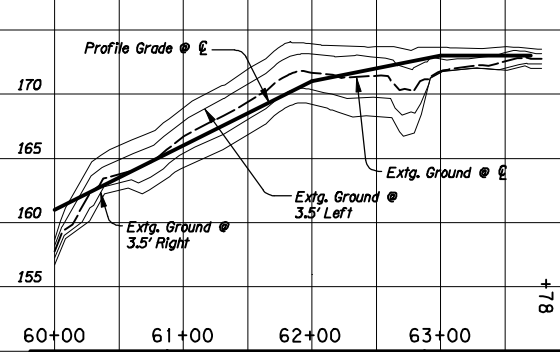


NOTE:  
 Finish Grade For Curb And Gutter Will  
 Closely Match Extg. Pavement And Will  
 Be Supplied At Regular Station Intervals  
 For Construction Staking.

"C" LINE



"D" LINE



**OREGON DEPARTMENT OF TRANSPORTATION  
 ROADWAY ENGINEERING SECTION**

**OR 214: WOODBURN-MT. ANGEL  
 PHASE 2 (SIDEWALKS)  
 HILLSBORO - SILVERTON HIGHWAY  
 MARION COUNTY**

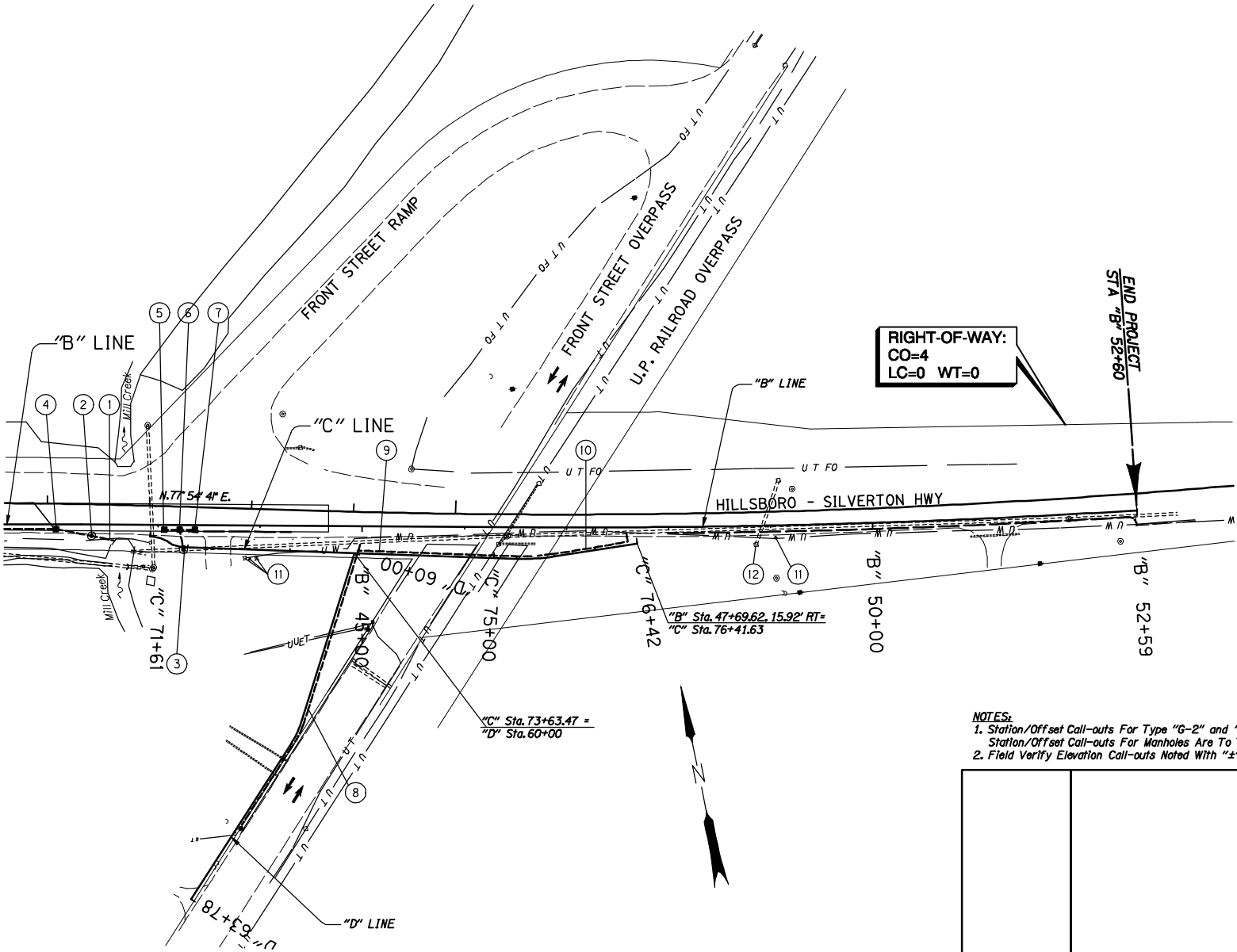
Design Team Leader - Clara O'Mara  
 Designed By - George B. Smilin  
 Drafted By - Lenda Hand

**PROFILES**

SHEET NO. **5B**

11-10

Sec. 07, T. 5 S, R. 1 W, W.M.



RIGHT-OF-WAY:  
CO=4  
LC=0 WT=0

- NOTES:**  
 1. Station/Offset Call-outs For Type "G-2" and "CG-3" Inlets Are To The Face Of Curb.  
 Station/Offset Call-outs For Manholes Are To The Center Of Structure.  
 2. Field Verify Elevation Call-outs Noted With "+/-".

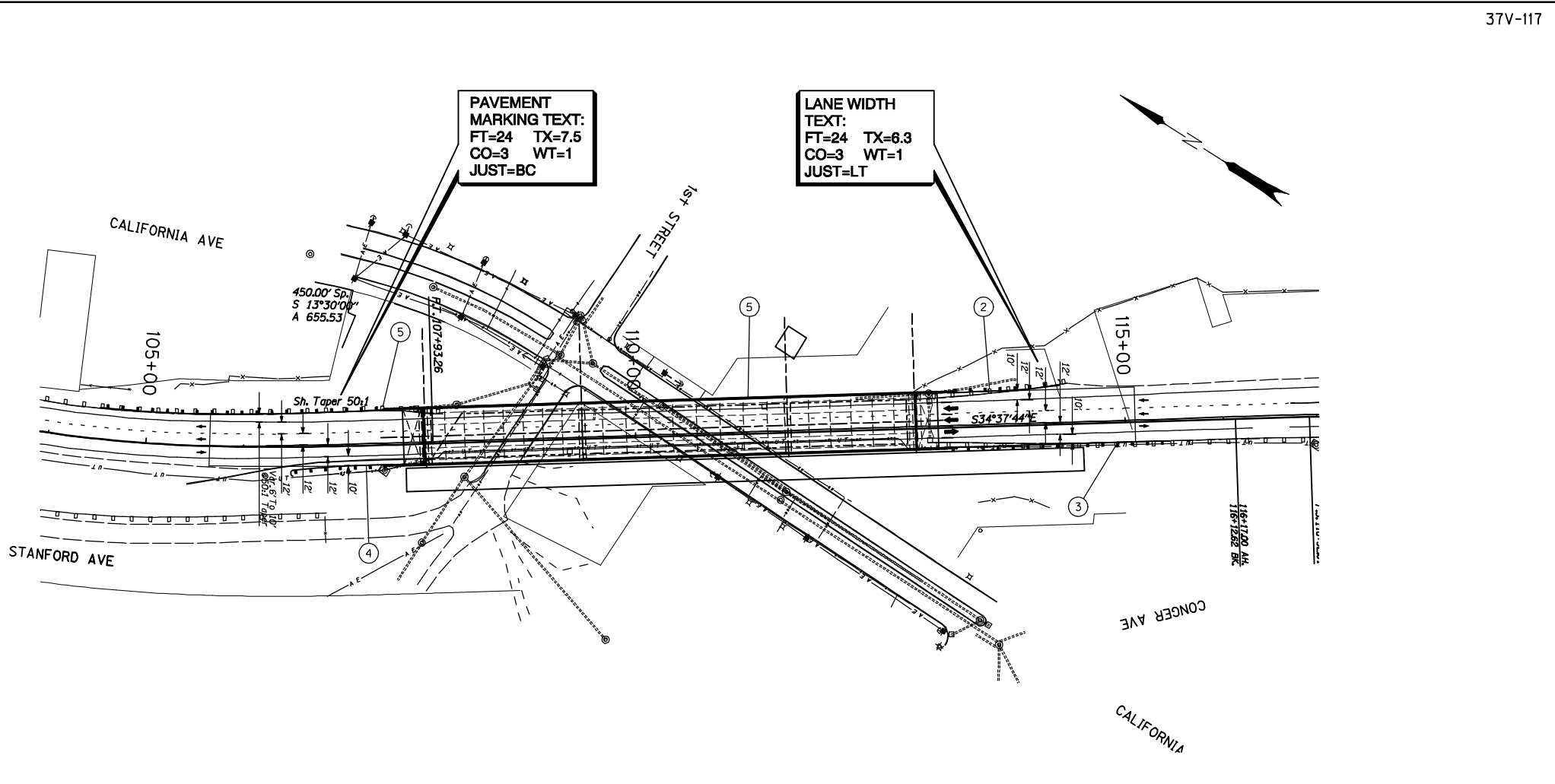
OREGON DEPARTMENT OF TRANSPORTATION ROADWAY ENGINEERING SECTION	
OR 214: WOODBURN-MT. ANGEL PHASE 2 (SIDEWALKS) HILLSBORO - SILVERTON HIGHWAY MARION COUNTY	
Design Team Leader - Clara O'Mara Designed By - George B. Smith Drafted By - Lenda Hand	
<b>DRAINAGE &amp; UTILITIES</b>	SHEET NO. 5C

11-11

- ① Sta. "B" 42+35.00, 11' Rt. To Sta. "B" 42+57.00, 15.7' Rt.  
Inst. 12" Storm Sew. Pipe - 23'  
5' Depth  
S = 0.0080'/Ft.  
I.E. (12" Outfall) - 150.32  
Connect Storm Sew. Pipe to Extg. Reinf. Conc. Wingwall  
(For Details, See Sht. 2B-7)
- ② Sta. "B" 42+35.00, 11' Rt.  
Const. Water Quality Manhole  
Rim Elev. = 155.80±  
I.E. (12" In) - 150.75  
I.E. (12" Out) - 150.50
- ③ Sta. "B" 43+25.00, 24' Rt.  
Const. Storm Sew. Manhole Over Extg. Storm Sewer  
Pothole To Locate Exist. 24" Conc. Pipe  
I.E. (12" In) - 151.37±  
I.E. (24" In) - Field Verify  
I.E. (24" Out) - Field Verify
- ④ Sta. "B" 42+00.6' Rt.  
Const. Type "G-2" Conc. Inlet  
Inst. 12" Storm Sew. Pipe - 35'  
5' Depth  
S = 0.0080'/Ft.  
I.E. (12" In) - 151.13 (W)  
I.E. (12" Out) - 151.03 (E)  
(See Drg. No. RD366)
- ⑤ Sta. "B" 43+06.00, 6' Rt.  
Const. Type "G-2" Conc. Inlet  
Inst. 12" Storm Sew. Pipe - 15'  
5' Depth  
S = 0.0100'/Ft.  
I.E. (12" Out) - 151.80
- ⑥ Sta. "B" 43+21.00, 6' Rt.  
Const. Type "G-2" Conc. Inlet  
Inst. 12" Storm Sew. Pipe - 18'  
5' Depth  
S = 0.0100'/Ft.±  
I.E. (12" In) - 151.65 (W)  
I.E. (12" In) - 151.65 (E)  
I.E. (12" Out) - 151.55 (SW)
- ⑦ Sta. "B" 43+36.00, 6' Rt.  
Const. Type "G-2" Conc. Inlet  
Inst. 12" Storm Sew. Pipe - 15'  
5' Depth  
S = 0.0100'/Ft.  
I.E. (12" Out) - 151.80
- ⑧ Inst. 6" Drain Pipe - 300'  
Connect To Extg. 24" Conc. Pipe  
Using "Core Drill" Method  
Pothole To Locate Extg. 24" Conc. Pipe
- ⑨ Inst. 6" Drain Pipe - 180'  
Connect To Extg. 24" Conc. Pipe  
Using "Core Drill" Method  
Pothole To Locate Extg. 24" Conc. Pipe
- ⑩ Inst. 6" Drain Pipe - 100'  
Connect To Extg. 24" Conc. Pipe  
Using "Core Drill" Method  
Pothole To Locate Extg. 24" Conc. Pipe
- ⑪ Adjust Boxes To Grade - 4
- ⑫ Adjust Cast-In-Place Inlet To Grade

		<b>T OREGON DEPARTMENT OF TRANSPORTATION ROADWAY ENGINEERING SECTION</b>	
		<b>OR 214: WOODBURN-MT. ANGEL PHASE 2 (SIDEWALKS) HILLSBORO - SILVERTON HIGHWAY MARION COUNTY</b>	
		Design Team Leader - Clara O'Mara Designed By - George B. Smith Drafted By - Lenda Hand	
		<b>DRAINAGE &amp; UTILITIES NOTES</b>	SHEET NO. <b>5D</b>

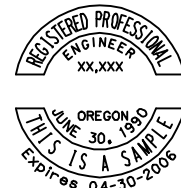
11-12



11-13

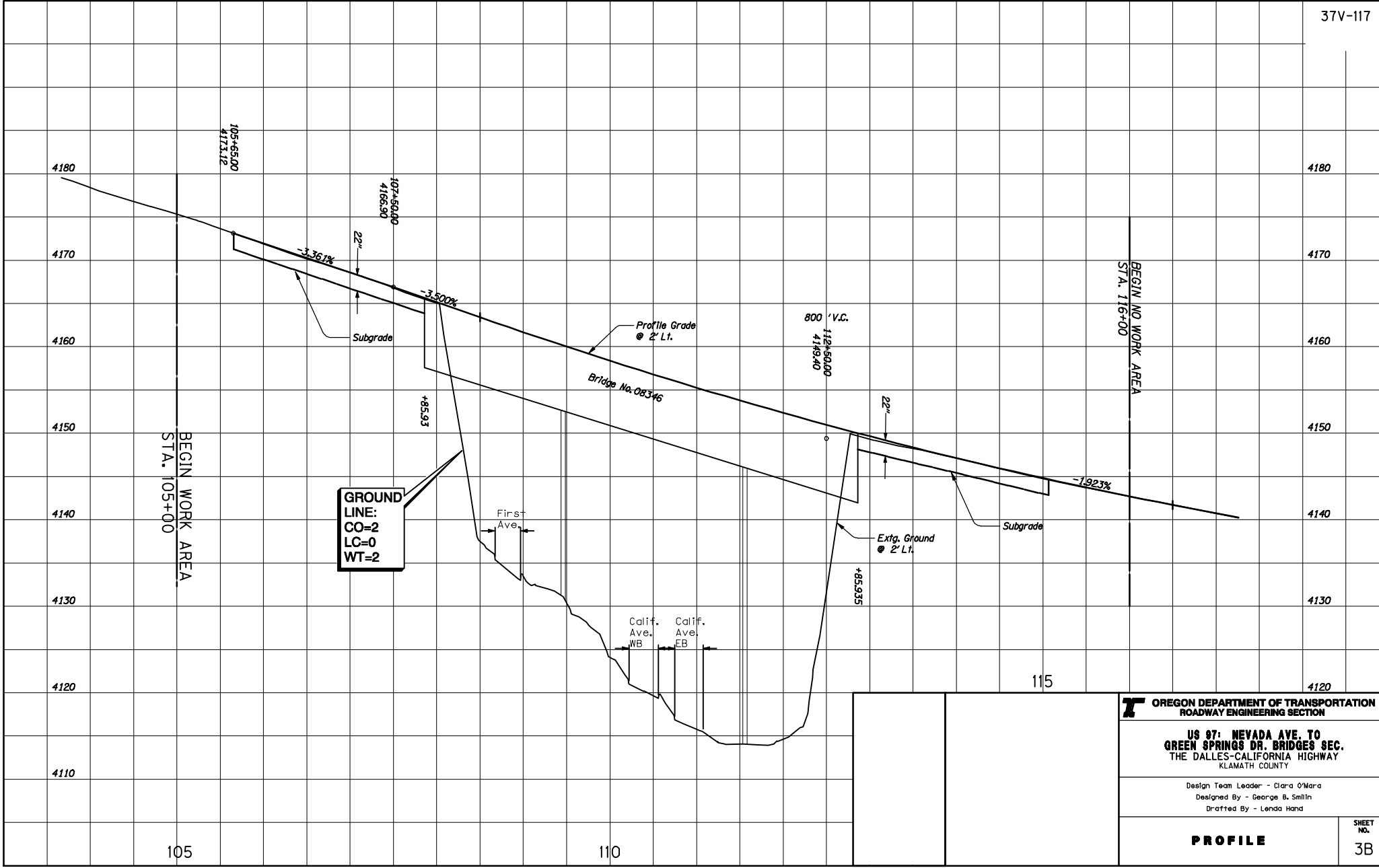
	<b>OREGON DEPARTMENT OF TRANSPORTATION</b> ROADWAY ENGINEERING SECTION
	<b>US 97: NEVADA AVE. TO GREEN SPRINGS DR. BRIDGES SEC.</b> THE DALLES-CALIFORNIA HIGHWAY KLAMATH COUNTY  Design Team Leader - Clara O'Mara Designed By - George B. Smlin Drafted By - Lenda Hand
<b>GENERAL CONSTRUCTION</b>	
SHEET NO. <b>3</b>	

- ① Sta. 104+50 To Sta. 107+50  
Remove Extg. Guardrail - 320'  
Const. Guardrail - 12.5' (Type 2A)  
                                  - 287.5' (Type 3)  
Const. Guardrail Transition  
Flare Rate 0, W=0, E=2'  
Connect to Extg. Guardrail  
(See Drg. Nos. RD400, RD405, RD415, RD440)
- ② Sta. 113+20 To Sta. 114+32.50  
Remove Extg. Guardrail - 150'  
Const. Guardrail - 12.5' (Type 2A)  
                                  - 62.5' (Type 3)  
Const. Guardrail Transition  
Flare Rate 0, W=4', E=2.0'  
Const. Guardrail Terminal, Flared (L=37.5')  
(See Drg. No. RD425)
- ③ Sta. 113+20 To Sta. 115+45  
Remove Extg. Guardrail - 220'  
Const. Guardrail - 12.5' (Type 2A)  
                                  - 212.5' (Type 3)  
Const. Guardrail Transition  
Flare Rate 0, W=4', E=2.0'  
Connect to Extg. Guardrail
- ④ Sta. 107+50 To Sta. 106+50  
Remove Extg. Guardrail - 250'  
Const. Guardrail - 12.5' (Type 2A)  
                                  - 37.5' (Type 3)  
Flare Rate 0, W=4', E=2.0'  
Const. Guardrail Transition  
Const. Guardrail Terminal, Flared (L=37.5')
- ⑤ Bridge No. 08346  
(For Details, See Sht. 1A)



<b>OREGON DEPARTMENT OF TRANSPORTATION ROADWAY ENGINEERING SECTION</b>	
<b>US 97: NEVADA AVE. TO GREEN SPRINGS DR. BRIDGES SEC. THE DALLES-CALIFORNIA HIGHWAY KLAMATH COUNTY</b>	
Design Team Leader - Clara O'Mara Designed By - George B. Smith Drafted By - Lenda Hand	
<b>CONSTRUCTION NOTES</b>	SHEET NO. <b>3A</b>

11-14



BEGIN WORK AREA  
STA. 105+00

BEGIN NO WORK AREA  
STA. 116+00

GROUND  
LINE:  
CO=2  
LC=0  
WT=2

Bridge No. 08346

800' V.C.  
112+50.00  
4169.40

115

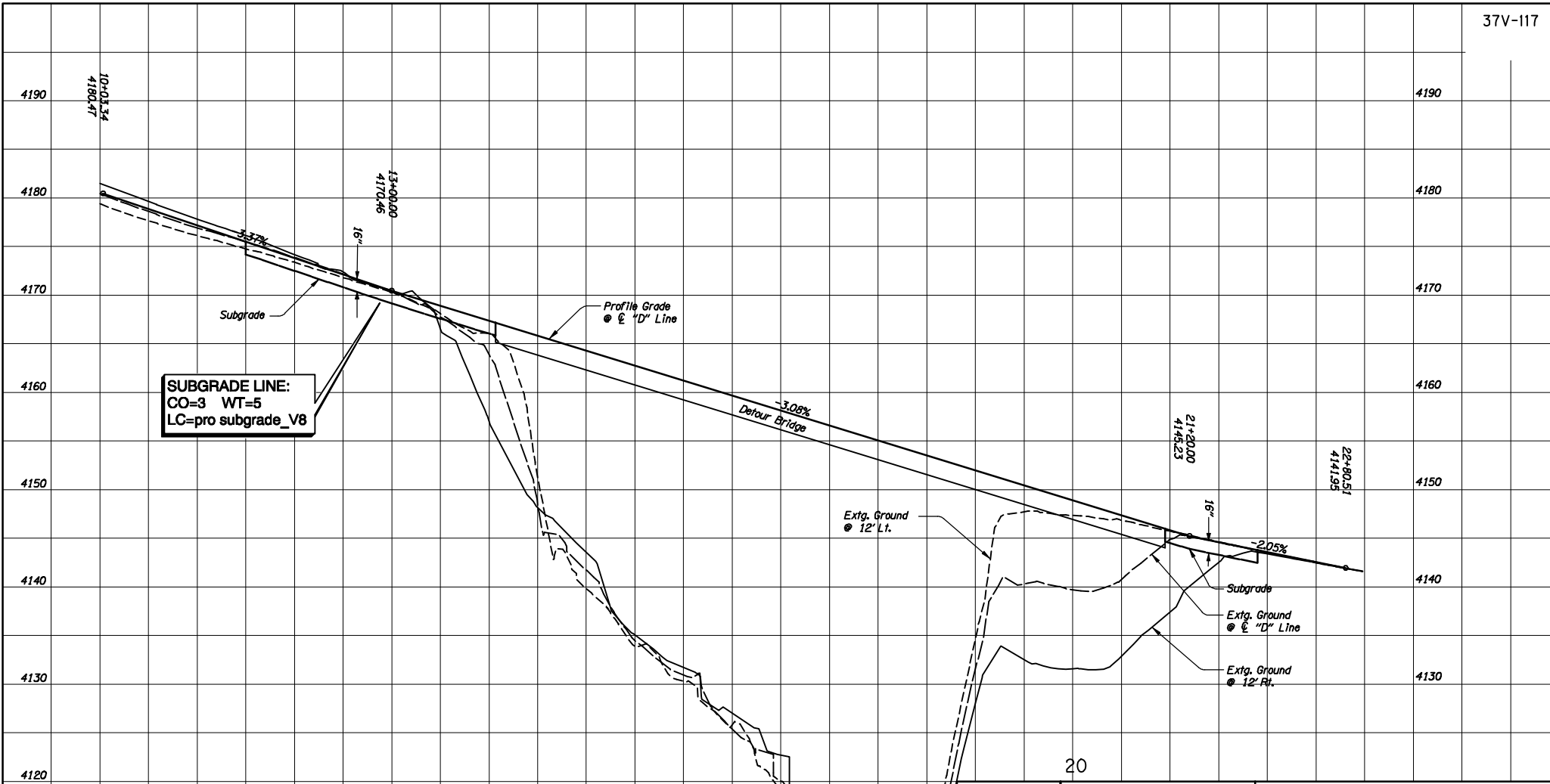
**OREGON DEPARTMENT OF TRANSPORTATION**  
ROADWAY ENGINEERING SECTION

**US 97: NEVADA AVE. TO  
GREEN SPRINGS DR. BRIDGES SEC.**  
THE DALLES-CALIFORNIA HIGHWAY  
KLAMATH COUNTY

Design Team Leader - Clara O'Mara  
Designed By - George B. Smith  
Drafted By - Lenda Hand

**PROFILE**

SHEET  
NO.  
**3B**



SUBGRADE LINE:  
 CO=3 WT=5  
 LC=pro subgrade\_V8

**OREGON DEPARTMENT OF TRANSPORTATION**  
 ROADWAY ENGINEERING SECTION

**US 97: NEVADA AVE. TO GREEN SPRINGS DR. BRIDGES SEC.**  
 THE DALLES-CALIFORNIA HIGHWAY  
 KLAMATH COUNTY

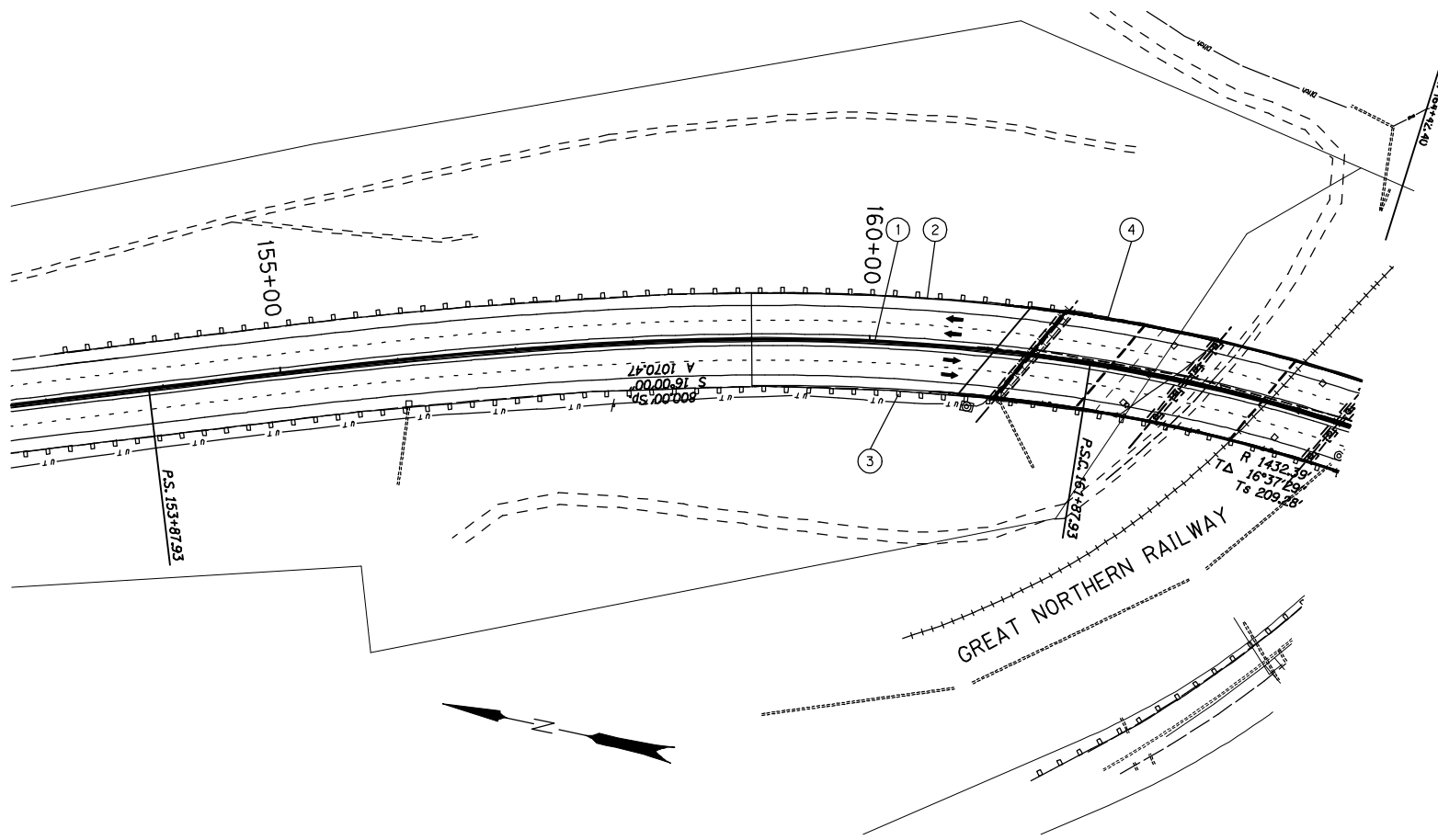
Design Team Leader - Clara O'Mara  
 Designed By - George B. Smilin  
 Drafted By - Lenda Hand

**PROFILE**

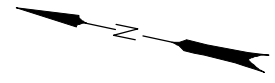
SHEET NO. 3C

11-16



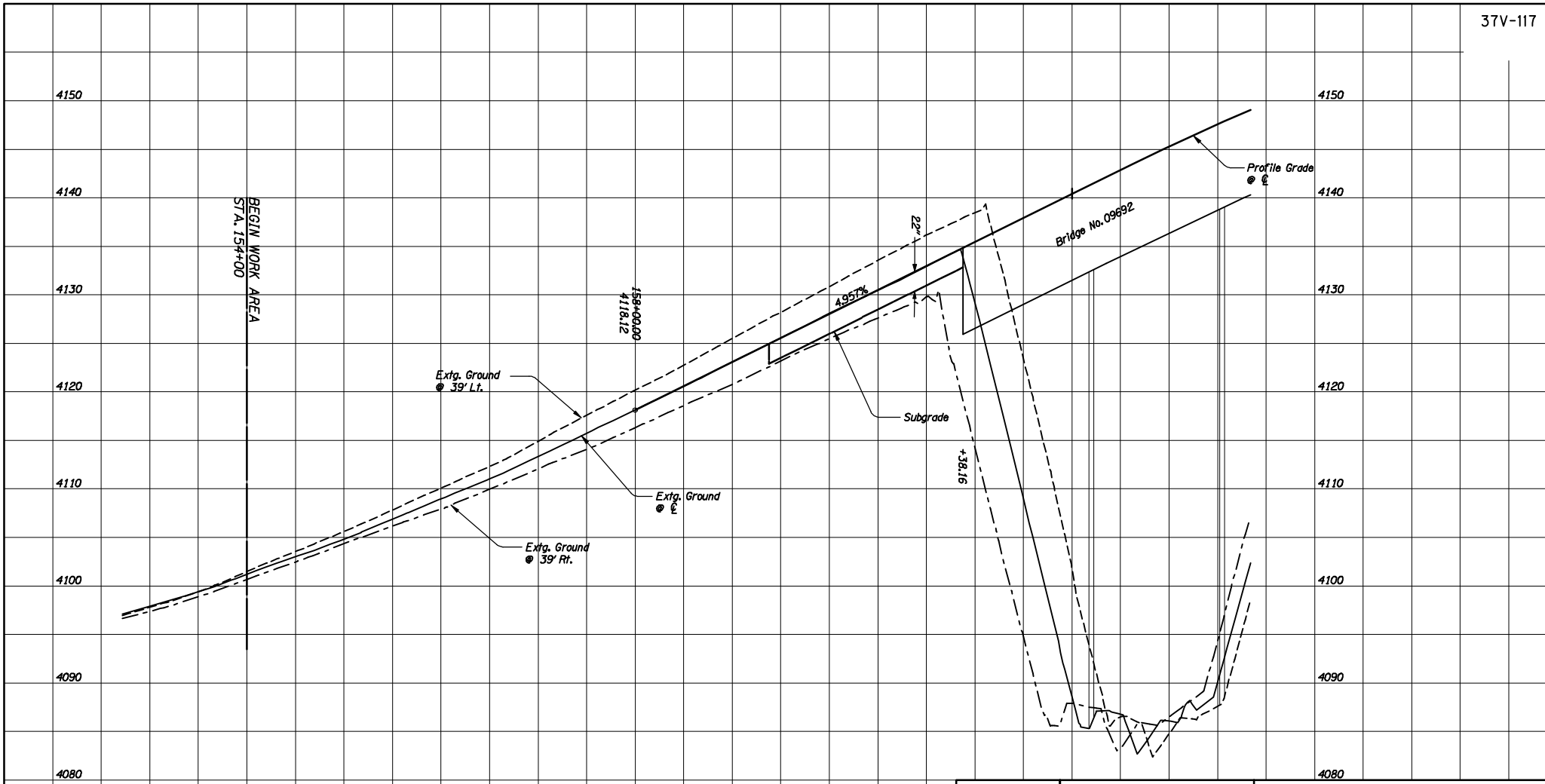


- ① Sta. 154+00 to Sta. 174+00  
Remove Extg. Concrete Barrier - 2000'  
Const. ReflectORIZED Pre-Cast Concrete  
Median Barrier - 2000'  
Const. Concrete Barrier Transition - 2  
(For Details, See Sht. 2B)  
(See Drg. No. RD500)
- ② Sta. 159+46 To Sta. 161+59  
Remove Extg. Guardrail - 212'  
Const. Guardrail - 212.5' (Type 2A)  
Const. Bridge Connection  
Flare Rate=0, W=0, E=2'  
(See Drg. No. BR236)  
Connect to Extg. Guardrail
- ③ Sta. 159+46 To Sta. 160+87.84  
Remove Extg. Guardrail - 160'  
Const. Guardrail - 162.5' (Type 2A)  
Const. Guardrail - 12.5' (Type 3)  
Const. Guardrail Transition  
Flare Rate=0, W=0, E=2'  
Connect to Extg. Guardrail
- ④ Bridge No. 09692  
(For Details, See Sht. 1A)



	<b>OREGON DEPARTMENT OF TRANSPORTATION</b> ROADWAY ENGINEERING SECTION  <b>US 97: NEVADA AVE. TO</b> <b>GREEN SPRINGS DR. BRIDGES SEC.</b> THE DALLES-CALIFORNIA HIGHWAY KLAMATH COUNTY  Design Team Leader - Clara O'Mara Designed By - George B. Smilin Drafted By - Lenda Hand
	<b>GENERAL CONSTRUCTION</b>
	SHEET NO. <b>4</b>

11-17



**OREGON DEPARTMENT OF TRANSPORTATION**  
ROADWAY ENGINEERING SECTION

**US 97: NEVADA AVE. TO GREEN SPRINGS DR. BRIDGES SEC.**  
THE DALLES-CALIFORNIA HIGHWAY  
KLAMATH COUNTY

Design Team Leader - Clara O'Mara  
Designed By - George B. Smith  
Drafted By - Londa Hand

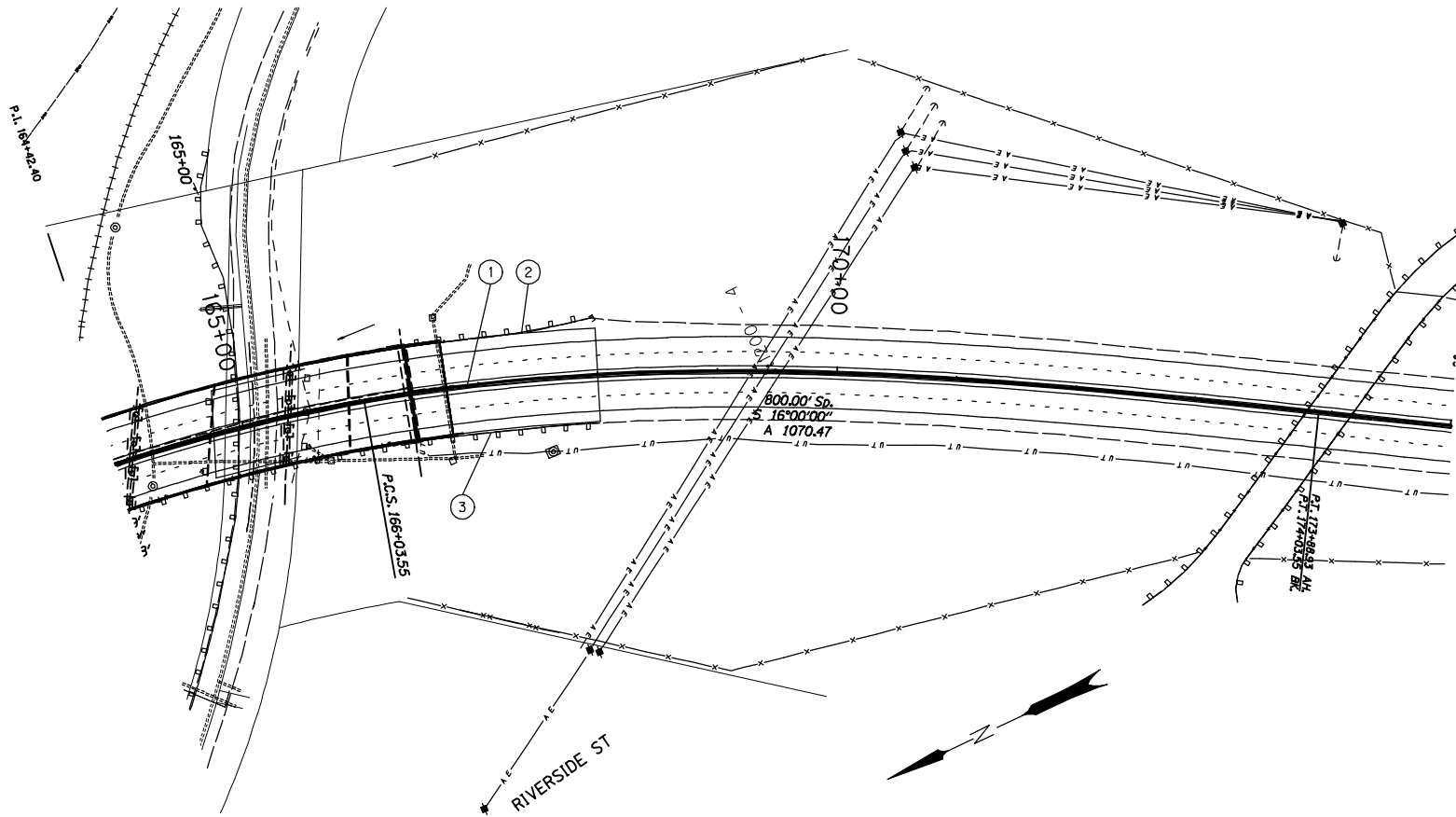
**PROFILE**

SHEET NO. 4A

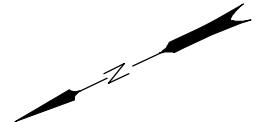
11-18

155

160

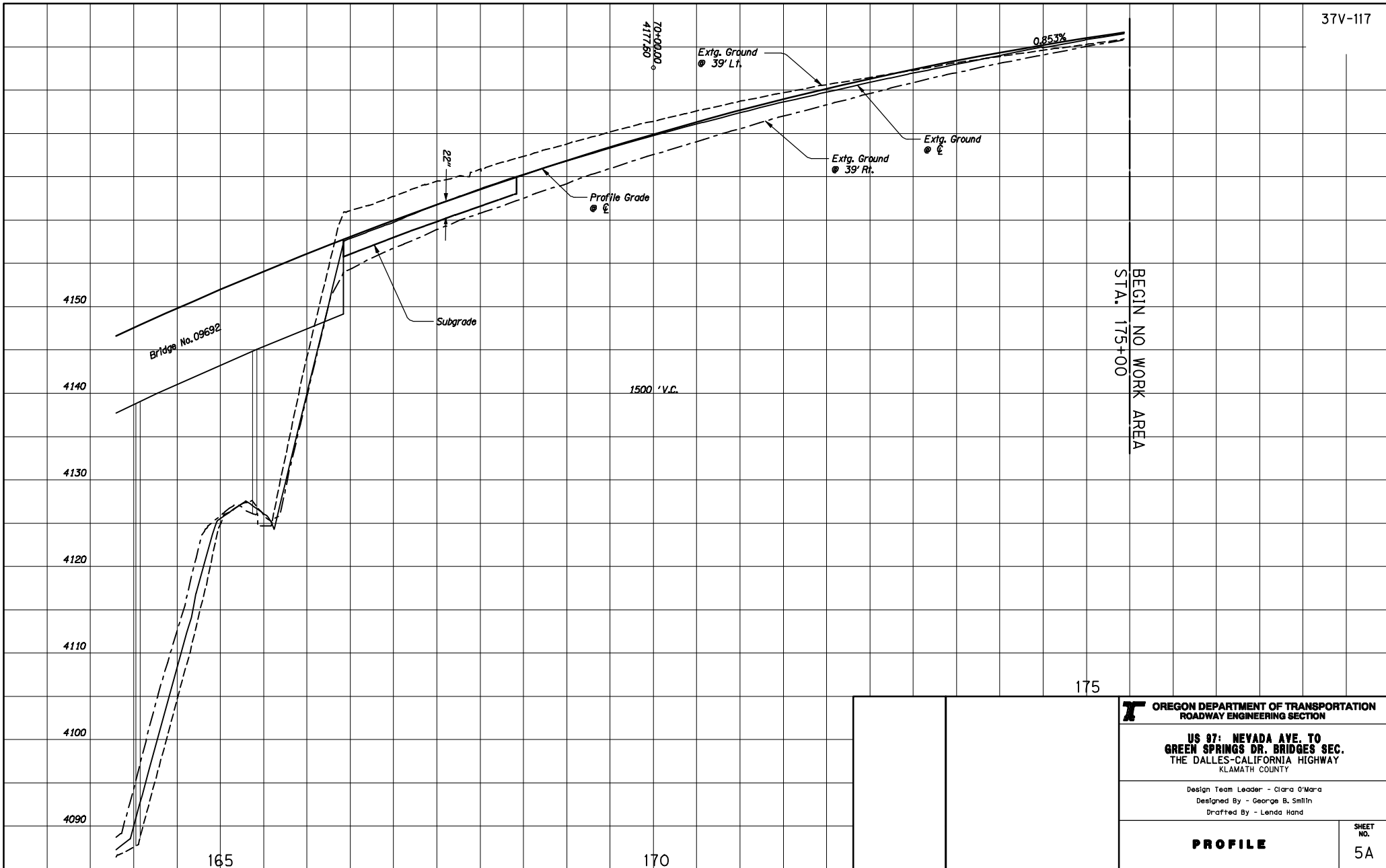


- ① See Note 1, Sht. 4
- ② Sta. 166+61.63 To Sta. 167+91.32  
Remove Extg. Guardrail - 160'  
Const. Guardrail - 12.5' (Type 3)  
      - 62.5' (Type 2A)  
Const. Guardrail Transition  
Flare Rate O, W = 4', E = 2'  
Const. Guardrail Terminal, Flared (L=37.5')
- ③ Sta. 166+61.63 To Sta. 167+74.13  
Remove Extg. Guardrail - 150'  
Const. Guardrail - 12.5' (Type 3)  
      - 100' (Type 2A)  
Const. Bridge Connection  
Flare Rate O, W = 0', E = 2'  
Const. Guardrail Anchor,  
Type 1 Modified (Steel)  
Type B End Piece



11-19

	<b>OREGON DEPARTMENT OF TRANSPORTATION ROADWAY ENGINEERING SECTION</b>
	<b>US 97: NEVADA AVE. TO GREEN SPRINGS DR. BRIDGES SEC. THE DALLES-CALIFORNIA HIGHWAY KLAMATH COUNTY</b>
	Design Team Leader - Clara O'Mara Designed By - George B. Smilin Drafted By - Lenda Hand
<b>GENERAL CONSTRUCTION</b>	SHEET NO. <b>5</b>



BEGIN NO WORK AREA  
STA. 175+00

175

<b>OREGON DEPARTMENT OF TRANSPORTATION</b> ROADWAY ENGINEERING SECTION	
<b>US 97: NEVADA AVE. TO GREEN SPRINGS DR. BRIDGES SEC.</b> THE DALLES-CALIFORNIA HIGHWAY KLAMATH COUNTY	
Design Team Leader - Clara O'Mara Designed By - George B. Smith Drafted By - Londa Hand	
<b>PROFILE</b>	SHEET NO. <b>5A</b>