

OREGON SHPO CLEARANCE FORM

Do not use this form for ODOT or Federal Highway projects or to record archaeological sites

*This form is for: **federal** cultural resource reviews (Section 106); **state** cultural resource reviews (ORS 358.653)*

SECTION 1: PROPERTY INFORMATION	SHPO Case Number:
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Resource Name: **I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643)**

Street Address: N/A (Coordinates: 43.55049, -123.28614)

City: Rice Hill (*vicinity*) County: Douglas

Agency Project # *Not Assigned* Project Name: I-5 MP149.39 Equipment Overpass Bridge Removal

If there is not a street address, include the Township, Range, and Section, cross streets, or other address description

Owner: Private Local Gov State Gov Federal Gov Other: _____

Are there one or more buildings or structures? YES NO – If no, skip to Section 2 and append photo(s)

Is the property listed in the National Register of Historic Places? YES – Individually YES – In a district NO

Original Construction date: 1953 Check box if date is estimated

Siding Type(s) and Material(s): Concrete Window Type(s) and Material(s): N/A

Has the property been physically altered? No Alterations Few Alterations Major / Many Alterations

SECTION 2: APPLICANT DETERMINATION OF ELIGIBILITY - Check the appropriate box

The purpose of this review is to avoid impacts to properties that are "eligible" (historic) or already listed in the National Register of Historic Places. Fully establishing historic significance can be very costly and time consuming. Therefore initial evaluations are based on age (50 years or greater) and integrity (historic appearance), which are the minimum qualifications for listing in the National Register. Additional documentation may be needed further in the process, but typically initial evaluations allow the review process to proceed expeditiously.

The property is considered **Eligible** at this time because it is already listed in the National Register **or**

- is at least 50 years old **and** retains its historic integrity (minimal alterations to key features)
- has potential significance (architectural or historical)

The property is considered **Not Eligible** at this time because it:

- is less than 50 years old **or** is 50 years or older but there have been major alterations to key features
- is known to have no significance, based on National Register-level documentation and evaluation

SECTION 3: APPLICANT DETERMINATION OF EFFECT - Check the appropriate box

The project has **NO EFFECT** on a property that is eligible or already listed in the National Register, either because there is no eligible property involved or the eligible property will not be impacted physically or visually.

The project will have a minor impact on a property that is eligible or already listed in the National Register, and therefore there is **NO ADVERSE EFFECT**. Minor impacts include replacement of some, but not all, siding, doors, or windows, etc.

The project will have a major impact on a property that is eligible or already listed in the National Register, therefore there is an **ADVERSE EFFECT**. Major impacts include full or partial demolition, complete residing, full window replacement, etc.

STATE HISTORIC PRESERVATION OFFICE COMMENTS – Official use only

Eligibility: Concur with the eligibility determination above.
 Do not concur with the eligibility determination above.

Effect: Concur with the effect determination above.
 Do not concur with the effect determination above.

RECEIVED STAMP

Signed: _____ Date: _____

RLS	
ILS	

CONTACT INFORMATION STAMP

Comments:

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SECTION 4: PREVIOUS ALTERATIONS TO THE BUILDING OR STRUCTURE

Only complete this section for buildings that are 50 years old or older. Describe any alterations that have already occurred to the building, such as material replacement, including siding, windows, and doors; any additions, including garages; and any removal or addition of architectural details, such as brackets, columns, and trim. Provide estimated dates for the work. Attach additional pages as necessary.

The Equipment Overpass Bridge located over Interstate 5 at mile-post 149.39 was constructed in 1953 and is identified by the Oregon Department of Transportation (ODOT) as Bridge #07643. The only visible and documented alteration is the addition of protective screening in the form of chain-link fencing attached directly to the concrete railing. Based on drawings for the proposed alteration on file at ODOT, it was determined that the screening was added circa 2002. The bridge is in fair condition, although there are visible signs on the underside of the structure that large vehicles traveling down the interstate have come into contact with the resource at least once, causing fairly significant structural and aesthetic damage.

SECTION 5: PROJECT DESCRIPTION

Describe what work is proposed, including what materials will be used and how they will be installed. Specifically identify what historic materials will be retained, restored, replaced, or covered. Include drawings, photos, cut sheets (product descriptions), additional sheets, and other materials as necessary. For vacant lots, please describe the intended use.

This project includes the removal of the bridge that crosses over Interstate 5 at MP 149.39. Traffic control for this project will be performed by ODOT District 7 maintenance forces, and the contract to remove the bridge will be handled via specification with supplemental as-built drawings. The bridge is the only technical resource required for this project and it will not be replaced. The bridge is not associated with a paved road and does not appear to have ever been associated with a designated transportation route.

SECTION 6: FUNDING SOURCE

- ARRA
 FCC
 FERC
 HUD
 ODOE
 USDARD
 USFS
 Other: Oregon Department of Transportation (ODOT) – No Federal Funding/State Funds Only

SECTION 7: AGENCY CONTACT INFORMATION

Name of Organization Submitting the Project: Oregon Department of Transportation (ODOT)

Project Contact Name and Title: Kenny Gunn, Historian

Street Address, City, Zip: 4040 Fairview Industrial Drive SE, Salem, OR 97302

Phone: 503.934.2004

Email: kenneth.j.gunn@odot.state.or.us

Date of Submission: October 17, 2017

SECTION 8: ATTACHMENTS

REQUIRED	<input checked="" type="checkbox"/> 3 – 4, color, 4 x 5 photographs of the subject property, digital or print. One photo is sufficient for vacant property
AS NEEDED <i>Contact SHPO staff with questions</i>	<input checked="" type="checkbox"/> Project area map, for projects including more than one tax lot
	<input checked="" type="checkbox"/> Additional drawings, reports, or other relevant materials
	<input checked="" type="checkbox"/> Continuation sheet for sections 4 or 5, or additional context to determine National Register Eligibility.

SHPO Mailing Address: Review and Compliance, Oregon SHPO, 725 Summer St. NE, Suite C, Salem, OR 97301
Documents meeting all aspects of the digital submission policy may be submitted by email to
ORSHPO.Clearance@oregon.gov

OREGON SHPO CLEARANCE FORM CONTINUATION SHEET

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CONTINUATION SHEET

- Include additional documentation for Section 4 or 5 as necessary. Attach maps, drawings, and reports as needed to illustrate current conditions and the planned project. If submitting this form by email, photos and maps may be inserted into continuation sheets.
- If completing a complete Determination of Eligibility (DOE) or Finding of Effect (FOE), use continuation sheets as necessary or include appendixes.

Physical Description/Statement of Significance

The Equipment Overpass Bridge located over Interstate 5 at mile-post 149.39 was constructed in 1953 and is identified by the Oregon Department of Transportation (ODOT) as Bridge #07643. The four-span, reinforced concrete girder bridge includes flared haunches and protective chain-link fence screening that was installed circa 2002. The bridge has a standard ODOT Type A concrete bridge rail identified by the three incised vertical bars, regularly spaced on both the inner and outer faces of the railing (see photo 3). This design feature was popular with ODOT Type A and Type B bridge rails constructed from the early 1950s through the 1960s; the feature lends the design its colloquial name of "Three Stripe" or "Three Bar" Bridge Rail. Type A/B Three Stripe first appears on Oregon bridges in the early 1950s and appears to have remained the common standard for much of the 1950s and 1960s for Interstate overpasses and grade separations. Currently, the design for this bridge is not unique and is seen repeated on countless overpass bridges located at various points along Interstate 5 in central and southern Oregon.

In addition, this bridge was originally constructed in 1953 to serve as an equipment and cattle overpass, where as it never was associated with a historic road or transportation route (as shown in attached maps and drawings). The overpass is only accessible to private property owners off of John Long Road through a locked gate and leads to open agricultural land. The bridge also meets the criteria outlined in the *Program Comment for Common Post-1945 Concrete and Steel Bridges* released by the Advisory Council on Historic Preservation (ACHP), at the request of the Federal Highway Administration (FHWA). The goal of the program comment is to eliminate individual historic review requirements under Section 106 of the National Historic Preservation Act for common concrete and steel bridges constructed after 1945, therefore, focusing the attention on more unique historic bridges. Although the program comment only applies to federally funded transportation projects, unlike this state funded project, it still provides a context and significance for bridges of this type.

The Interstate 5 Equipment Overpass Bridge located at mile-post 149.39 was constructed in 1953 and is potentially eligible for listing in the National Register of Historic Places (NRHP) based upon its age. However, based on the *Program Comment for Common Post-1945 Concrete and Steel Bridges* and the lack of historic significance, it has been determined that the bridge is not potentially eligible for listing in the NRHP. Therefore, as determined by ODOT, the removal of the I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643) will have no effect on a property that is eligible or already listed in the NRHP because there is no eligible property involved with the proposed project.

Sources

Oregon Department of Transportation (ODOT) Bridge Data System (BDS), Bridge #07643. Drawing No. 10516, 10708, 10709 & BR240-241.

"Slab, Beam & Girder Bridges in Oregon – Historic Context Statement." Prepared by George Kramer for ODOT. May 2004.

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Photo 1: I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643), looking South (taken 10/4/17 by Kenny Gunn)



Photo 2: I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643), showing underside of bridge and damage from large vehicles, looking SE (taken 10/4/17 by Kenny Gunn)

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Photo 3: I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643), showing “Three Bar” and protective screening detail, looking NE (taken 10/4/17 by Kenny Gunn)



Photo 4: I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643), looking West (taken 10/4/17 by Kenny Gunn)

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Photo 5: I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643), looking East (taken 10/4/17 by Kenny Gunn)

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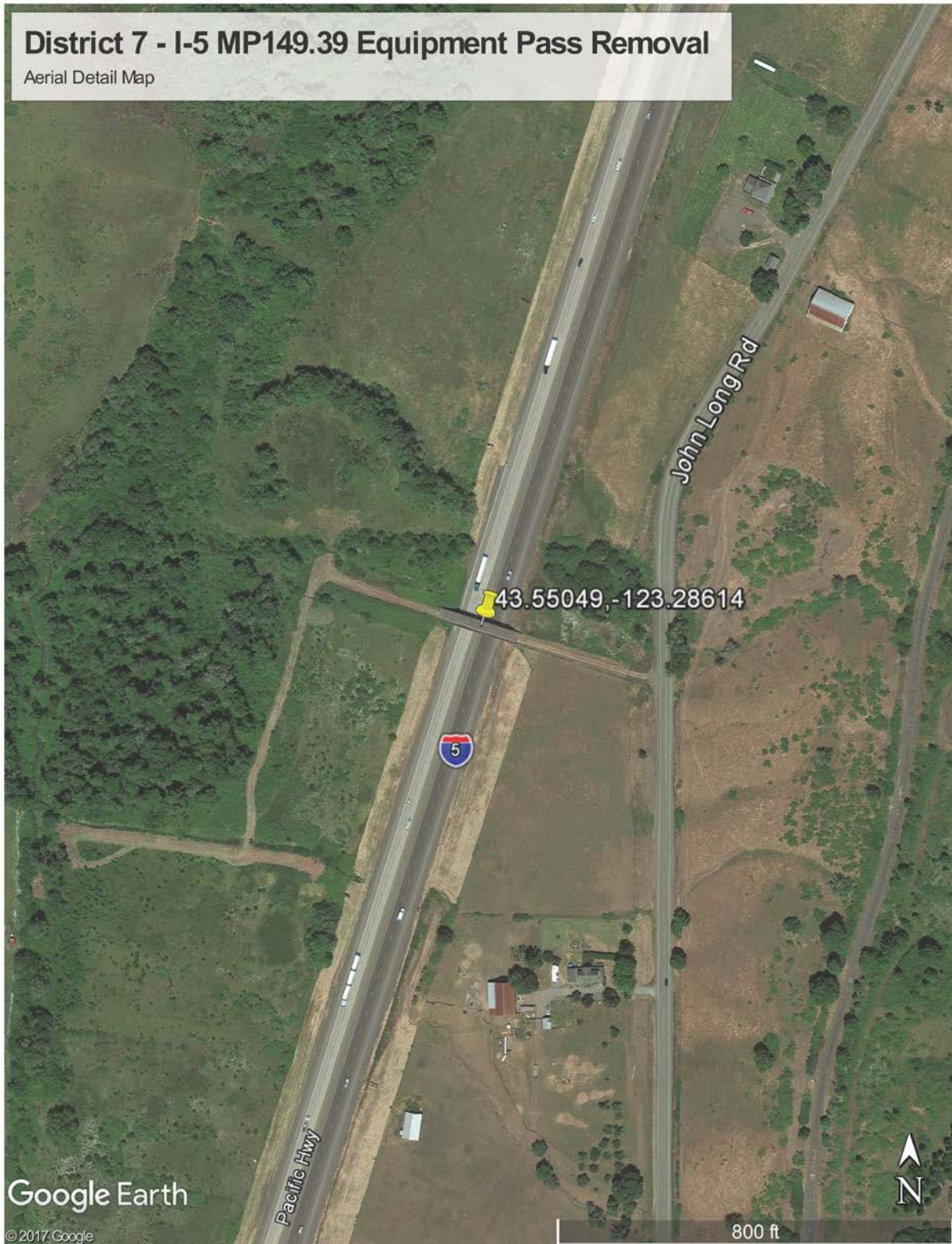


Figure 1: I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643) Aerial Detail Map (Source: Google Earth)

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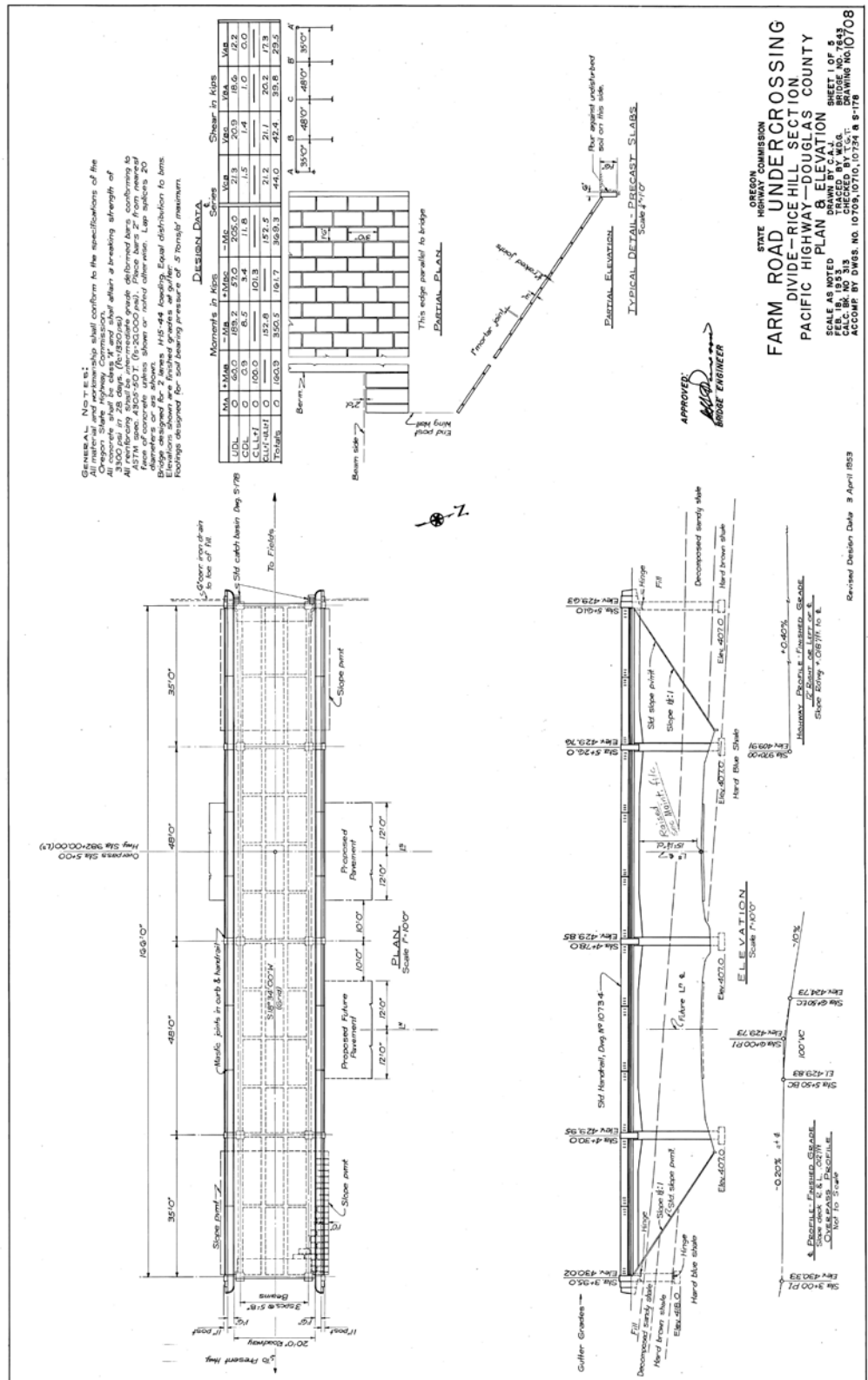


Figure 2: I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643) – 1952 Elevation & Plan Drawing (Source: ODOT)

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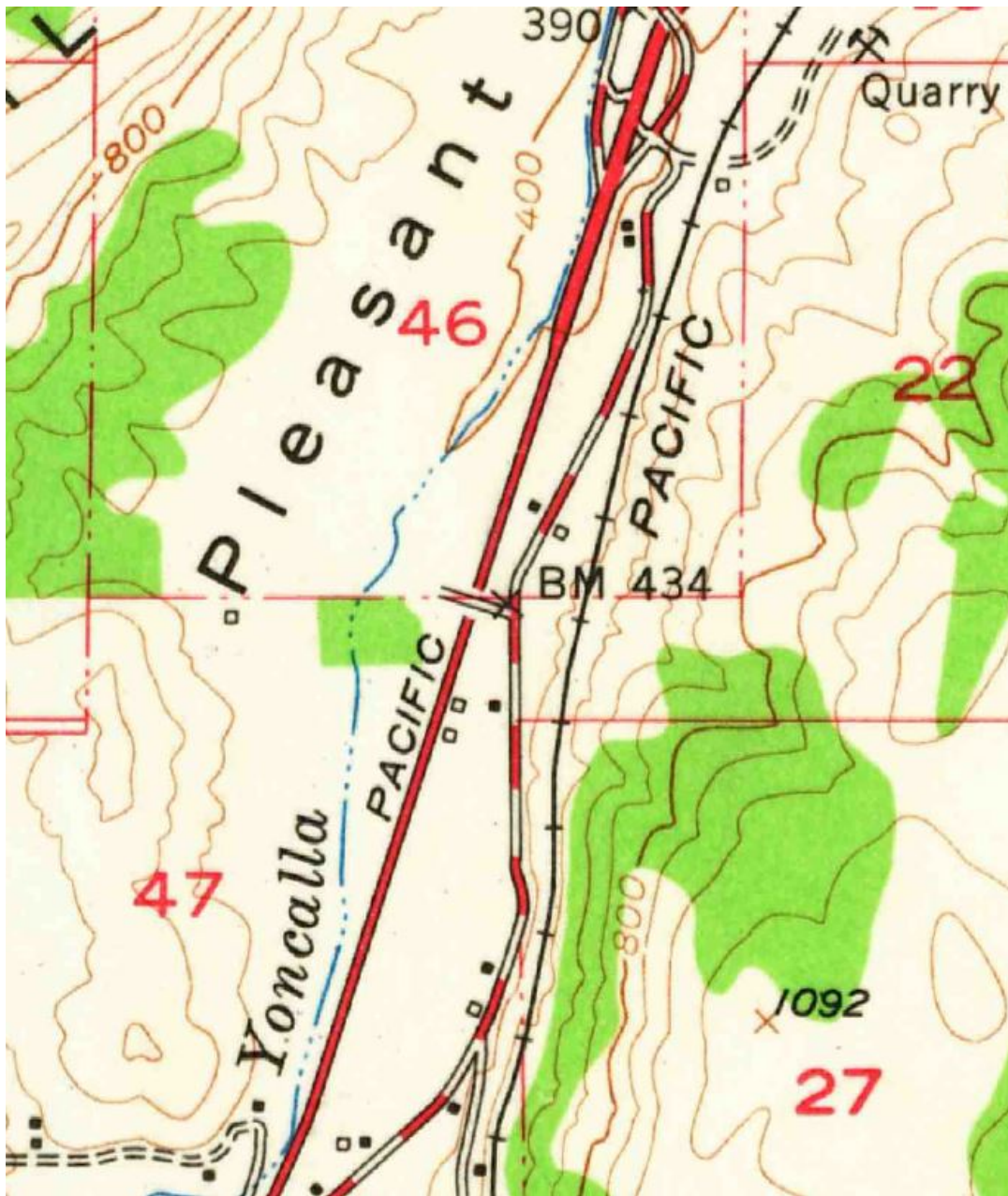


Figure 5: I-5 MP149.39 Equipment Overpass Bridge (Bridge #07643) – 1954 USGS Topographic Map (Source: USGS)

OREGON SHPO CLEARANCE FORM INSTRUCTIONS

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SECTION 1: PROPERTY INFORMATION:

- Include complete address and agency project number and name, if applicable.
- Check YES if there are any buildings on the site. Check NO if it is a vacant parcel (in which case it will be evaluated for archaeology and the potential impact on surrounding buildings only.)
- Check YES if your research (look on our website at <http://heritagedata.prd.state.or.us/historic/> and/or call your local planning office) shows the property is listed. Check NO if you find that it is not listed.
- Fill in the construction date. Check box if date is estimated.
- Describe the siding and window types and materials. Examples: double hung wood windows; vertical wood siding.
- Check to what degree the property has been altered. Ask yourself, would the original owner recognize the building, or have there been many changes that obscure the historic features?

SECTION 2: APPLICANT DETERMINATION OF ELIGIBILITY:

- Check the ELIGIBLE box if the building is listed (National Register OR Local landmarks register), has previously been evaluated as eligible, or is 50 years of age AND the majority of the exterior historic features are retained.
- Check the NOT ELIGIBLE box if the building is not yet 50 years old, or if in your opinion there have been many and/or major changes (e.g. additions, siding and/or window replacement, porch enclosures).
- Applicants who acknowledge that the property meets the minimum qualifications for listing in the National Register but choose to contest this determination must complete a Determination of Eligibility (DOE). The DOE must demonstrate that the property is not eligible for the National Register using the Criteria listed in National Register Bulletin 15, "How to Apply the National Register Criteria for Evaluation." The DOE may be submitted on continuation sheets or as a separate document.

SECTION 3: APPLICANT DETERMINATION OF EFFECT:

- Check the NO EFFECT box if the property is NOT ELIGIBLE for listing in the National Register or if the work will not replace or alter the appearance of any of the building's exterior features.
- Check the NO ADVERSE EFFECT box if the property is ELIGIBLE for listing or is already listed in the National Register and the work is visible (e.g. re-roofing with same materials, window or siding repair, adding a vent) but will not remove or obscure historic features.
- Check ADVERSE EFFECT if the property is ELIGIBLE or listed in the National Register and the work includes major changes, such as replacing the siding or windows.

SECTION 4: PREVIOUS ALTERATIONS

- List the changes that **already** occurred to the building, including siding, windows, doors, porches, additions including dormers, or if the property was moved. Include the approximate date of each alteration. The information can be provided in list format.

SECTION 5: PROJECT DESCRIPTION:

- Clearly describe what is being repaired or replaced, and how that work will be done. What materials and installation process are proposed? Include sufficient information (e.g. close-up photos, product specification sheets) so we can compare what exists with what is proposed. The information can be provided in list format.

SECTION 6: FUNDING SOURCE:

- Check the federal or state agency funding the project; or check "other" and fill in the agency name.

SECTION 7: AGENCY CONTACT INFORMATION:

- List the name of the organization submitting the Clearance Form

SECTION 8: ATTACHMENTS:

- Photos: Include photos of the entire building, especially the elevations that can be seen from the street. Include close-ups of features that will be impacted by the project.
- Additional Information: When applicable, include window specifications, plans or diagrams that illustrate pertinent existing conditions and/or proposed work
- Continuation sheets for additional Section 4 or 5 narrative or to append a formal Determination of Eligibility (DOE) or Finding of Effect (FOE). These materials may also be submitted as a separate document.